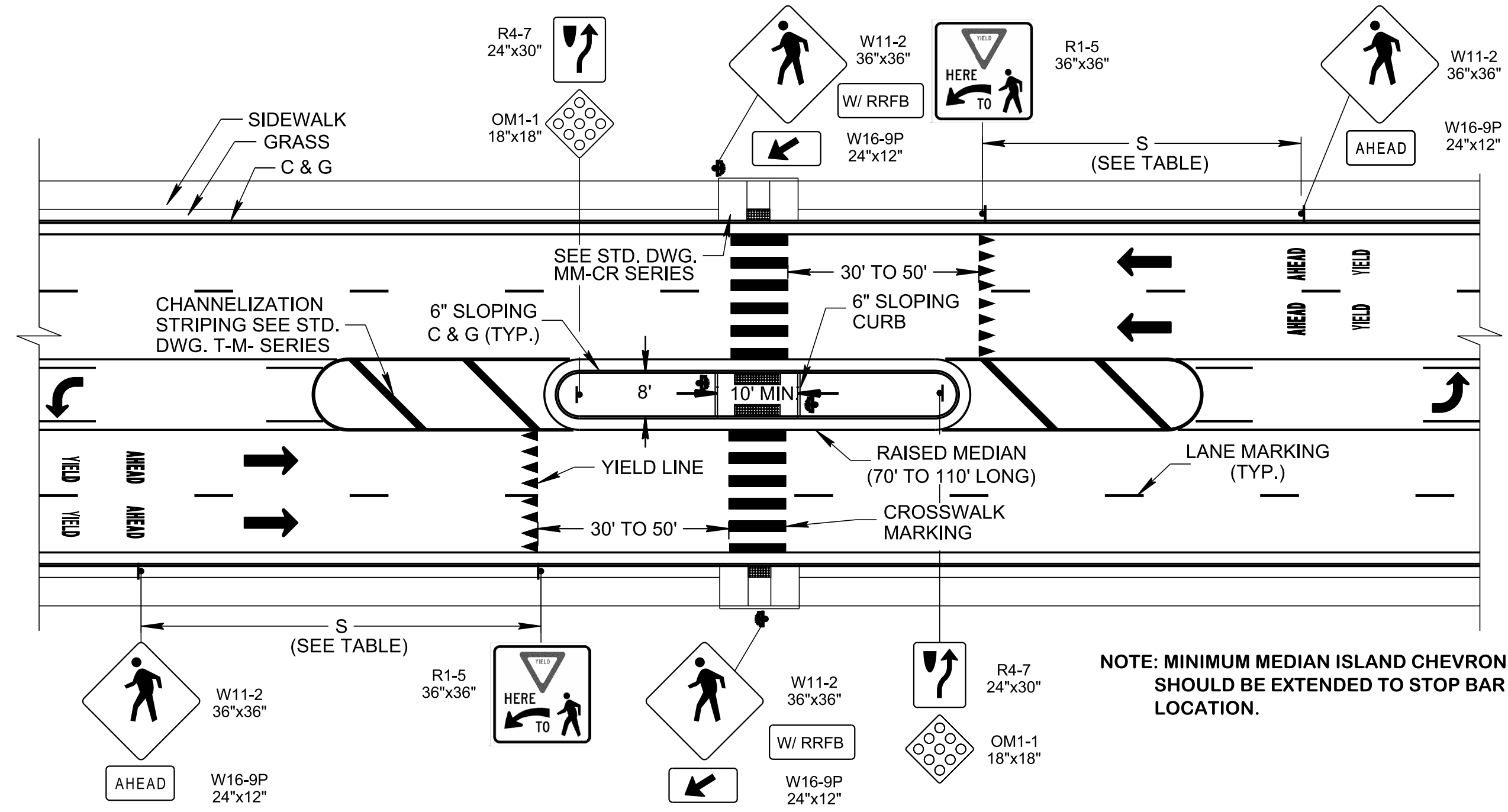


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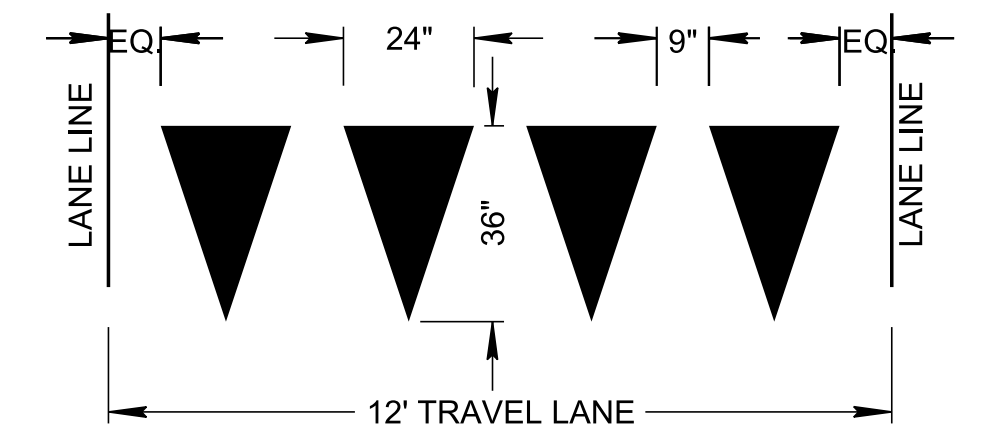
5 LANES WITH RECTANGULAR RAPID FLASHING BEACON MID-BLOCK CROSSING

POSTED SPEED	WARNING SIGNS MINIMUM ADVANCE PLACEMENT DISTANCE - S
20 MPH	100 FT
25 MPH	100 FT
30 MPH	100 FT
35 MPH	100 FT
40 MPH	125 FT

NOTE: WHERE THE SPEED LIMIT EXCEEDS 40 MPH, MARKED CROSSWALKS ALONE SHOULD NOT BE USED AT UNSIGNALIZED (NO SIGNAL) LOCATIONS.

LEGEND	
	GROUND MOUNT SIGN
	COUNTDOWN PEDESTRIAN SIGNAL HEAD WITH PUSH BUTTON AND SIGN
	DETECTABLE WARNING SURFACE

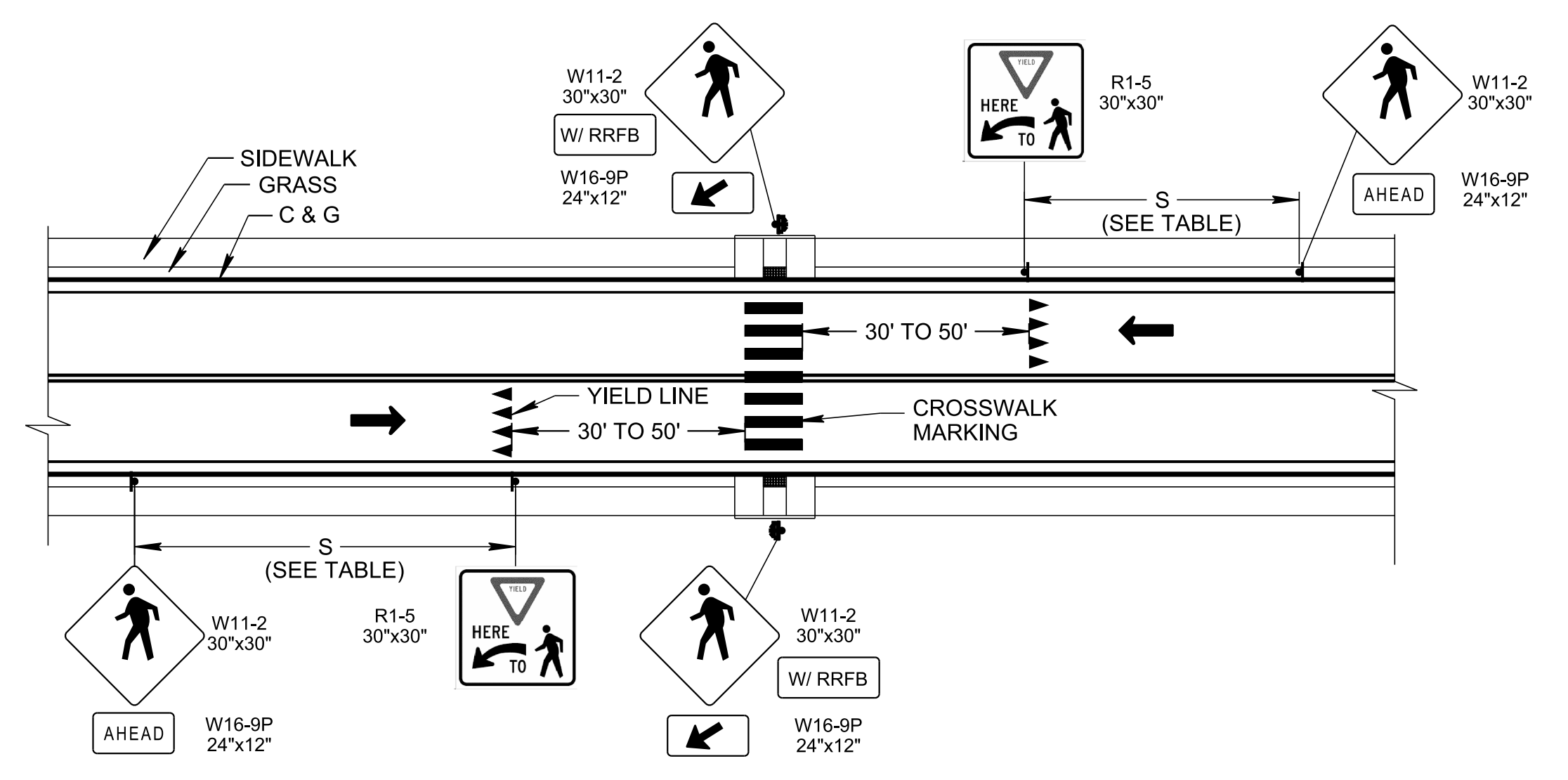
NOTE: MINIMUM MEDIAN ISLAND CHEVRON SHOULD BE EXTENDED TO STOP BAR LOCATION.



RECOMMENDED YIELD LINE LAYOUTS

NOTES: YIELD LINES MAY BE SMALLER THAN SUGGESTED WHEN INSTALLED ON MUCH NARROWER, SLOW SPEED FACILITIES SUCH AS SHARED -USE PATHS.

AREA OF EACH TRIANGLE A BASE OF 24 INCHES AND A HEIGHT OF 36 INCHES IS = 3 SF.

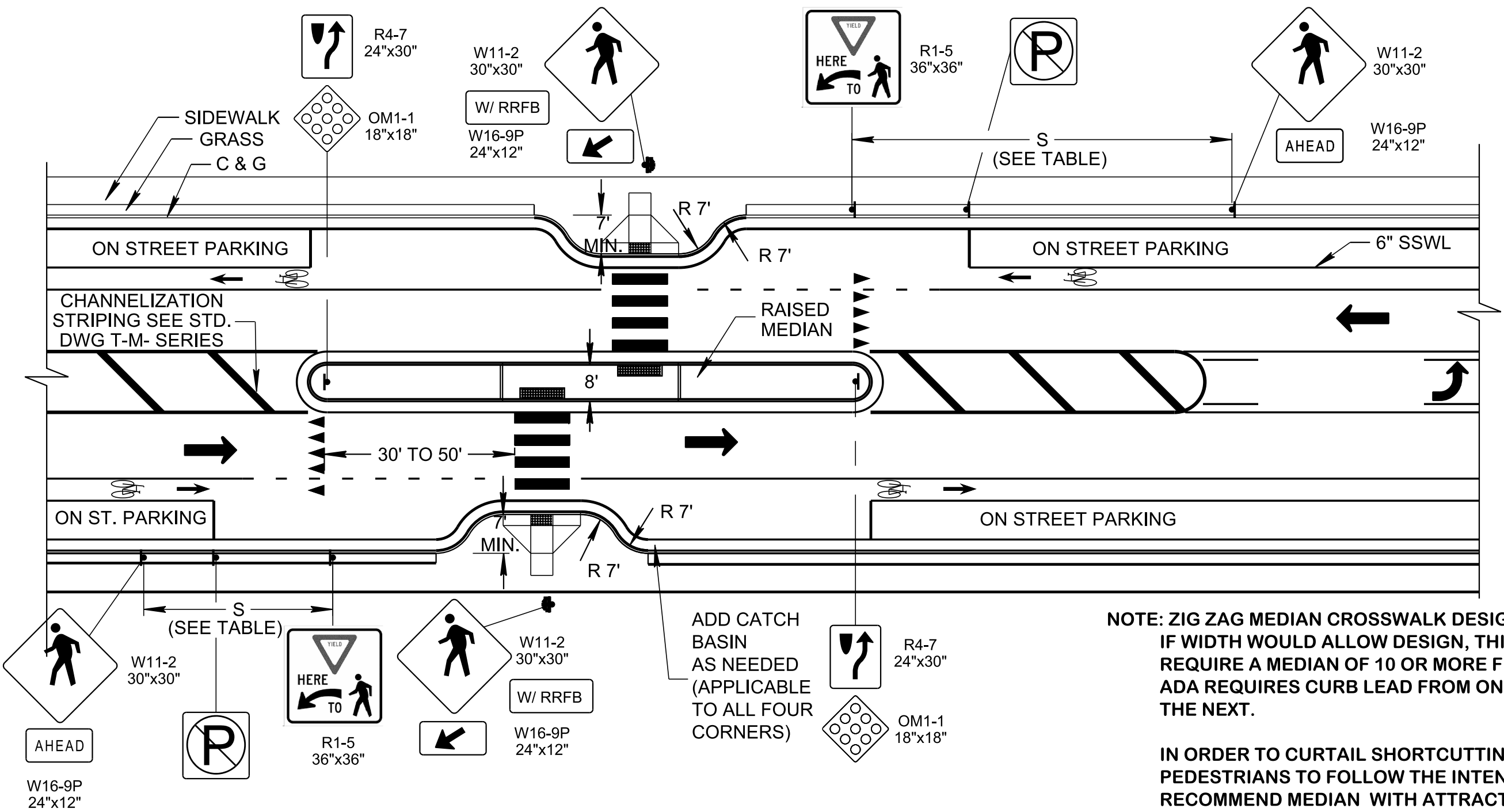


2 LANES WITH RECTANGULAR RAPID FLASHING BEACON MID-BLOCK CROSSING

GENERAL NOTES

- (A) DETAILS SHOWN ON THIS STANDARD DRAWING APPLY TO THE CONSTRUCTION OR RECONSTRUCTION OF MID-BLOCK CROSSINGS AND MODIFICATION OF STREETS, CURBS, OR SIDEWALKS ASSOCIATED WITH IT. SEE TDOT-RDG FOR ADDITIONAL INFORMATION FOR SITE SELECTION, NEW CONSTRUCTION OR RECONSTRUCTION DURING PEDESTRIAN SAFETY INITIATIVE, SPOT SAFETY IMPROVEMENTS AT LOCATIONS MAX 45 MPH. OTHER LOCATIONS WILL NEED SITE SPECIFIC ANALYSIS.
- (B) NEW CONSTRUCTION A TRAFFIC ENGINEERING STUDY WILL HAVE TO BE CONDUCTED TO DETERMINE IF A MID-BLOCK CROSSING IS WARRANTED. MID-BLOCK CROSSINGS SHALL BE INSTALLED DURING RECONSTRUCTION PROJECTS AND REPAVING PROJECTS AT LOCATIONS WHERE EXISTING PEDESTRIAN SAFETY CONCERNED.
- (C) PEDESTRIAN IN CROSSWALK SIGNS (W11A-2) SHALL BE INSTALLED AT EACH END OF THE CROSSWALK LOCATION. THE SIGNS SHALL BE PLACED IN ADVANCE OF THE CROSSWALK ADJACENT TO THE TRAVEL LANE AND FACING THE DRIVER. REFER TO THE MUTCD ADDITIONAL FOR WARNING SIGNS, TYPE AND LOCATION.
- (D) FOR CURB RAMPS, THE DETECTABLE WARNING SURFACE, PAVEMENT MARKINGS, AND CROSSWALK MARKING DETAILS, SEE STD. DWG. SERIES MM-CR AND MM-PM RESPECTIVELY. FOR MARKING STANDARDS AND CONCRETE CURB AND GUTTER SEE STD. DWG T-M- SERIES AND RP-VC SERIES RESPECTIVELY.
- (E) FOR PEDESTRIAN SIGNAL PUSH BUTTONS, HAWK, RRFB AND PHB, SEE TDOT TRAFFIC DESIGN MANUAL.
- (F) YIELD LINES SHOULD BE PLACED AT A SUFFICIENT DISTANCE (30' TO 50') FROM THE CROSSWALK TO ENSURE VISIBILITY IS PROVIDED FOR BOTH MOTORISTS AND PEDESTRIANS. YIELD LINES SHALL CONSIST OF A ROW OF SOLID WHITE ISOSCELES TRIANGLES POINTING TOWARD APPROACHING VEHICLES EXTENDING ACROSS APPROACH LANES TO INDICATE THE POINT AT WHICH THE YIELD IS INTENDED OR REQUIRED TO BE MADE. YIELD LINES CONSIST OF WHITE TRIANGLES WHICH FACE TRAFFIC. WHEN A BIKE LANE IS PRESENT, ADD ONE ADDITIONAL TRIANGLE IN THE CENTER OF BIKE LANE.
- (G) IF YIELD LINES ARE USED AT A CROSSWALK THAT CROSSES AT AN UNCONTROLLED MULTI-LANE APPROACH, YIELD HERE FOR PEDESTRIANS (R1-5 SERIES) SIGNS SHALL BE USED.
- (H) A DEVICE THAT MAY BE USED TO ASSIST PEDESTRIANS CROSSING IN A MARKED CROSSWALK AT AN UNSIGNALIZED INTERSECTION IS A RECTANGULAR RAPID FLASHING BEACON (RRFB). RRFBS ARE PARTICULARLY EFFECTIVE AT MULTILANE CROSSINGS WITH SPEED LIMITS LESS THAN 40 MPH. CONSIDER THE PEDESTRIAN HYBRID BEACON (PHB) INSTEAD OF RRFBS FOR ROADWAYS SPEED LIMITS ARE EQUAL TO OR GREATER THAN 40 MPH.
- (I) A MEDIAN SHOULD BE AT LEAST 8.0 FEET WIDE TO ALLOW THE PEDESTRIAN TO WAIT COMFORTABLY IN THE CENTER. IF THE DESIRED 8 FEET CANNOT BE ACHIEVED, USE A MINIMUM WIDTH OF 6 FEET. THE PEDESTRIAN CROSSWALK MEDIAN ISLAND ARE ADA-APPROVED RAMPS (1:12 GRADE) SHOULD BE USED. IT IS BEST TO PROVIDE A SLIGHT GRADE 2 PERCENT TO PERMIT WATER AND SILT TO DRAIN FROM THE AREA. DRAINAGE STRUCTURES SHALL NOT BE PLACED IN LINE WITH RAMPS. INSTALL CATCH BASINS ON UPSTREAM SIDE OF RAMP FOR ROADS WITH GRADES LESS THAN 2%.
- (J) PARKING AND OTHER SIGHT OBSTRUCTIONS SHOULD BE PROHIBITED FOR AT LEAST 100 FEET IN ADVANCE OF AND AT LEAST 20 FEET BEYOND THE MARKED CROSSWALK, OR SITE ACCOMMODATIONS SHOULD BE MADE THROUGH CURB EXTENSIONS OR OTHER TECHNIQUES TO PROVIDE ADEQUATE SIGHT DISTANCE. THE INSTALLATION SHOULD INCLUDE SUITABLE STANDARD SIGNS AND PAVEMENT MARKINGS.
- (K) STREETLIGHTS SHOULD BE INSTALL AT THE CROSSWALK ON BOTH SIDES ROAD TO IMPROVE PEDESTRIAN COMFORT, SECURITY, AND SAFETY DURING DARK AND BAD WEATHER CONDITIONS. FLUORESCENT YELLOW- GREEN SIGNS PROVIDE SUPERIOR VISIBILITY AND ARE EASILY NOTICEABLE IN DAYLIGHT AND DARK CONDITIONS. USE FLUORESCENT YELLOW- GREEN SIGNS FOR PEDESTRIAN AND BICYCLE WARNING AND KEEP PEDESTRIANS AND DRIVERS SAFE.
- (L) MIDBLOCK CROSSWALKS SHOULD BE LOCATED AT LEAST 100 FEET FROM THE NEAREST SIDE STREET OR DRIVEWAY SO THAT DRIVERS TURNING ONTO THE MAJOR STREET HAVE A CHANCE TO NOTICE PEDESTRIANS AND PROPERLY YIELD TO PEDESTRIANS WHO ARE CROSSING THE STREET.
- (M) PAYMENT

702-01,	CONCRETE CURB,	PER C.Y.,
702-03,	CONCRETE COMBINED CURB AND GUTTER,	PER C.Y.,
716-02.03	PLASTIC PAVEMENT MARKING (CROSS-WALK),	PER L.F.,
716-02.04,	PLASTIC PAVEMENT MARKING (CHANNELIZATION STRIPING),	PER S.F.,
716-02.05,	PLASTIC PAVEMENT MARKING (STOP LINE),	PER L.F.,
716-04.12,	PLASTIC PAVEMENT MARKING (YIELD LINE),	PER S.F.,
713-15.40,	SIGN INSTALLATION (DESCRIPTION),	PER LS,
730-26.07,	FLASHING WARNING BEACON (DESCRIPTION),	PER EACH.



2 LANES WITH RECTANGULAR RAPID FLASHING BEACON ALT. MID-BLOCK CROSSING

NOTE: ZIG ZAG MEDIAN CROSSWALK DESIGN ONLY ALLOWED IF WIDTH WOULD ALLOW DESIGN, THIS MAY REQUIRE A MEDIAN OF 10 OR MORE FEET WIDE. ADA REQUIRES CURB LEAD FROM ONE RAMP TO THE NEXT.

IN ORDER TO CURTAIL SHORTCUTTING AND FORCE PEDESTRIANS TO FOLLOW THE INTENDED PATH, RECOMMEND MEDIAN WITH ATTRACTIVE FENCING TO CORRAL PEDESTRIANS IN THE CORRECT DIRECTION.

STATE OF TENNESSEE
STANDARD DRAWING
DEPARTMENT OF TRANSPORTATION

STANDARD UNSIGNALIZED MID-BLOCK CROSSING