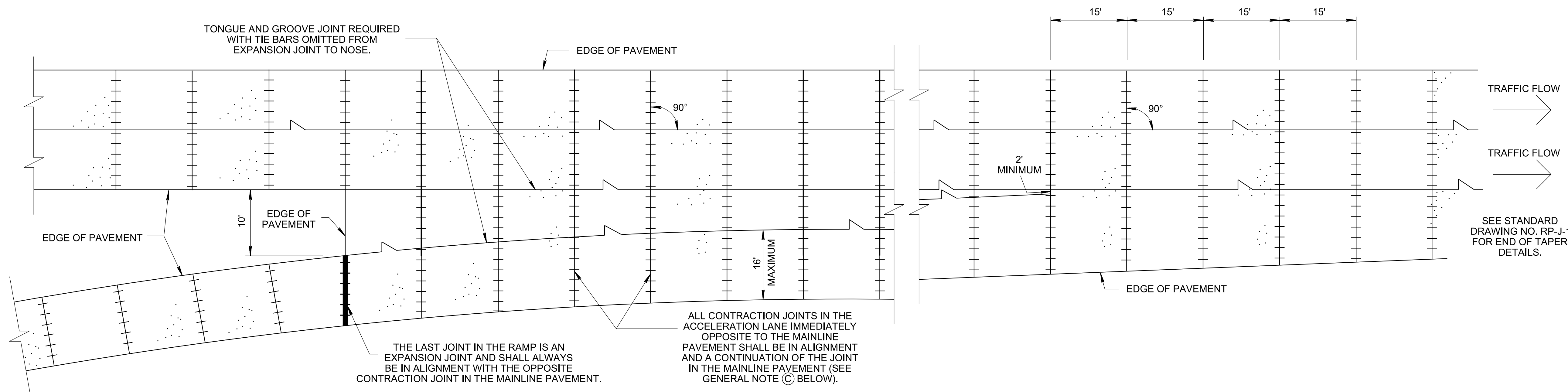


4/15/2020 1:21:26 PM P:\StandDraw\DESIGN STANDARDS\Standards Drawings\Standard Roadway Drawings - CURRENT\In Progress\10-104.00 Roadway, Pavement Appurtenances and Fence IP\104.01 Concrete Pavement IP\RP-J-5



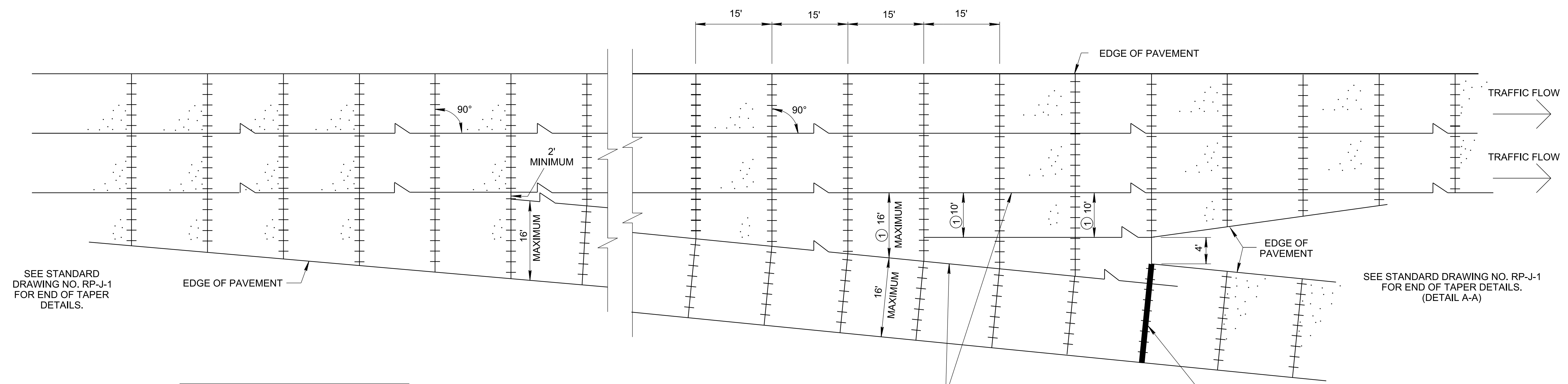
SINGLE LANE ACCELERATION RAMP

THE SPACING OF JOINTS SHALL BE STAGGERED AS SHOWN ON STANDARD DRAWING NO. RP-J-7.

THE LAST JOINT IN THE RAMP IS AN EXPANSION JOINT AND SHALL ALWAYS BE IN ALIGNMENT WITH THE OPPOSITE CONTRACTION JOINT IN THE MAINLINE PAVEMENT.

ALL CONTRACTION JOINTS IN THE ACCELERATION LANE IMMEDIATELY OPPOSITE TO THE MAINLINE PAVEMENT SHALL BE IN ALIGNMENT AND A CONTINUATION OF THE JOINT IN THE MAINLINE PAVEMENT (SEE GENERAL NOTE (C) BELOW).

TRAFFIC FLOW
 TRAFFIC FLOW
 SEE STANDARD DRAWING NO. RP-J-1 FOR END OF TAPER DETAILS.



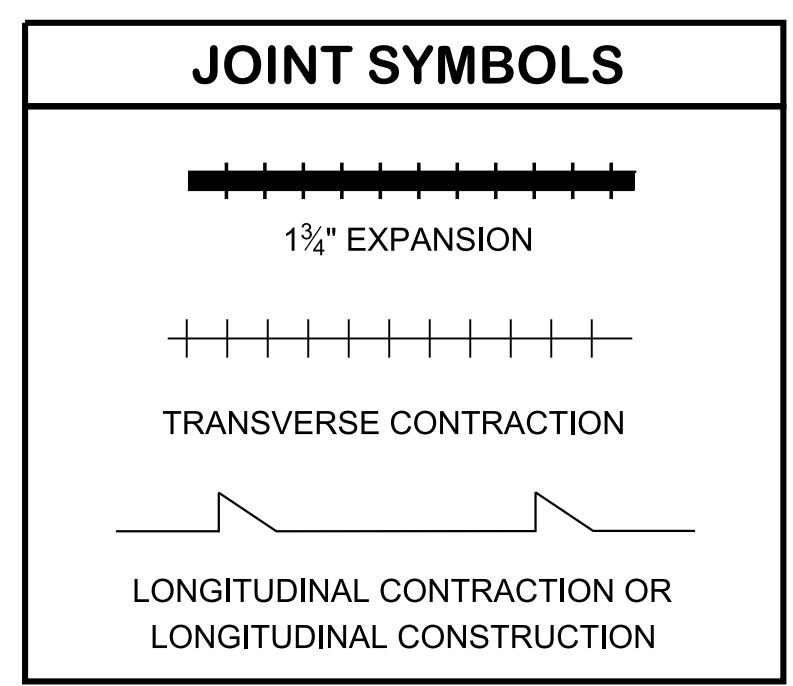
SINGLE LANE DECELERATION RAMP

SEE STANDARD DRAWING NO. RP-J-1 FOR END OF TAPER DETAILS.

SEE STANDARD DRAWING NO. RP-J-1 FOR END OF TAPER DETAILS. (DETAIL A-A)

① WHEN THE EXIT GORE WIDTH REACHES 16'-0", ANOTHER LONGITUDINAL JOINT SHALL BE PLACED AS SHOWN, BEGINNING AT THE NEAREST TRANSVERSE JOINT AND EXTENDING PARALLEL TO THE MAINLINE TO THE "NOSE" OF THE GORE AREA.

THE FIRST JOINT IN RAMP WILL BE AN EXPANSION JOINT AND SPACING OF JOINTS SHALL STAGGERED AS SHOWN ON STANDARD DRAWING NO. RP-J-7.



GENERAL NOTES

(A) UNLESS OTHERWISE NOTED IN THE PLANS, THE TRANSVERSE CONTRACTION AND EXPANSION JOINTS SHALL BE SKEWED AT 90° TO THE ROADWAY CENTERLINE OR BASELINE.

(B) NORMAL SPACING OF THE TRANSVERSE CONTRACTION JOINTS INDICATED ON THIS DRAWING ARE TO BE IN ACCORDANCE WITH SPACING INDICATED ON STANDARD DRAWING NO. RP-J-1.

(C) WHEN PROPOSED ASPHALT PAVEMENT (MAINLINE) AND PROPOSED PORTLAND CEMENT CONCRETE PAVEMENT (RAMPS) ARE USED, THE NORMAL SPACING OF THE TRANSVERSE CONTRACTION JOINTS INDICATED ON THE "RP-J" STANDARD SERIES OF DRAWINGS IS TO BE USED.

- REV. 1-31-83: ADDED JOINT SKEW NOTE AND NOTE ①. REQUIRED EXIT GORE JOINT.
- REV. 6-23-88: DELETED JOINT SKEW.
- REV. 3-20-91: REDREW SHEET AND CHANGED JOINT SPACING FOR CONCRETE PAVEMENT USING STONE. ELIMINATED PARTIAL USE OF EXPANSION JOINTS.
- REV. 12-18-94: CHANGED DRAWING REFERENCE NUMBER IN CROSS-REFERENCE BLOCK.
- REV. 10-26-00: CHANGED VARIABLE JOINT SPACING TO 15' CONSTANT.
- REV. 7-1-01: CHANGED DRAWING NAME.
- REV. 05-01-20: REDREW SHEET.

CROSS-REFERENCE DRAWINGS FOR THIS DRAWING: RP-J-1, RP-J-7, RP-J-9, RP-J-11, RP-J-13 AND RP-J-15.

APPROVED BY FHWA (ALL OTHERS APPROVED BY TDOT)

STATE OF TENNESSEE
 STANDARD DRAWING
 DEPARTMENT OF TRANSPORTATION

TYPICAL ACCELERATION AND DECELERATION LANE JOINT TYPES AND SPACING FOR CONCRETE RAMPS