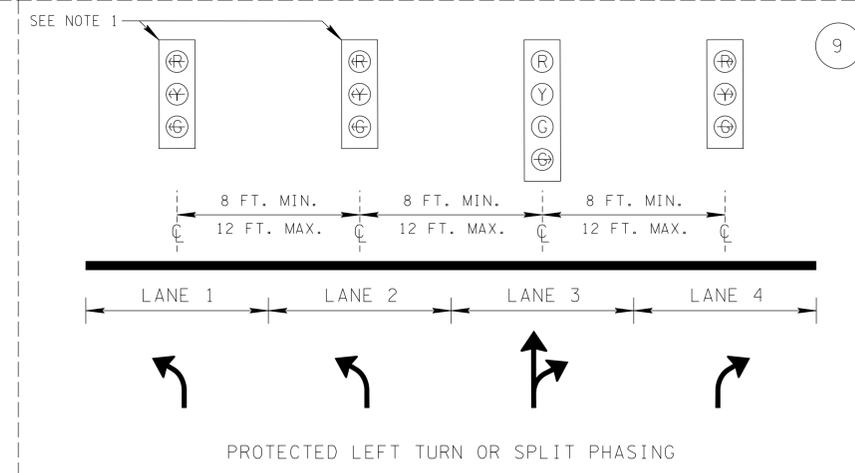
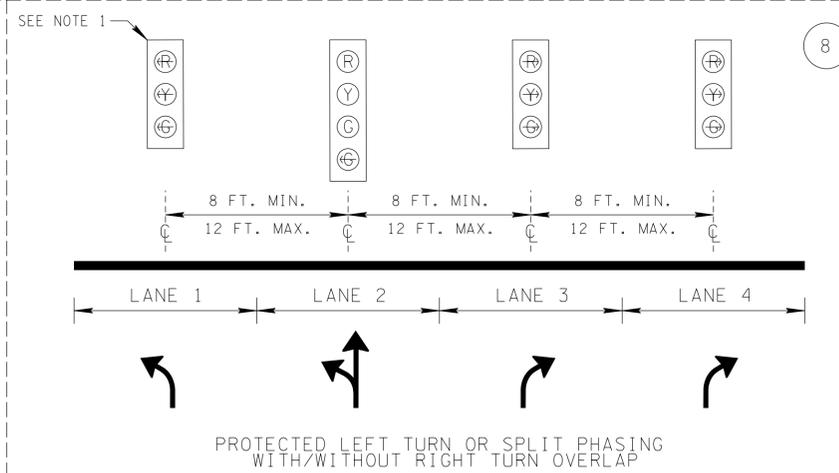
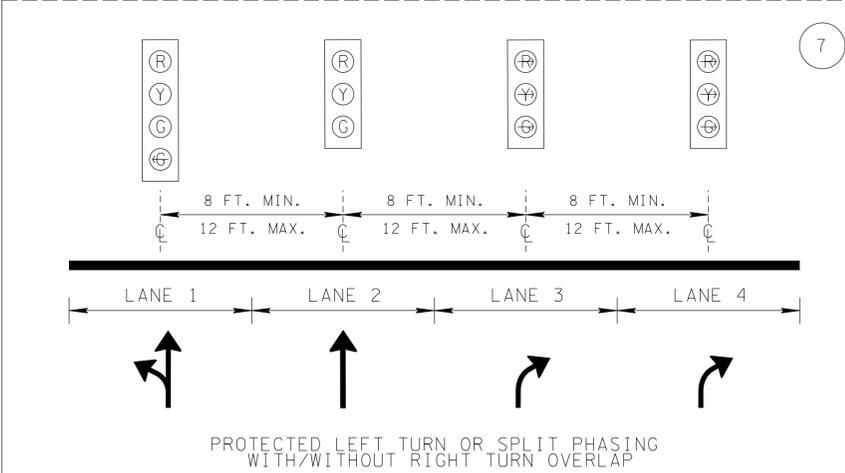
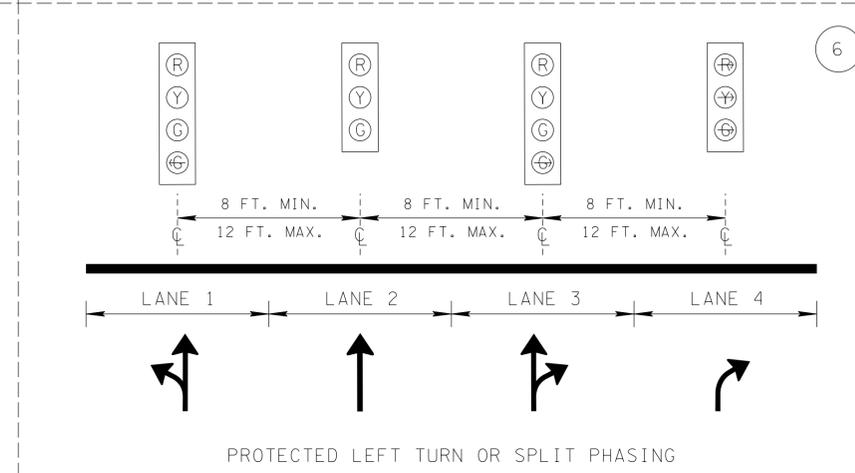
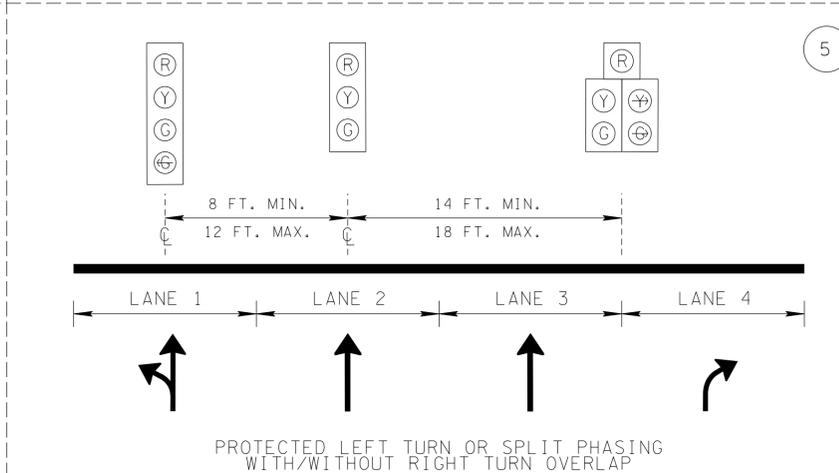
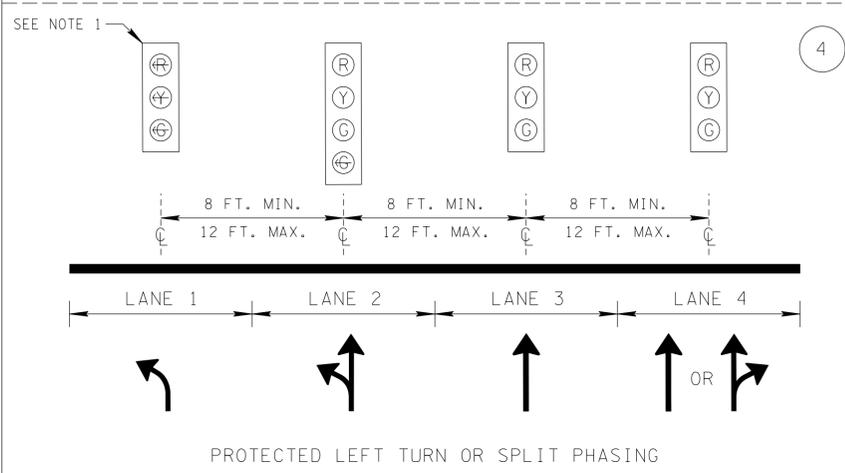
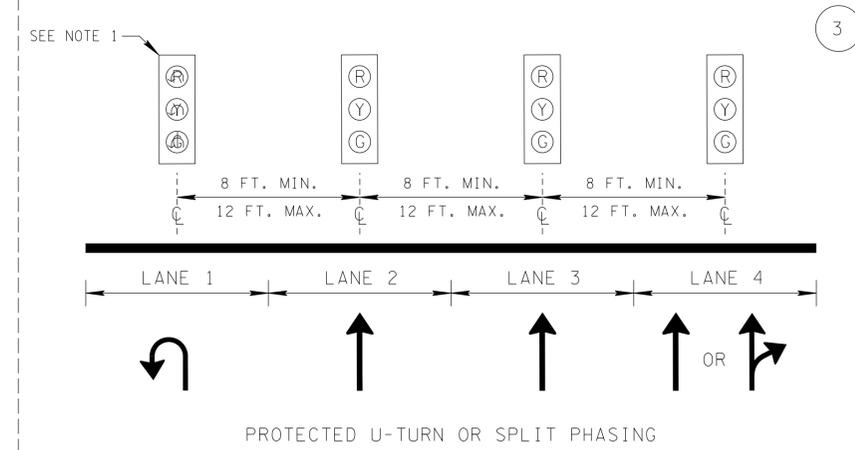
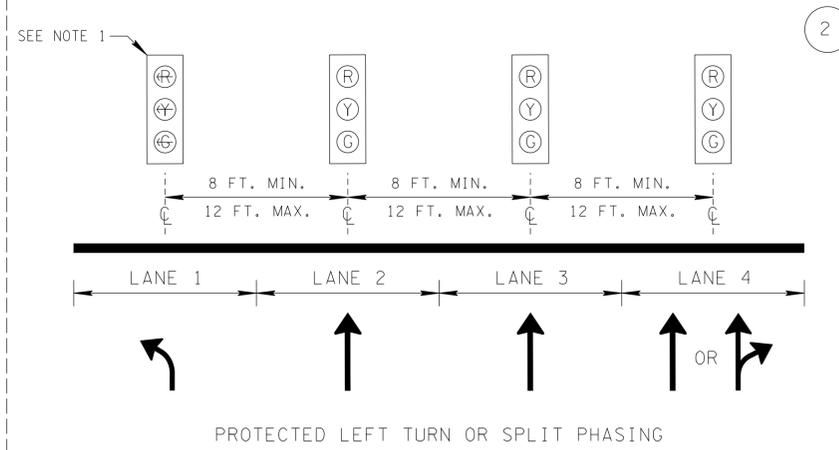
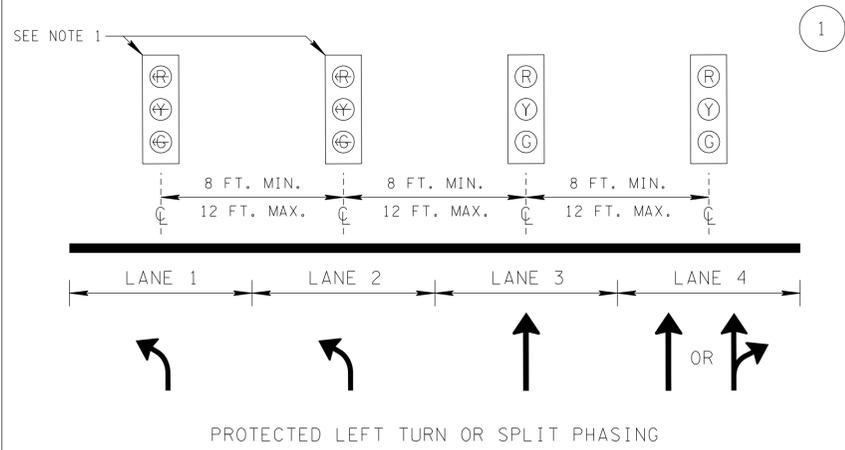


2/19/2025 1:20:40 PM
C:\Users\jll706\OneDrive - Tennessee Documents\Traffic Services\Standard Drawings w. DGN s\TSG7A-S_11172020_Update.dgn



REV. 11-17-20: ADDED DETAILS "PROTECTED LEFT TURN OR SPLIT PHASING WITH/WITHOUT RIGHT TURN OVERLAP" AND "PROTECTED LEFT TURN OR SPLIT PHASING."
REV. 12-18-25: ADDED BLOCK NUMBERS TO SHEET.

NOTE 1: WHERE THERE IS AN OPPOSITE LEFT TURN LANE APPROACH IN THE SAME ALIGNMENT AS THE LEFT TURN LANE, THE LEFT TURN SIGNAL HEAD IS OFFSET TWO FEET (MIN.) CLOSER TO THE THROUGH LANE IN ORDER TO INCREASE THE APPROACH'S SIGNAL HEAD VISIBILITY.
NOTE 2: SEE THE CURRENT EDITION OF THE MUTCD FOR ADDITIONAL INFORMATION REGARDING SIGNAL HEAD PLACEMENTS.