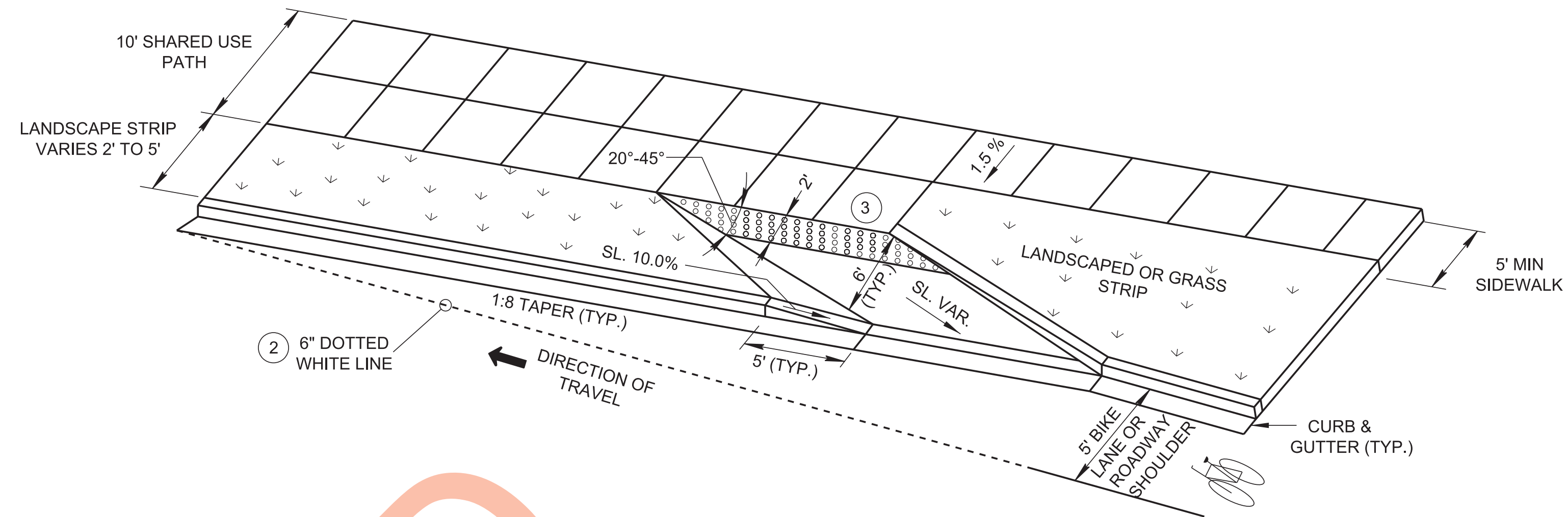
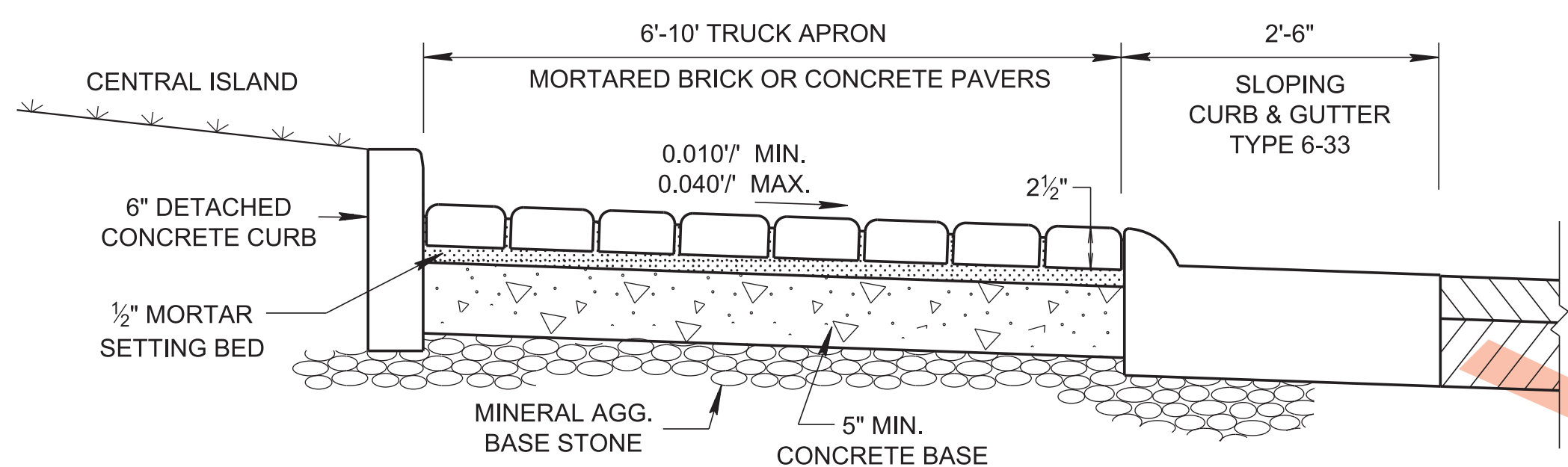


**PEDESTRIAN REFUGE AREA AT SPLITTER ISLAND**

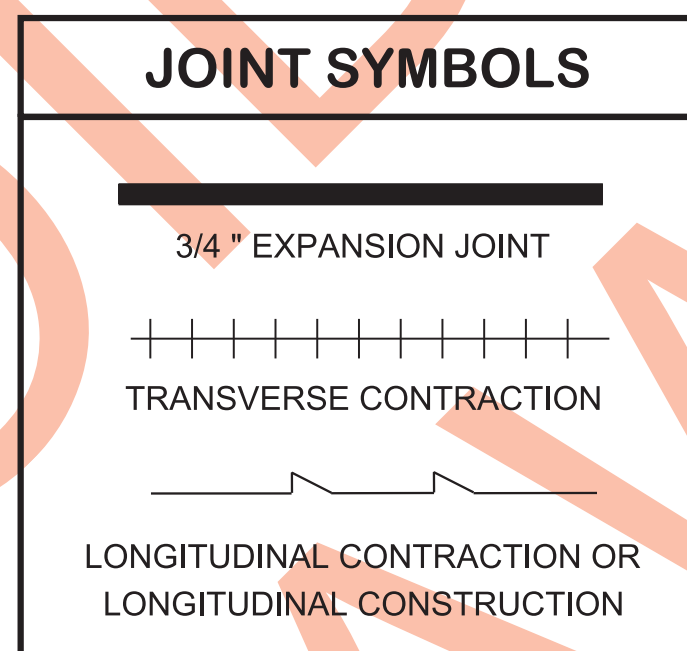


**BICYCLE EXIT RAMP DETAIL**  
(ENTRANCE RAMP SIMILAR)

**TYPICAL SPLITTER ISLAND**

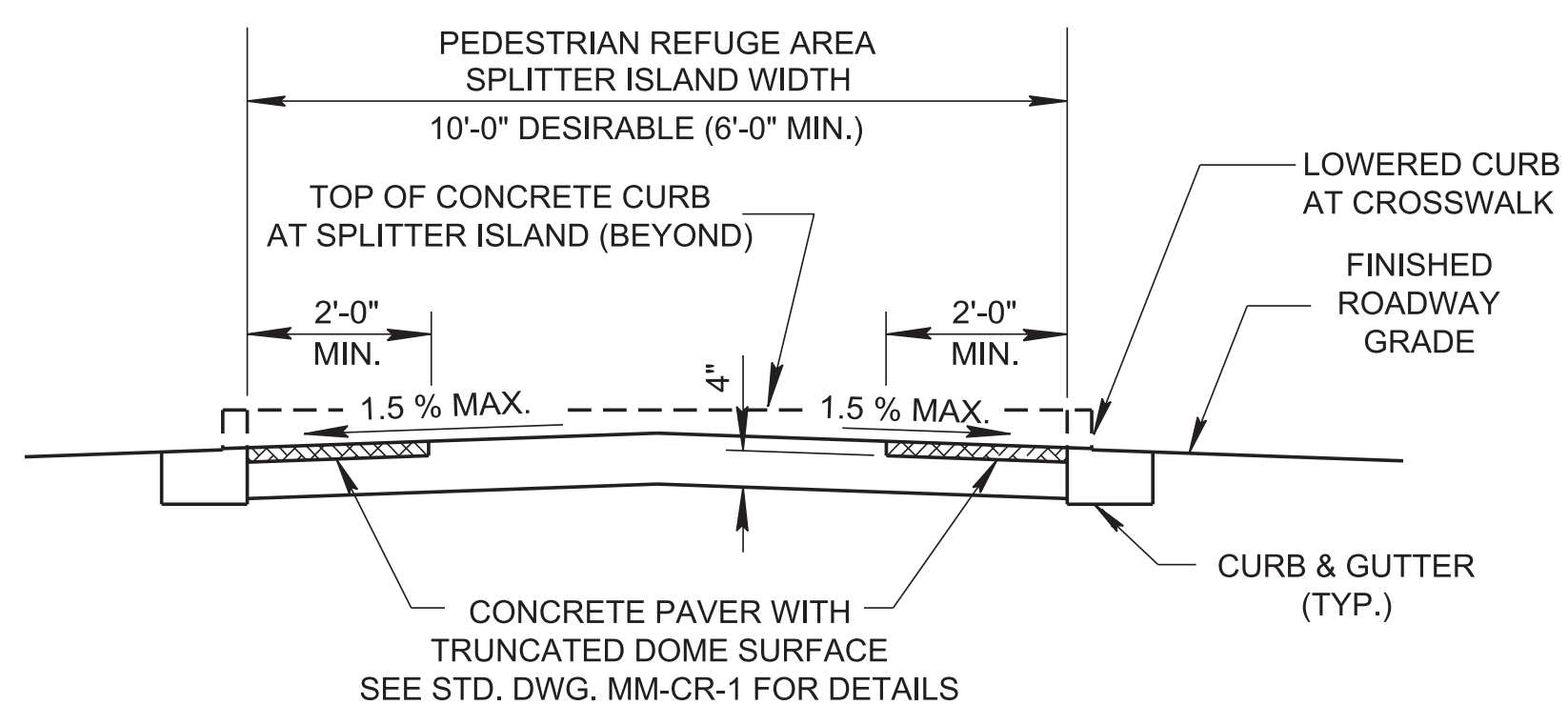


**TRUCK APRON WITH CONCRETE OR BRICK PAVERS**  
(BRICK PAVERS SHOWN)

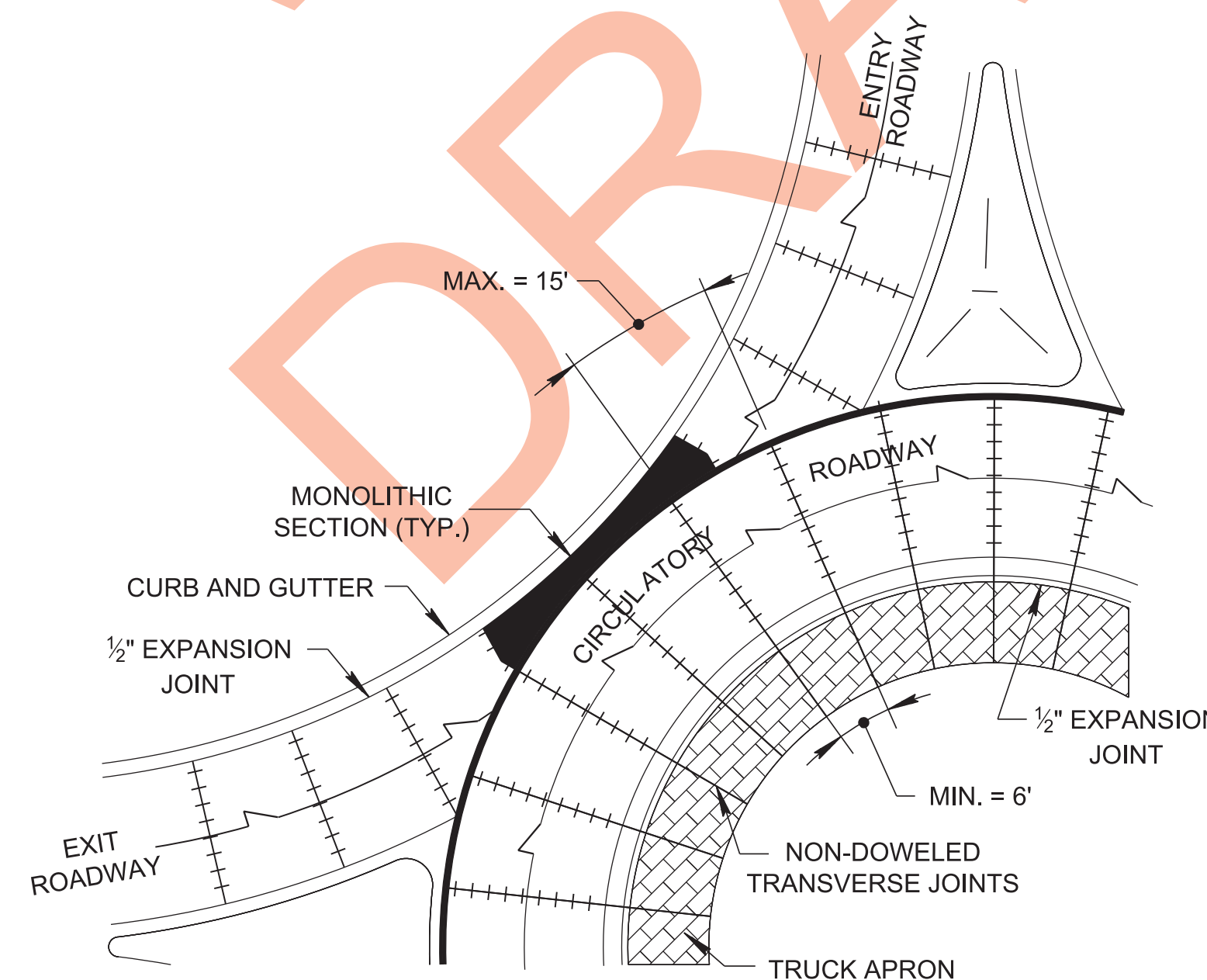


- DESIGN NOTES**
- ① SPLITTER ISLAND SHOULD BE A RAISED MEDIAN WITH CONCRETE HARDSCAPING (PREFERRED). SPLITTER ISLAND SHOULD EXTEND A MINIMUM OF 50' FROM THE YIELD LINE. SEE STD. DWG. MM-CR-4 FOR ADDITIONAL DETAILS OF MEDIAN CROSSINGS.
  - ② 6" X 2" DOTTED WHITE LINE ALONG ENTIRE LENGTH OF TAPER AT BICYCLE RAMP.
  - ③ DETECTABLE WARNING SURFACE SHALL BE INSTALLED AT BOTH SIDES OF THE SPLITTER ISLAND PEDESTRIAN REFUGE AREA AND AT THE TOP OF ALL BICYCLE EXIT AND ENTRANCE RAMPS. SEE STD. DWG. MM-CR-1 FOR DETAILS.
  - ④ WHEN CIRCULATORY ROADWAY IS CONCRETE, THE TRANSVERSE CONTRACTION JOINTS SHOULD LINE UP WITH CONTRACTION JOINTS IN THE TRUCK APRON. THE JOINTS IN THE TRUCK APRON SHOULD NOT BE DOWELED. THE COMBINED CURB AND GUTTER SHOULD BE TIED TO THE ROADWAY CONCRETE.
  - ⑤ FOR CONCRETE CURB AND CONCRETE CURB AND GUTTER DETAILS SEE STD. DWG. RP-SC-1 & RP-VC-10.
  - ⑥ FOR CONCRETE PAVEMENT DETAILS SEE STD. DWG. RP-J- SERIES.
  - ⑦ FOR PAVEMENT MARKINGS AND SIGNAGE AT BICYCLE RAMP AND SHARED USE PATH DETAILS, SEE STD. DWG. MM-PM-1.
  - ⑧ FOR TRUNCATED DOME SURFACE DETAILS SEE STD. DWG. MM-CR-1.

- GENERAL NOTES**
- (A) 6" SLOPING CONCRETE COMBINED CURB AND GUTTER SHOULD BE USED BETWEEN CIRCULATORY ROADWAY AND TRUCK APRON UNLESS OTHERWISE NOTED. 6" DETACHED CONCRETE CURB SHOULD BE USED BETWEEN THE TRUCK APRON AND THE CENTRAL ISLAND.
  - (B) THE CROSS SLOPE OF THE LANDING AREA SHALL BE 1.5% IN THE SIDEWALK AREA.
  - (C) SPLITTER ISLAND SIZE AND SHAPE WILL BE DETERMINED BY THE ROADWAY DEFLECTION.
  - (D) THE FINISH ON THE TRUCK APRON SHOULD CREATE A CONTRAST IN COLOR OR TEXTURE BETWEEN THE CIRCULATORY ROADWAY AND THE APRON. THIS CAN BE ACCOMPLISHED WITH THE USE OF CONCRETE, STAMPED CONCRETE, COLORED CONCRETE, CONCRETE PAVERS, OR BRICK PAVERS. WHEN PAVERS ARE USED, A BASKETWEAVE OR HERRINGBONE PATTERN SHOULD BE APPLIED.
  - (E) UNLESS OTHERWISE NOTED ON PLANS, THE CIRCULATORY ROADWAY SHOULD BE CONSTRUCTED OF ASPHALT. THE USE OF CONCRETE PAVEMENT SHALL BE ON A CASE-BY-CASE BASIS.
- PAYMENT:**
- (F) ALL COST FOR BICYCLE EXIT AND ENTRANCE RAMPS TO BE INCLUDED IN COST OF SIDEWALK, ITEM NO. 701-01.01, CONCRETE SIDEWALK (4"), PER SQUARE FOOT, OR ITEM NO. 701-01.02, CONCRETE SIDEWALK (6"), PER SQUARE FOOT
  - (G) ALL COST FOR CONSTRUCTING A TRUCK APRON INCLUDING MORTARED BRICK OR CONCRETE PAVERS, MORTAR SETTING BED AND CONCRETE BASE WILL BE INCLUDED IN, ITEM NO. 604-01.01, CLASS A CONCRETE (ROADWAY), PER CUBIC YARD
  - (H) PAYMENT FOR TYPE 6-33 SLOPING CURB & GUTTER AND 6" DETACHED CONCRETE CURB WILL BE AS FOLLOWS:  
ITEM NO. 702-01, CONCRETE CURB, PER CUBIC YARD  
ITEM NO. 702-03, CONCRETE COMBINED CURB AND GUTTER, PER CUBIC YARD



**SPLITTER ISLAND CROSSING SECTION A-A**



**TYPICAL JOINTING DETAIL** ④  
**FOR CONCRETE PAVEMENT**

(Replaced Std Dwg RP-R-2)

STATE OF TENNESSEE  
STANDARD DRAWING  
DEPARTMENT OF TRANSPORTATION

STANDARD CONSTRUCTION DETAILS FOR ROUNDABOUTS

06-28-2019

RP-J-26

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