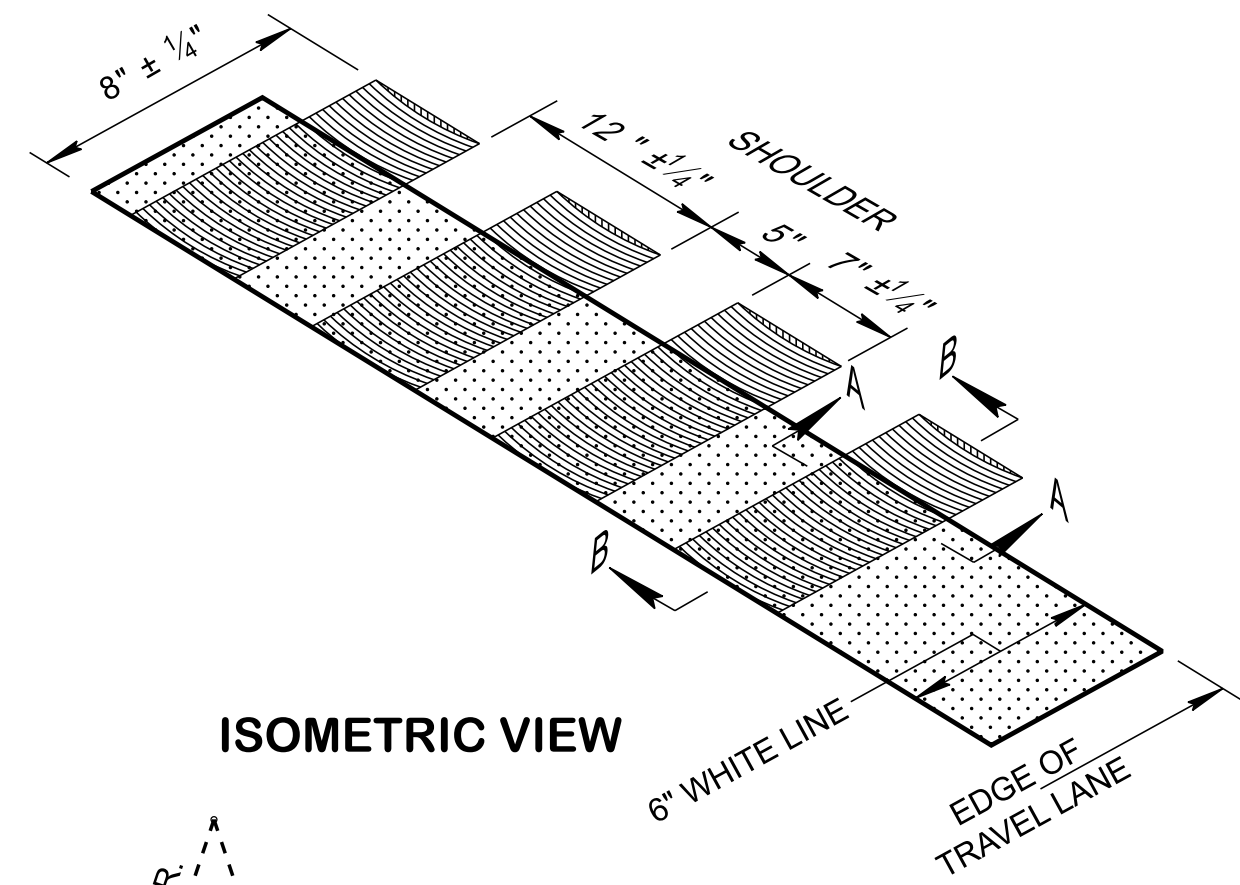
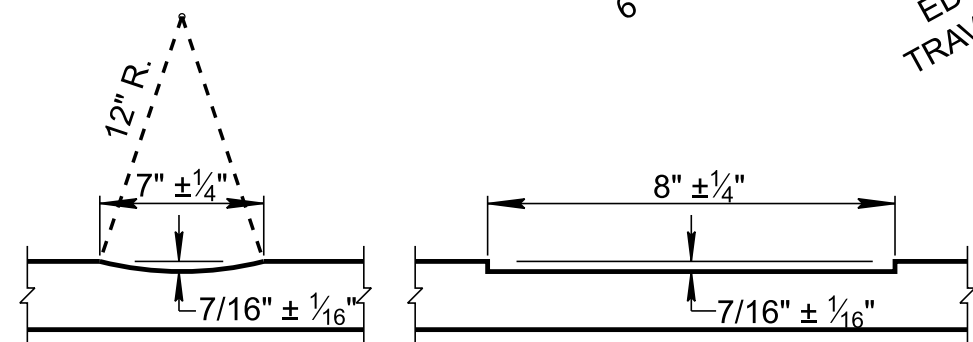


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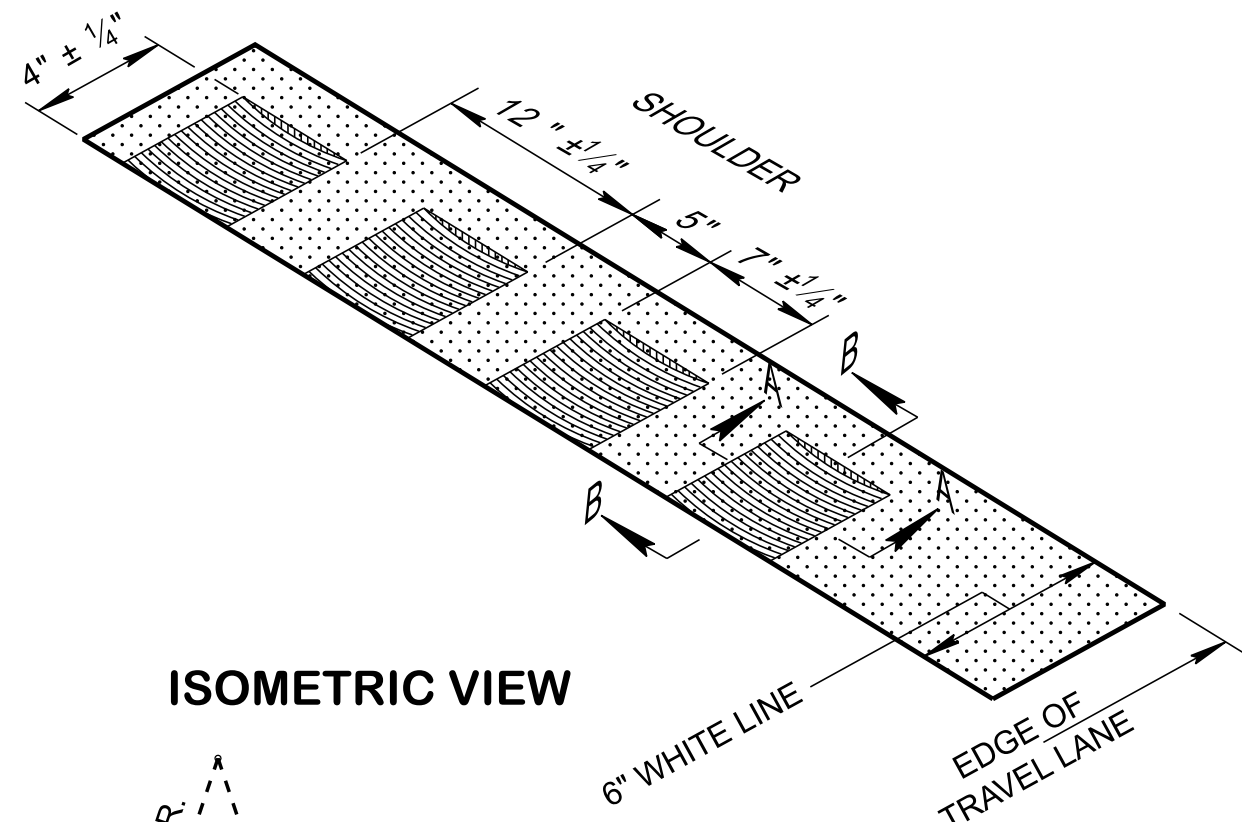
ISOMETRIC VIEW



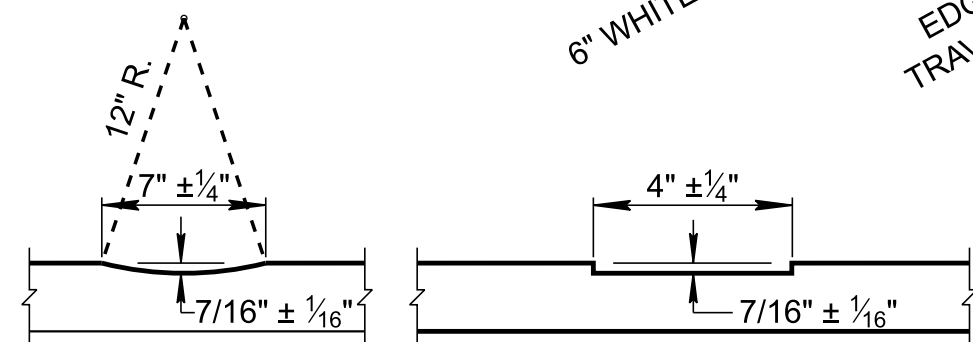
SECTION A-A J

SECTION B-B J

TYPICAL 8" WIDE
RUMBLE STRIPE INSTALLATION



ISOMETRIC VIEW

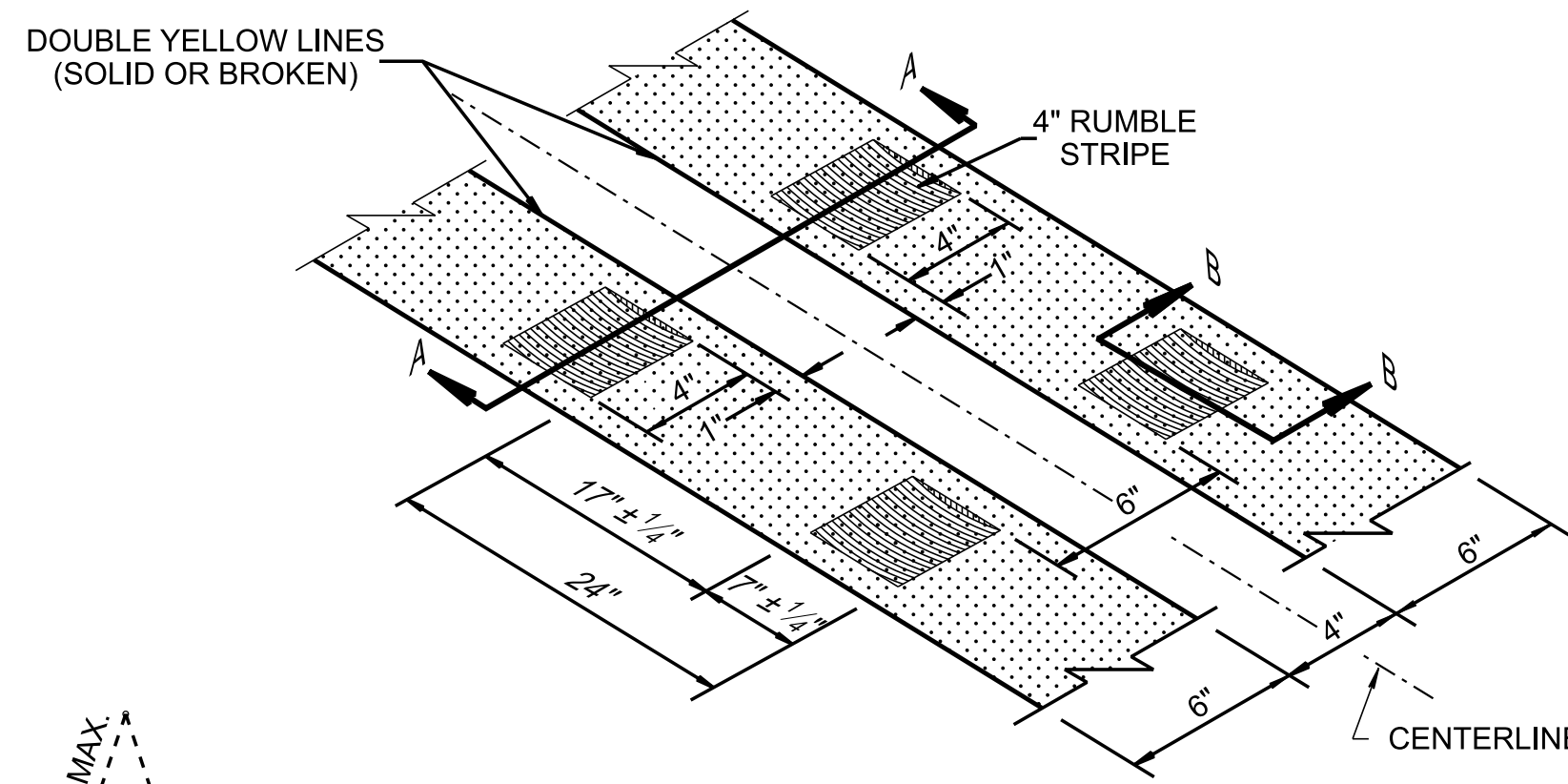


SECTION A-A J

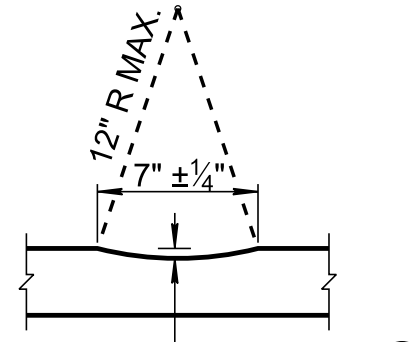
SECTION B-B J

TYPICAL 4" WIDE
RUMBLE STRIPE INSTALLATION

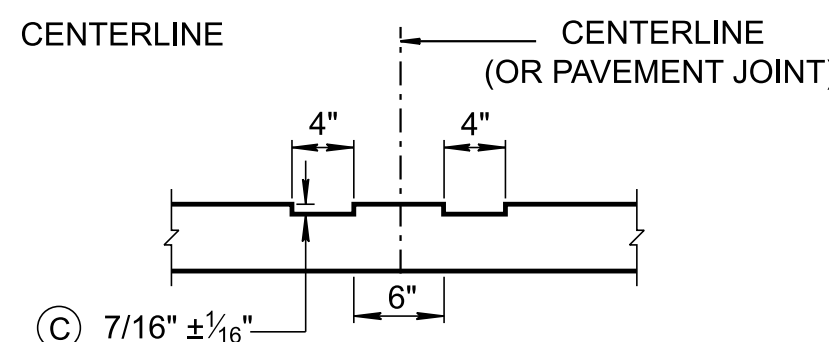
NOTE: 4" WIDE RUMBLE STRIPE IS NOT A PREFERRED APPLICATION. IT SHOULD BE USED IN LOCATIONS WHERE NO SHOULDER IS AVAILABLE AND RUMBLE STRIPE IS REQUIRED FOR A SAFETY UPGRADE. PROFILE THERMOPLASTIC MAY BE USED AS AN ALTERNATE AT LOCATIONS WHERE PAVEMENT SCHEDULE LIMITS USE OF RUMBLES.



ISOMETRIC VIEW



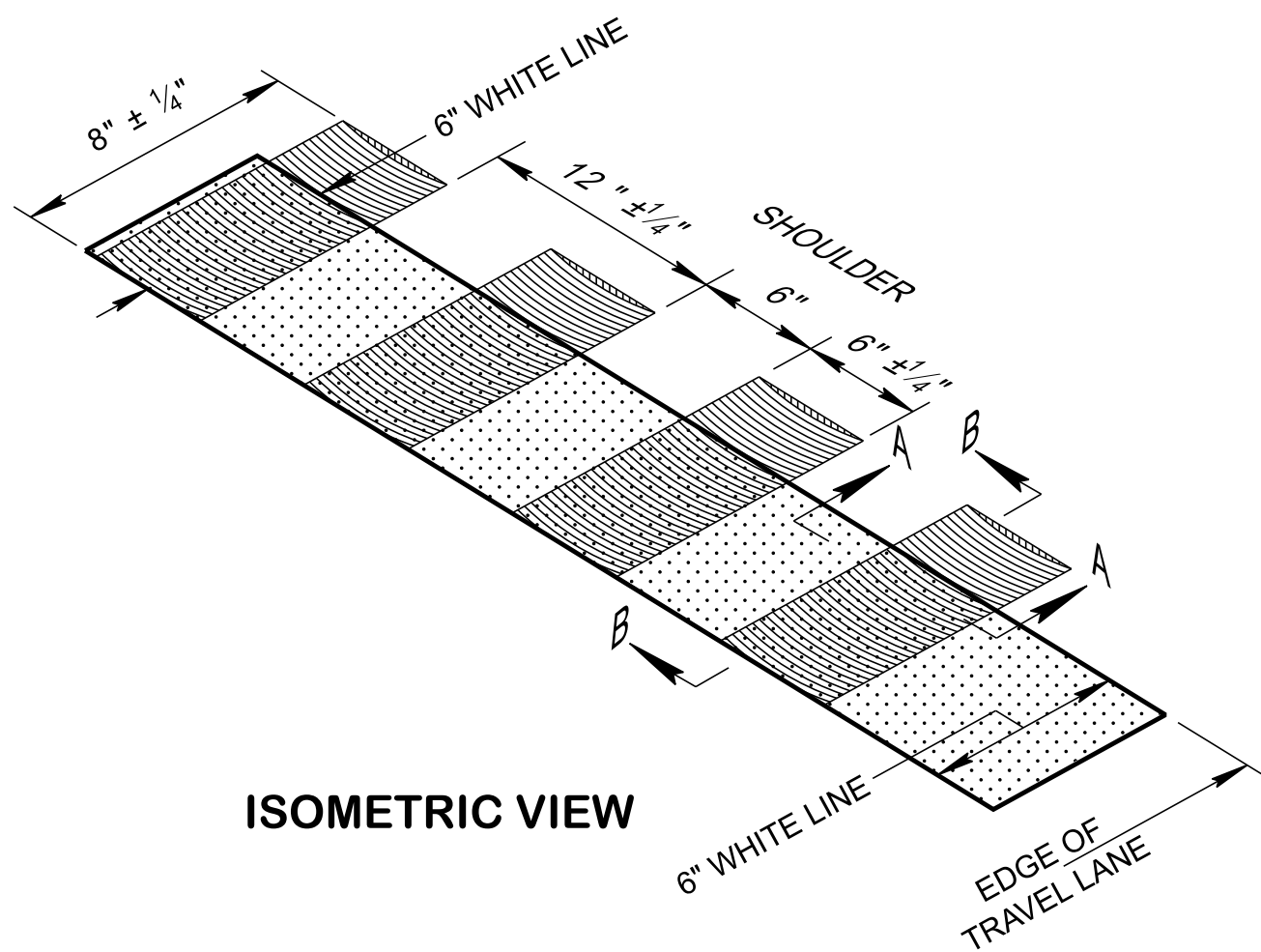
SECTION B-B



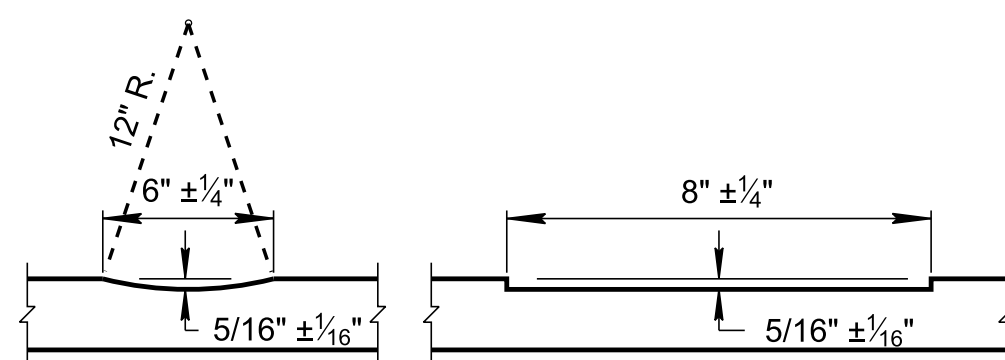
SECTION A-A

CENTERLINE RUMBLE STRIPE
NOT TO BE USED IN PASSING ZONES OR ONE WAY PASSING ZONES

EDGE OF PAVEMENT RUMBLE STRIPE FOR CONVENTIONAL PAVEMENT



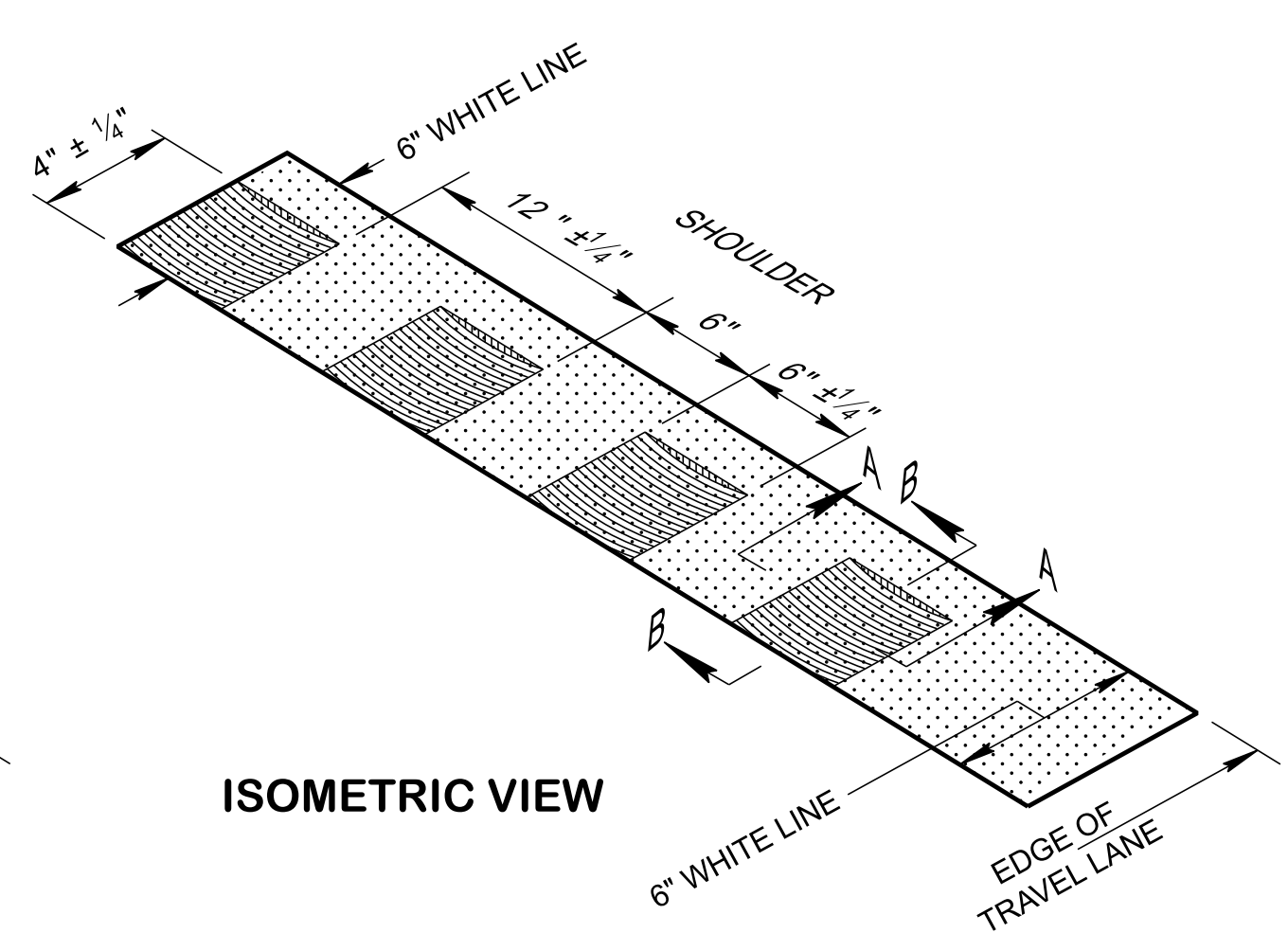
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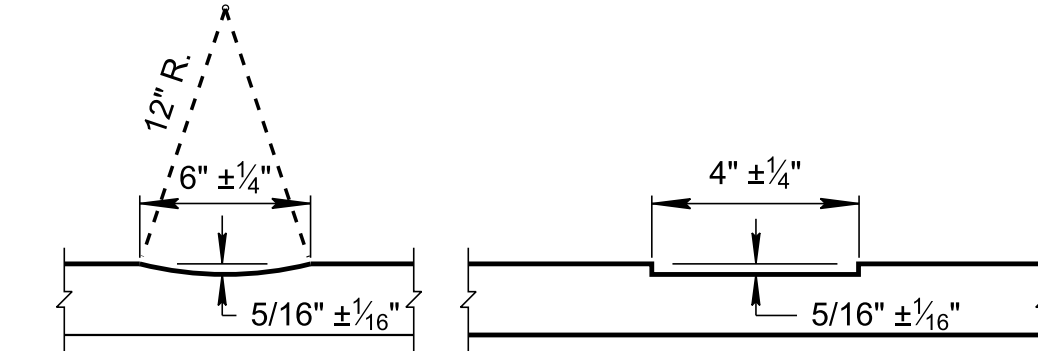
SECTION A-A J

SECTION B-B J

TYPICAL 8" WIDE
RUMBLE STRIPE INSTALLATION



ISOMETRIC VIEW



SECTION A-A J

SECTION B-B J

TYPICAL 4" WIDE
RUMBLE STRIPE INSTALLATION

EDGE OF PAVEMENT RUMBLE STRIPE FOR THIN LIFT PAVEMENT (<1") TYPE (OR SIMILAR)

GENERAL NOTES

- (A) FOR INSTALLATION OF RUMBLE STRIPES SEE THE PLAN VIEWS ON STD. DWG. NO. T-M-16.
- (B) THE COLOR OF AN EDGE LINE OR CENTERLINE ASSOCIATED WITH LONGITUDINAL RUMBLE STRIPE SHALL BE IN ACCORDANCE WITH SECTION 3A.03 OF THE MUTCD.
- CENTERLINE RUMBLE STRIPE**
- (C) THE MILLED-IN RUMBLE STRIPE FOR THE CENTERLINE SHALL HAVE A WITH 7" $\pm 1/4$ " GROOVES, 7/16" $\pm 1/16$ " SCORING DEEP AND ON 24" $\pm 1/4$ " SPACING.
- (D) THE PAVEMENT JOINT SHALL NOT BE MILLED.
- (E) CENTERLINE RUMBLE STRIPES SHALL NOT BE USED ON BRIDGES.
- (F) RUMBLE STRIPE SHALL BE DISCONTINUED WHENEVER THE CENTERLINE PAVEMENT MARKING IS DISCONTINUED.
- (G) RUMBLE STRIPE SHOULD NOT BE USED IN RESIDENTIAL OR COMMERCIAL AREAS.
- (H) IF RAISED PAVEMENT MARKINGS ARE REQUIRED, SEE STD. DWG. T-M-1 FOR SPACING. IN LOCATIONS WHERE RPMS ARE PRESENT, STAGGER RUMBLES SUCH THAT RPMS ARE CENTERED BETWEEN RUMBLES.

EDGE OF PAVEMENT RUMBLE STRIPE

- (I) WHEN RUMBLE STRIPES ARE USED ON NON-ACCESS CONTROLLED FACILITIES, THEY SHOULD BE DISCONTINUED IN ADVANCE OF DRIVEWAYS, INTERSECTIONS, AND MEDIAN OPENINGS AS SHOWN IN THE PLAN VIEW ON STD. DWG. NO. T-M-16.
- (J) ON CONVENTIONAL PAVEMENT, APPLY MILLED-IN RUMBLE STRIPE WITH 7" $\pm 1/4$ " GROOVES, 7/16" $\pm 1/16$ " SCORING DEPTH AND ON 12" $\pm 1/4$ " SPACING. ON THIN LIFT PAVEMENT (<1"), APPLY MILLED-IN RUMBLE STRIPE WITH 6" $\pm 1/4$ " GROOVES, 5/16" $\pm 1/16$ " SCORING DEPTH AND ON 12" $\pm 1/4$ " SPACING.
- (K) ON A THIN LIFT PAVEMENT SECTION THE CONTRACTOR SHALL NOT REMOVE MORE THAN 50% OFF NEW PAVEMENT DEPTH DURING SCORING.

PAYMENT

- (L) SEE STD. DWG. T-M-16 FOR PAYMENT.

REV. 4-21-14: REMOVED TWO WAY PASSING ZONE OPTION. MODIFIED NOTES (A) AND (B).

REV. 7-24-14: CHANGED STRIPE WIDTH.

REV. 08-02-18: CHANGED PAY ITEM NUMBER IN NOTE (B). MODIFIED NOTE (C). CHANGED LENGTH OF RUMBLE STRIPE GROOVE FROM 7" TO 5". CHANGED DISTANCE BETWEEN GROOVES FROM 17" TO 19". ADDED \pm TO BOTH THESE DIMENSIONS. ADDED GENERAL NOTE (I). REDREW SHEET.

REV. 06-28-19: REMOVED 4" PAVEMENT MARKING FROM DRAWING AND REDREW SHEET.

REV. 02-03-20: REVISED DRAWING NAME AND ALL GENERAL NOTES. ADDED EDGE OF PAVEMENT RUMBLE STRIPE DETAILS FOR THIN LIFT SECTION.

REV. 01-24-25: UPDATED TYPICAL DETAILS TO CONFORM WITH MUTCD 11TH EDITION.

APPROVED BY FHWA
(ALL OTHERS APPROVED BY TDOT)

STATE OF TENNESSEE
STANDARD
DRAWING
DEPARTMENT OF TRANSPORTATION

RUMBLE STRIPE
DETAILS FOR
EDGE OF PAVEMENT
AND
CENTERLINE

01-03-13

T-M-16A

NOT TO SCALE