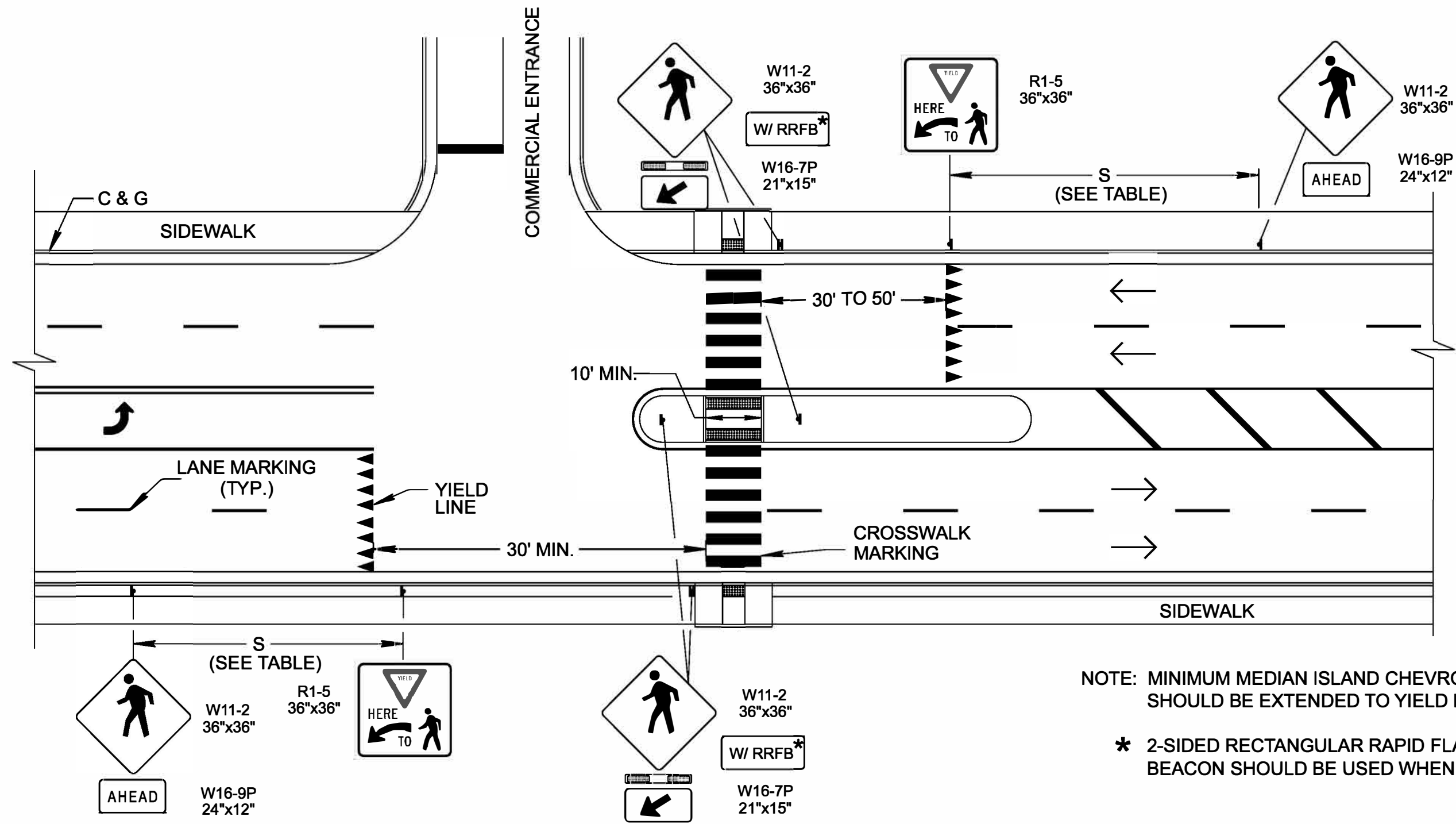
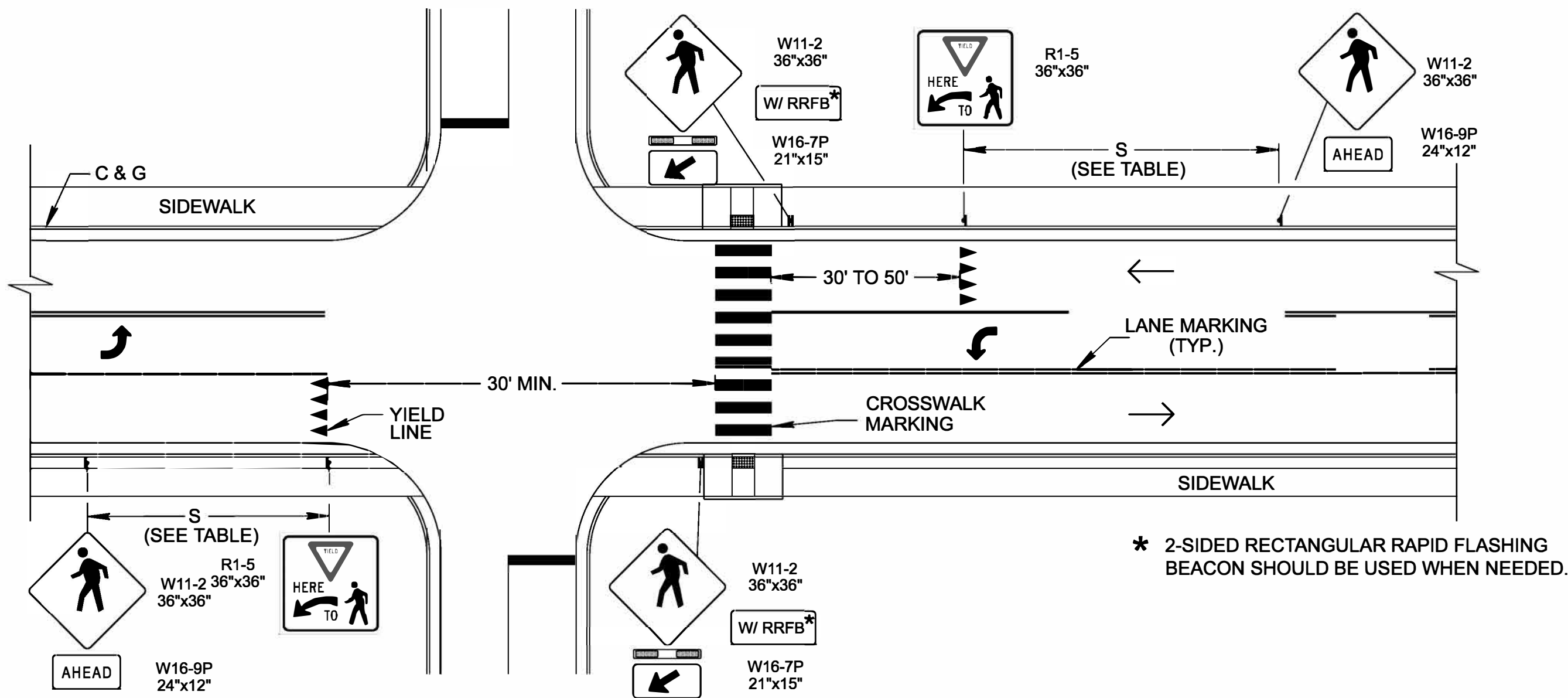


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**FIVE-LANE WITH RECTANGULAR RAPID FLASHING BEACON
MID-BLOCK CROSSING**



**THREE-LANE WITH RECTANGULAR RAPID FLASHING BEACON
INTERSECTION CROSSING**

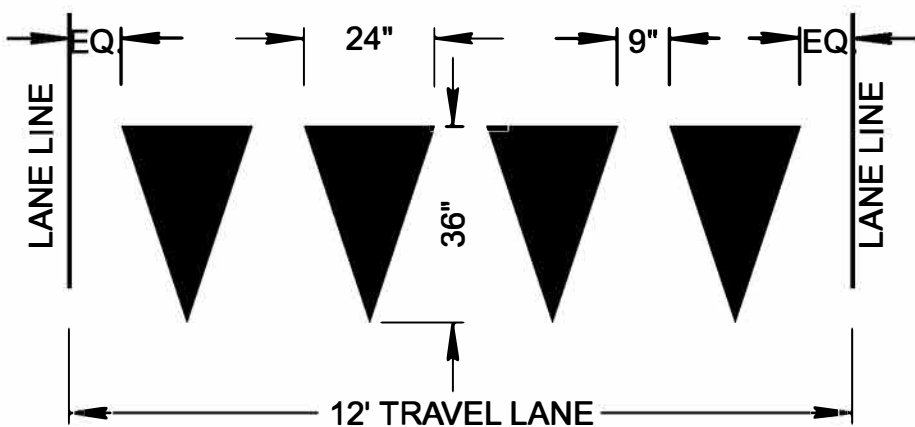
**MINIMUM ADVANCE PLACEMENT
OF PEDESTRIAN WARNING SIGNS**

POSTED SPEED	WARNING SIGNS MINIMUM ADVANCE PLACEMENT DISTANCE - S
≤ 35 MPH	250 FT
40 MPH	305 FT

NOTES: WHERE THE SPEED LIMIT EXCEEDS 40 MPH,
MARKED CROSSWALKS ALONE SHOULD NOT BE
USED AT UNSIGNALIZED (NO SIGNAL) LOCATIONS.

PEDESTRIAN WARNING SIGNS SHOULD NOT BE
USED WITHIN 100' OF THE EDGE OF AN INTERSECTION.

LEGEND	
	GROUND MOUNT SIGN
	DETECTABLE WARNING SURFACE
	RECTANGULAR RAPID FLASHING BEACON
	PAVEMENT MARKING SYMBOLS
	DIRECTION OF TRAFFIC



RECOMMENDED YIELD LINE LAYOUTS

NOTES: YIELD LINES MAY BE SMALLER THAN SUGGESTED
WHEN INSTALLED ON MUCH NARROWER, SLOW SPEED
FACILITIES SUCH AS SHARED -USE PATHS.

AREA OF EACH TRIANGLE A BASE OF 24 INCHES
AND A HEIGHT OF 36 INCHES IS = 3 SF.

GENERAL NOTES

- RECTANGULAR RAPID FLASHING BEACONS INSTALLED ON TDOT FACILITIES SHALL COMPLY WITH THE REQUIREMENTS OUTLINED IN CHAPTER 4L OF PART 2 OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- DETAILS SHOWN ON THIS STANDARD DRAWING APPLY TO THE CONSTRUCTION OR RECONSTRUCTION OF MID-BLOCK CROSSINGS AND MODIFICATION OF STREETS, CURBS, OR SIDEWALKS ASSOCIATED WITH IT. SEE TDOT-RDG FOR ADDITIONAL INFORMATION FOR SITE SELECTION, NEW CONSTRUCTION OR RECONSTRUCTION DURING PEDESTRIAN SAFETY INITIATIVE, SPOT SAFETY IMPROVEMENTS AT LOCATIONS MAX 45 MPH. OTHER LOCATIONS WILL NEED SITE SPECIFIC ANALYSIS.
- FOR NEW CONSTRUCTION, A TRAFFIC ENGINEERING STUDY WILL HAVE TO BE CONDUCTED TO DETERMINE IF A MID-BLOCK CROSSING IS WARRANTED. MID-BLOCK CROSSINGS SHALL BE INSTALLED DURING RECONSTRUCTION PROJECTS, REPAVING PROJECTS AND AT LOCATIONS WHERE EXISTING PEDESTRIAN SAFETY IS A CONCERN.
- PEDESTRIAN IN CROSSWALK SIGNS (W11-2) SHALL BE INSTALLED AT EACH END OF THE CROSSWALK LOCATION. THE SIGNS SHALL BE PLACED IN ADVANCE OF THE CROSSWALK ADJACENT TO THE TRAVEL LANE AND FACING THE DRIVER. REFER TO THE MUTCD FOR ADDITIONAL WARNING SIGNS, TYPE AND LOCATION. S1-1 (SCHOOL) OR W11-15 (TRAIL) CAN BE USED INSTEAD OF THE W11-2 IN THE VICINITIES OF SCHOOLS AND TRAILS."
- FOR CURB RAMPS, THE DETECTABLE WARNING SURFACE, PAVEMENT MARKINGS, AND CROSSWALK MARKING DETAILS, SEE STD. DWG. SERIES MM-CR AND MM-PM RESPECTIVELY. FOR MARKING STANDARDS AND CONCRETE CURB AND GUTTER SEE STD. DWG T-M- SERIES AND RP-VC SERIES RESPECTIVELY.
- WHILE LOCATION OF SIGN OR PEDESTRIAN SIGNAL MAY VARY BASED ON SITE CONDITIONS, ANY PEDESTRIAN PUSHBUTTON SHALL MEET ALL REQUIREMENTS FOR HEIGHT AND REACH RANGE PER PROWAG. 4FT BY 4FT LANDINGS (MAX 2% SLOPE) SHALL BE ADJACENT TO ANY PUSHBUTTON. PEDESTRIAN PUSHBUTTON SHALL BE INSTALLED IN COMPLIANCE WITH SECTION 4I.05 OF THE MUTCD. AND REFER TO STANDARD DRAWING T-SG-6 FOR PEDESTRIAN PUSHBUTTON POST DETAILS.
- SEE SECTION 3B.19, FIGURE 3B-16(B), AND SECTION 3C OF THE MUTCD FOR UNSIGNALIZED MIDBLOCK CROSSWALKS.
- RECTANGULAR RAPID FLASHING BEACONS (RRFBs) MAY BE USED TO ASSIST PEDESTRIANS CROSSING IN A MARKED CROSSWALK AT AN UNSIGNALIZED INTERSECTION. RRFB'S ARE PARTICULARLY EFFECTIVE AT MULTILANE CROSSINGS WITH SPEED LIMITS LESS THAN 40 MPH. CONSIDER THE PEDESTRIAN HYBRID BEACON (PHB) INSTEAD OF RRFBs FOR ROADWAYS SPEED LIMITS ARE EQUAL TO OR GREATER THAN 40 MPH SEE STANDARD DRAWING T-M-4B FOR SIGNALIZED MID-BLOCK CROSSING.
- A MEDIAN SHOULD BE AT LEAST 8.0 FEET WIDE TO ALLOW THE PEDESTRIAN TO WAIT COMFORTABLY IN THE CENTER, IF THE DESIRED 8 FEET CANNOT BE ACHIEVED, USE A MINIMUM WIDTH OF 6 FEET. THE PEDESTRIAN CROSSWALK MEDIAN ISLAND ARE ADA-APPROVED RAMPS (1:12 GRADE) SHOULD BE USED. IT IS BEST TO PROVIDE A SLIGHT GRADE 2 PERCENT TO PERMIT WATER AND SILT TO DRAIN FROM THE AREA. DRAINAGE STRUCTURES SHALL NOT BE PLACED IN LINE WITH RAMPS. INSTALL CATCH BASINS ON UPSTREAM SIDE OF RAMP FOR ROADS WITH GRADES LESS THAN 2%.
- PARKING AND OTHER SIGHT OBSTRUCTIONS SHOULD BE PROHIBITED FOR AT LEAST 100 FEET IN ADVANCE OF AND AT LEAST 20 FEET BEYOND THE MARKED CROSSWALK, OR SITE ACCOMMODATIONS SHOULD BE MADE THROUGH CURB EXTENSIONS OR OTHER TECHNIQUES TO PROVIDE ADEQUATE SIGHT DISTANCE. THE INSTALLATION SHOULD INCLUDE SUITABLE STANDARD SIGNS AND PAVEMENT MARKINGS.
- DESIGNERS OF MIDBLOCK CROSSINGS SHOULD ALSO CONSIDER ADDING OR ENHANCING STREET ILLUMINATION TO IMPROVE PEDESTRIAN SAFETY.
- MIDBLOCK CROSSWALKS SHOULD BE LOCATED AT LEAST 100 FEET FROM THE NEAREST SIDE STREET OR DRIVEWAY.
- ADD CHANNELIZING DEVICES AT MID-BLOCK PEDESTRIAN CROSSINGS IN CONJUNCTION WITH IN STREET PEDESTRIAN CROSSING (R1-6 SERIES) SIGNS AS NEEDED.
- PAYMENT
702-01, CONCRETE CURB, PER C.Y.,
702-03, CONCRETE COMBINED CURB AND GUTTER, PER C.Y.,
713-15.40, SIGN INSTALLATION (DESCRIPTION), PER LS,
716-02.03, PLASTIC PAVEMENT MARKING (CROSS-WALK), PER L.F.,
716-02.04, PLASTIC PAVEMENT MARKING (CHANNELIZATION STRIPING), PER S.Y.,
716-02.05, PLASTIC PAVEMENT MARKING (STOP LINE), PER L.F.,
716-04.12, PLASTIC PAVEMENT MARKING (YIELD LINE),
730-26.07, FLASHING WARNING BEACON (DESCRIPTION), PER S.F.,
730-50.20, RECTANGULAR RAPID FLASHING BEACON ASSEMBLY (SOLAR POWERED), PER EACH,
730-50.21, RECTANGULAR RAPID FLASHING BEACON ASSEMBLY (HARD-WIRED), PER EACH