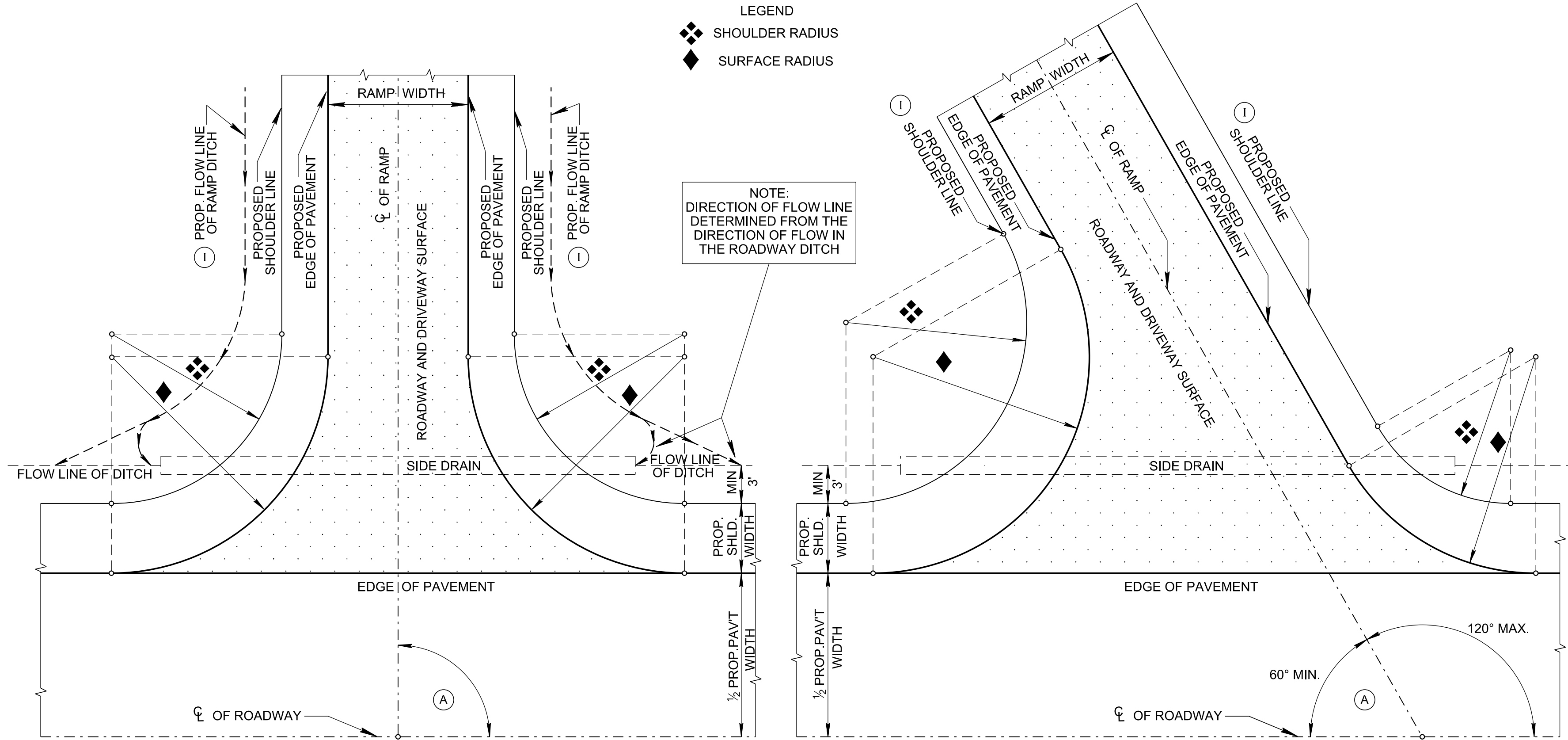


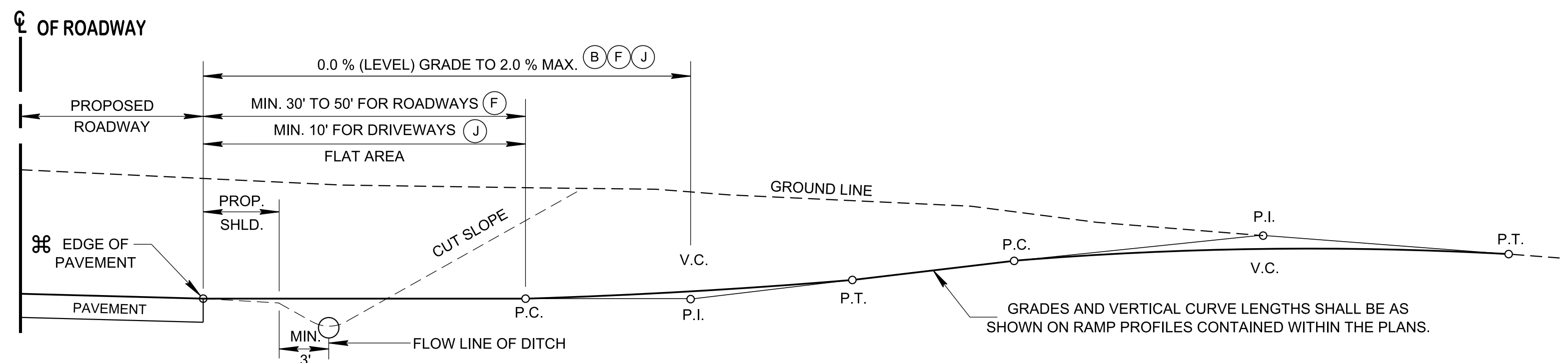
LEGEND  
 ◆ SHOULDER RADIUS  
 ◆ SURFACE RADIUS



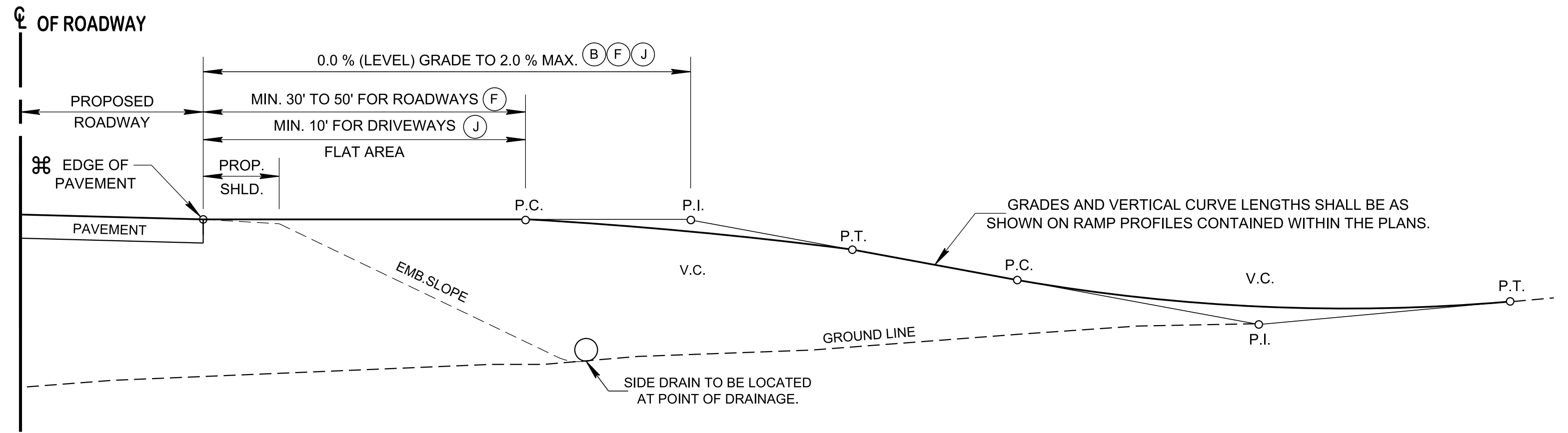
PLAN OF RAMP-90° WITH CL OF SURVEY

PLAN OF RAMP-SKEWED TO CL OF SURVEY

FLOW LINE ARE LEFT OUT FOR CLARITY



PROFILE OF RAMP IN EXCAVATION



PROFILE OF RAMP IN EMBANKMENT

ON ALL PAVING PROJECTS THE ROADWAY AND DRIVEWAY SURFACE MATERIAL SHALL BE PLACED FLUSH WITH ADJACENT EDGE OF PAVEMENT.

### GENERAL NOTES

- (A) ALIGNMENT OF INTERSECTING ROADWAYS AND PRIVATE DRIVES (RAMPS) SHALL BE MODIFIED SO AS TO ATTAIN ADEQUATE SIGHT DISTANCES AND DESIRABLE GRADES. DESIRABLE SKEW IS 90° AND MINIMUM IS 60°.
- (B) SEE AASHTO CURRENT PUBLICATION "A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS" AND TDOT "ROADWAY DESIGN GUIDELINES" FOR INTERSECTION & INTERCHANGE DESIGN REQUIREMENTS.
- (C) WHERE EXISTING SHOULDER LINES AND EDGES OF EXISTING PAVEMENT ARE NOT THE SAME, PROPOSED SHOULDER LINES SHALL TIE INTO EXISTING SHOULDER LINES.
- (D) PROFILE OF EMBANKMENT AND EXCAVATION RAMPS ARE THE SAME EXCEPT FOR LOCATION OF SIDE DRAIN AND DITCHES.
- (E) ALL ROADWAYS AND PRIVATE DRIVES (RAMPS) THAT REQUIRE SIDE DRAINS SHALL USE STANDARD DRAWING NO. D-SEW-1A, UNLESS THE ENDWALL IS PROTECTED BY GUARDRAIL.
- (F) ALL ROADWAYS AND RAMPS AT INTERSECTIONS AND INTERCHANGES SHALL HAVE A FLAT SPACE AT THE TIE IN POINT OF THE INTERSECTING ROADWAY OF 30' TO 50' IN LENGTH WITH A GRADE OF 0.00% DESIRABLE TO A 2.00% MAX. SEE CL PROFILE OF RAMP IN EXCAVATION AND EMBANKMENT DETAILS.
- (G) ANY NECESSARY EXCAVATION FOR INSTALLING RAMPS OR ROADWAYS SHALL BE PAID FOR UNDER ITEM 203-01, ROAD & DRAINAGE EXCAVATION (UNCLASSIFIED), C.Y.
- (H) DRIVEWAY VERTICAL CURVE LENGTHS SHALL BE BASED ON K VALUES OF: CREST, K = 1 AND SAG, K = 2.  

$$K = \frac{L \text{ (LENGTH OF VERTICAL CURVE)}}{A \text{ (ALGEBRAIC DIFFERENCE OF GRADE)}} \quad \text{OR} \quad K \times A = L \text{ (VERTICAL CURVE LENGTH)}$$
- (I) SHOULDERS NOT REQUIRED FOR DRIVEWAYS.
- (J) ALL PRIVATE DRIVES AND BUSINESS ENTRANCE SHALL HAVE A FLAT SPACE AT THE TIE IN POINT OF THE INTERSECTING ROADWAY OF 10' MINIMUM IN LENGTH WITH A GRADE OF 0.00% DESIRABLE TO A 2.00% MAX. SEE CL PROFILE OF RAMP IN EXCAVATION AND EMBANKMENT DETAILS. FOR ADDITIONAL TDOT DRIVEWAY REQUIREMENTS SEE "MANUAL FOR CONSTRUCTING DRIVEWAY ENTRANCES ON STATE HIGHWAYS".

APPROVED BY FHWA  
 (ALL OTHERS APPROVED BY TDOT)

STATE OF TENNESSEE  
 STANDARD DRAWING  
 DEPARTMENT OF TRANSPORTATION

STANDARD RAMP DETAILS FOR ROADWAYS AND DRIVEWAYS

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