

Tennessee Transportation Improvement Program



FISCAL YEARS 2020-2023

State Transportation Improvement Program

Tennessee Department of Transportation

Clay Bright, Commissioner



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Tennessee Department of Transportation

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Chapter 1: Purpose of the STIP.....	8
Linkage to TDOT’s Vision, Mission, and Guiding Principles	8
Linkage to National Planning Factors and Goals.....	8
Chapter 2: Title VI and Environmental Justice Analysis	10
<i>Minority Populations Across Tennessee Map</i>	11
Minority	11
<i>Low Income Households Across Tennessee Map</i>	12
Low Income	12
<i>Hispanic Populations Across Tennessee Map</i>	13
Hispanic.....	13
<i>Senior Populations Across Tennessee Map</i>	14
Senior	14
<i>Disabled Populations Across Tennessee Map</i>	15
Disabled.....	15
<i>Zero-Auto Households Across Tennessee Map</i>	16
No Vehicle	16
<i>Zero-Auto Households Across Tennessee Chart</i>	17
Analysis of STIP Projects	17
<i>Population/ Households Across Tennessee Chart</i>	17
<i>STIP Analysis Map</i>	18
Chapter 3: Development of the STIP	19
Stakeholder Involvement	19
Public Engagement.....	20
<i>TDOT Region/MPO/RPO Map</i>	20
STIP Revision Process	22
<i>Project Cost Change Thresholds (Table A)</i>	22
Amendments.....	22
Administrative Modifications.....	23
Chapter 4: Execution of the STIP	25
STIP Projects, Programs, and Policies	25
Revenue Inflation and Cost Estimation.....	25
Maintenance	26
Bicycle and Pedestrian	26
Advance Construction	28
Funding Programs Used in the STIP	28
<i>Highway Funding Program Descriptions</i>	29
<i>Counties Eligible for CMAQ Funding</i>	30

<i>Federal Lands Access Program (FLAP)</i>	31
<i>Tennessee Federal Lands Map</i>	31
<i>Forest Highway/Public Lands (FH/PL)</i>	32
<i>National Highway Performance Program (NHPP)</i>	33
Transit Funding Program Descriptions	39
Highway Funding Project Phases in the STIP	42
<i>Federal, State, and Local Match Requirements by Funding Program Chart</i>	43
Development of STIP Funding Levels	44
<i>Estimated Available and Estimated Costs for Fiscal Year 2020</i>	45
<i>Estimated Available and Estimated Costs for Fiscal Year 2021</i>	46
<i>Estimated Available and Estimated Costs for Fiscal Year 2022</i>	47
<i>Estimated Available and Estimated Costs for Fiscal Year 2023</i>	48
Transit Funding Project Phases in the STIP	49
<i>Transit Estimated Available and Estimated Costs for Fiscal Year 2020</i>	50
<i>Transit Estimated Available and Estimated Costs for Fiscal Year 2021</i>	51
<i>Transit Estimated Available and Estimated Costs for Fiscal Year 2022</i>	52
<i>Transit Estimated Available and Estimated Costs for Fiscal Year 2023</i>	53
Chapter 5: Performance Measures	54
Introduction	54
Federal Requirements	54
FY 2020 – FY 2023 STIP and Performance Measures	56
Safety Performance Measures (PM1)	56
<i>Safety Performance Measure Targets Chart</i>	57
Bridge and Pavement Condition Performance Measures (PM2)	57
<i>PM2 Bridge - TDOT Performance Measure Targets</i>	59
<i>PM2 Pavement - TDOT Performance Measure Targets</i>	59
System Performance Measures (PM3)	59
<i>TDOT PM3 System Performance - Performance Measure Targets</i>	61
Transit Asset Management Performance Measures and Targets	62
Introduction	62
Federal Requirements	62
<i>Rolling Stock</i>	63
<i>Equipment</i>	63
<i>Facilities</i>	63
<i>Link to Transit Asset Management Plan and Targets</i>	64
Chapter 6: Performance Goals & Targets	65
What are Performance Goals & Targets?	65
What are the National Performance Management Measures & Minimum Standards for Pavements & Bridges?.....	66
PAVEMENTS	66
<i>Table 3-1: FAST Act Pavement Metrics and Performance Ratings</i>	66

Table 3-2: FAST Act Good/Fair/Poor Determination for Interstate Pavements and Minimum Standard	67
BRIDGES	68
Table 3-3: FAST Act Components and Performance Ratings	68
Table 3-4: FAST Act Good/Fair/Poor Determination for NHS Bridges and Minimum Standard	69
What are TDOT’s Targets for the National Performance Management Measures for Pavements and Bridges?	69
Table 3-5: TDOT National Performance Management Targets	69
Table 3-6: State of Good Repair Performance Measures *(Based on square feet of bridge deck)	71
Table 3-7: TDOT State of Good Repair Targets *(Based on square feet of bridge deck)	71
Figure 3-5: Bridge Condition Rating	74
Table 3-8: Growth Rate Table	75
What is TDOT’s Predicted Bridge Condition (10 years)?	78
Figure 3-14: TDOT’s Guiding Principles for Developing the STIP	82
Figure 3-15: STIP Project Selection Prioritization Matrix Weighting	83
How Will TDOT Monitor the Performance of Pavement and Bridges?	84
Chapter 7: Rural STIP Projects	85
Region Map with County Numbers List	85
Chapter 8: Appendices	186
External Resources	186
Statewide Grouping Descriptions	187
Statewide Grouping Crosswalk	187
Public Involvement Documentation	201
TDOT’s Project Development Process (Fig. A.)	202
TDOT’s Project Development Process (Fig. B.)	203
Tennessee RPO Contacts	218
Tennessee MPO Contacts	220
TDOT Region Contacts	223
Important Links	223
Eastern Federal Lands Highway Division TIP	224

ACRONYMS:

3R	Resurfacing, Restoring, or Rehabilitating
AASHTO	American Association of State Highway and Transportation Officials
AC	Advance Construction
ADA	Americans with Disabilities Act
CFR	Code of Federal Regulations
DBE	Disadvantaged Business Enterprise
EJ	Environmental Justice
EPA	US Environmental Protection Agency
FAST Act	Fixing America's Surface Transportation Act
FHWA	Federal Highway Administration
FLMAs	Federal Land Management Agencies
FTA	Federal Transit Administration
FY	Fiscal Year
HOV	High-Occupancy Vehicle
HRRR	High Risk Rural Roads
HSIP	Highway Safety Improvement Program
IA	Improved Act
ITS	Intelligent Transportation System
MAP21	Moving Ahead for Progress in the 21st Century Act
MMS	Maintenance Management System
MPO	Metropolitan Planning Organization
NEPA	National Environmental Policy Act
RPO	Rural Planning Organization
SHSP	Strategic Highway Safety Plan
STIP	State Transportation Improvement Program
TDOT	Tennessee Department of Transportation
TIP	Transportation Improvement Plan
Uniform Act	Uniform Relocation Assistance and Real Property Acquisition Policies Act
U.S.C.	United States Code

Chapter 1: Purpose of the STIP

Linkage to TDOT's Vision, Mission, and Guiding Principles

In 2013, the Tennessee Department of Transportation (TDOT) outlined its vision – to serve the public by providing the best multimodal transportation system in the nation. To guide the Department in aligning with this vision, its seven Guiding Principles were reevaluated to ensure the promotion of priorities such as fiscal responsibility, efficiency, sustainability, safety, economic vitality, preservation of assets, and protection of resources. These Guiding Principles serve as the foundation upon which TDOT's policies, projects, and programs are based.

By federal law, the Department is tasked with developing a Long-Range Transportation Plan, which documents the changes occurring statewide as well as the needs stemming from those changes. TDOT completed an update of its plan, the 25-Year Policy Plan (<https://www.tn.gov/tdot/long-range-planning-home/25-year-transportation-policy-plan.html>), in late 2015. The 25-Year Policy Plan was developed with the intention of providing consistency between plans, programs, and policies through the linkage between the 25-Year Policy Plan and its complement, the STIP. The STIP is a “fiscally constrained” document, which means that TDOT reasonably expects sufficient funds to be available with which to implement the proposed improvements as well as to operate and maintain the entire system. As a condition to receiving federal project funds, the STIP must list all regionally significant highway and public transit transportation projects proposed for funding under Title 23 U.S.C. (highways) and Title 49 U.S.C. (transit), as well as, state and locally funded regionally significant transportation projects regardless of funding source. The STIP includes state and local roadway, bridge, bicycle, pedestrian, safety and public transportation (transit) projects. Project related activities, described later in this document, are eligible for funding through FHWA and FTA; as such, the STIP must be in place for these agencies to authorize funding for projects. The STIP is developed every three years to provide a statewide listing of transportation projects covering a period of four years. These projects are consistent with the direction ultimately laid out in the 25-Year Policy Plan as well as the metropolitan transportation plans for which TDOT intends to provide funding. The linkage between all of these elements is illustrated in the diagram below.

Linkage to National Planning Factors and Goals

The FAST Act states that projects and strategies should consider ten (10) planning factors pursuant to 23 U.S.C. Section 134 and 23 U.S.C. Section 135:

1. Support the economic vitality of the metropolitan area, especially by enabling global competitiveness, productivity, and efficiency;
2. Increase the safety of the transportation system for motorized and non-motorized users;
3. Increase the security of the transportation system for motorized and non-motorized users;
4. Increase the accessibility and mobility of people and for freight;

5. Protect and enhance the environment, promote energy conservation, improve the quality of life, and promote consistency between transportation improvements and state and local planned growth and economic development patterns;
6. Enhance the integration and connectivity of the transportation system, across and between modes, for people and freight;
7. Promote efficient system management and operation
8. Emphasize the preservation of the existing transportation system.
9. Improve the resiliency and reliability of the transportation system and reduce or mitigate storm water impacts of surface transportation;
10. Enhance travel and tourism.

The Guiding Principles identified in the Statewide Transportation Plan are in accordance with the Federal Planning Factors, and work towards achievement of the vision that came out of the Plan's public participation process. The policies and goals outlined in the Statewide Transportation Plan guide the selection of projects and programs for the STIP, and inform TDOT's planning and programming activities.

1. Preserve and Manage the Existing System - Protect existing assets and maintain efficiency of the system through cost-effective management and new technologies.
2. Support the State's Economy - Make transportation investments that support economic growth, competitiveness and tourism; build partnerships with communities and regions to link employment, commercial/retail areas and other key activity centers.
3. Maximize Safety and Security - Reduce injuries and fatalities in all modes of transportation; minimize construction-related safety incidents; improve disaster preparedness and incident response.
4. Provide for the Efficient Movement of People and Freight - Optimize the movement of people and goods by providing greater access to transportation services for all people and by building better connections among different modes of transportation.
5. Build Partnerships for Sustainable and Livable Communities - Provide early and ongoing opportunities for broad public input on plans and programs; work closely with local public and private planning efforts; coordinate land use and transportation planning.
6. Protect Natural, Cultural, and Environmental Resources - Maintain the integrity of communities and historical sites; minimize impacts on natural resources and conserve energy.
7. Emphasize Financial Responsibility - Provide accountability; maximize Tennessee's share of federal transportation funding; develop alternative funding strategies; select projects based on identified regional needs; allow flexibility in local management of projects where feasible.

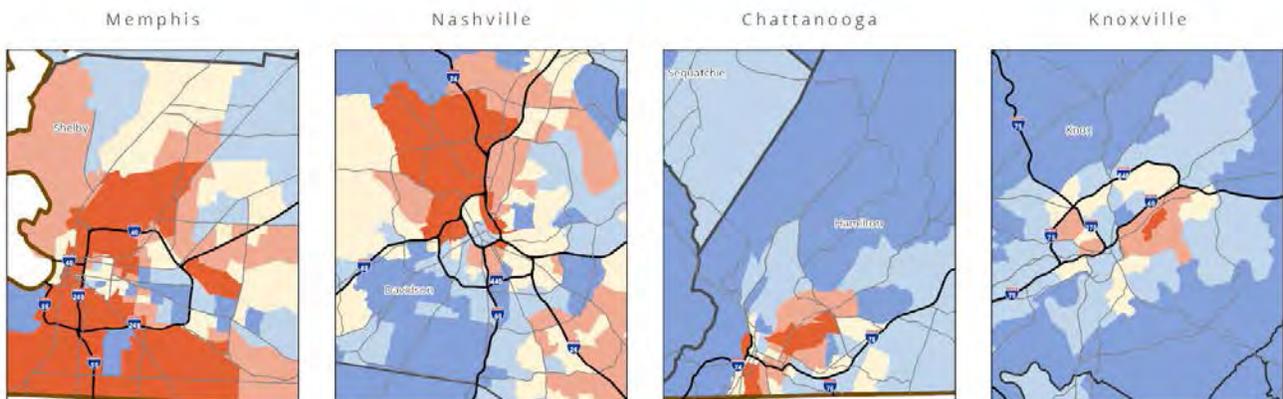
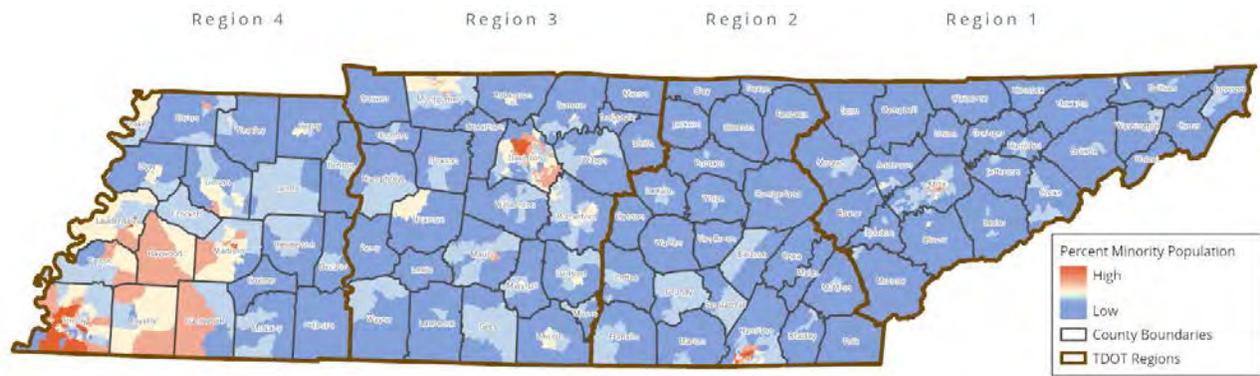
Chapter 2: Title VI and Environmental Justice Analysis

As a recipient of Federal funding, TDOT is required to conduct analysis of equitable project investments under the guidance of Executive Order 12898 (1994) and Title VI of the 1964 Civil Rights Act. Specifically, Environmental Justice relates to identifying and addressing impacts on minority and low-income populations, and Title VI relates to discrimination based on race, color, and national origin; these terms are commonly intermingled, the combination of which is generally referred to as equity. The populations associated with equity analyses are commonly referred to as ‘traditionally underserved’ populations. For TDOT, these policies collectively mean that the Department must:

- Conduct an analysis of equity related to transportation decisions, including policies, plans, programming, project development, and implementation.
- Develop and conduct public participation strategies to engage traditionally underserved populations in the planning and project development processes.
- Determine whether there are disparate impacts on traditionally underserved communities.

(Thus, assist Long Range Planning in developing the Long-Range Plan and assisting in the development of the Comprehensive Multimodal Program (3 Year Plan))

In order to examine the impacts of the STIP projects on traditionally underserved populations, block group data from the U.S. Census Bureau was analyzed at a state and TDOT Region level. Specifically, data from the latest 2013-2017 American Community Survey (ACS) 5-Year estimates were utilized for the following analyses, which include an examination of minority, low income, Hispanic, senior, and disabled populations as well as those without access to a vehicle.



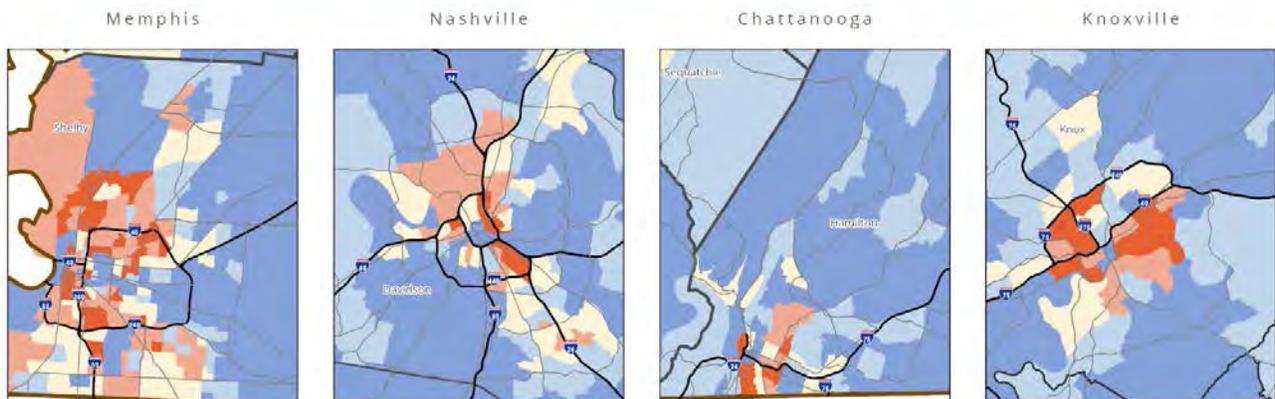
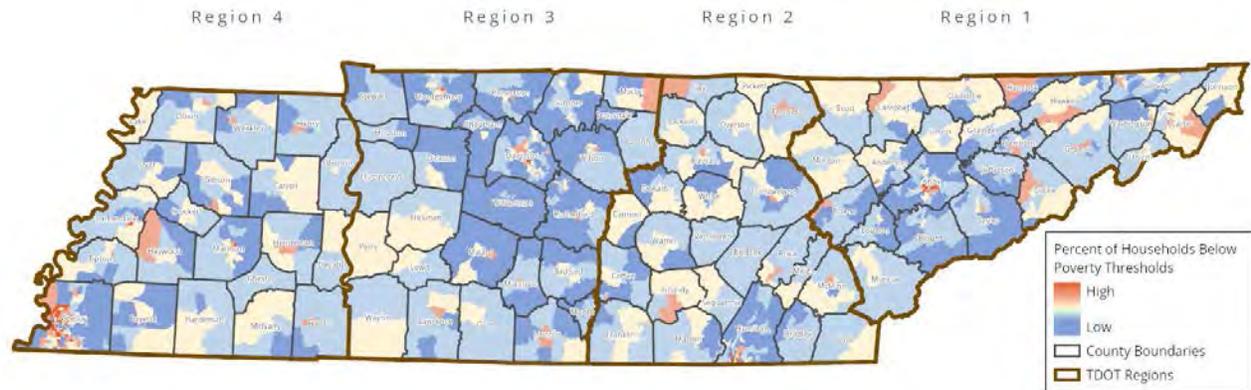
Minority Populations Across Tennessee Map

Minority

Minority populations as defined by the USDOT and FHWA include the following:

- Black (a person having origins in any of the black racial groups of Africa)
- Hispanic or Latino (a person of Mexican, Puerto Rican, Cuban, Central or South American, or other Spanish culture or origin, regardless of race);
- Asian American (a person having origins in any of the original peoples of the Far East, Southeast Asia, the Indian subcontinent);
- American Indian and Alaskan Native (a person having origins in any of the original people of North America, South America, including Central America, and who maintains cultural identification through tribal affiliation or community recognition); and
- Native Hawaiian or Other Pacific Islander (people having origins in any of the original peoples of Hawaii, Guam, Samoa, or other Pacific Islands).

Approximately 22.2% of the state's population is a minority with the majority of these populations predominately residing in the major urban areas as shown in the map above compared to the national average of 27%.

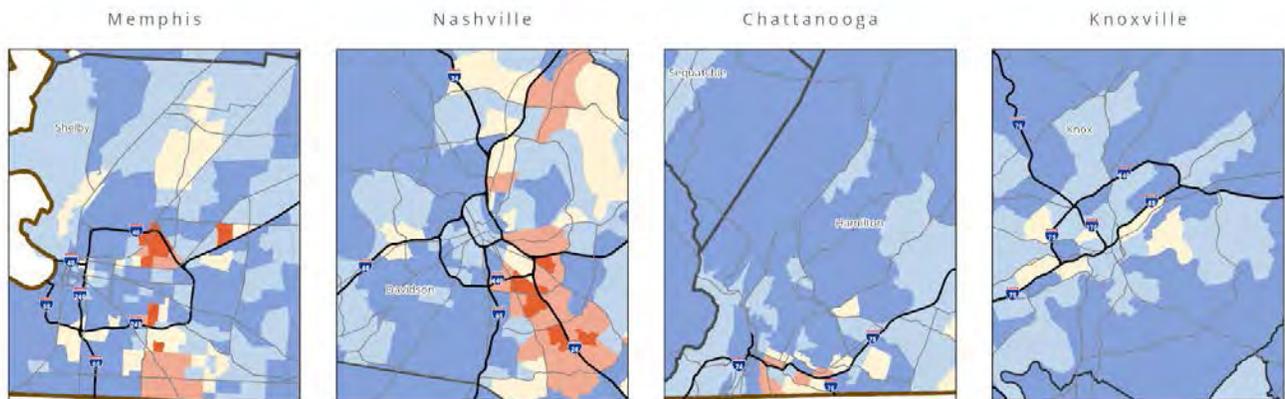
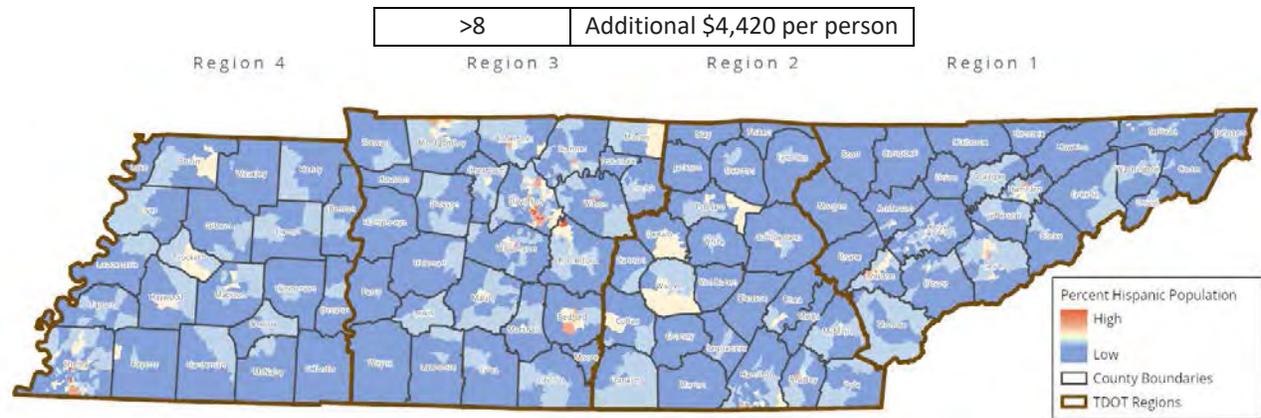


Low Income Households Across Tennessee Map

Low Income

Low income is defined by the Department of Health and Human Services (HHS) poverty guidelines, which are updated each year by the Census Bureau. For Tennessee, the poverty thresholds are shown in the table below by household size. The average household size in Tennessee is three (3) people which has a poverty threshold of \$21,330 shown in the table below. In Tennessee, approximately one-fifth (19.4%) of all households earn below the poverty threshold for a family of three compared to a national average of 16.4%. Statewide, the distribution of low income households is distributed fairly equally across urban and rural areas as seen in the map above.

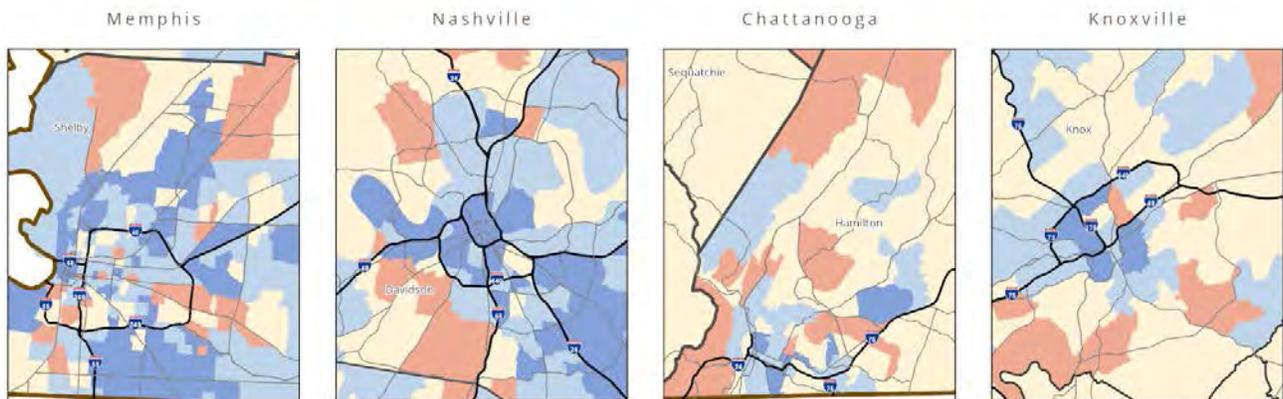
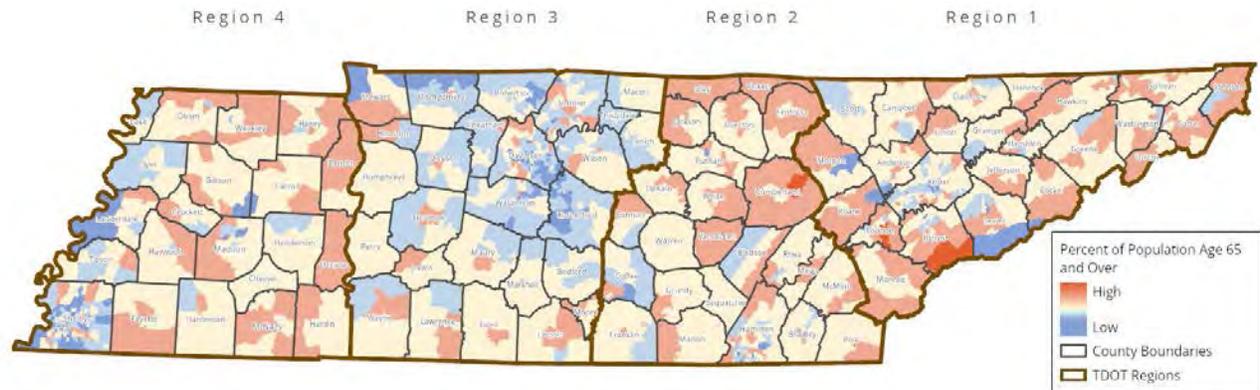
2019 HHS Poverty Guidelines	
Household Size	Poverty Threshold
1	\$12,490
2	\$16,910
3	\$21,330
4	\$25,750
5	\$30,170
6	\$34,590
7	\$39,010
8	\$43,300



Hispanic Populations Across Tennessee Map

Hispanic

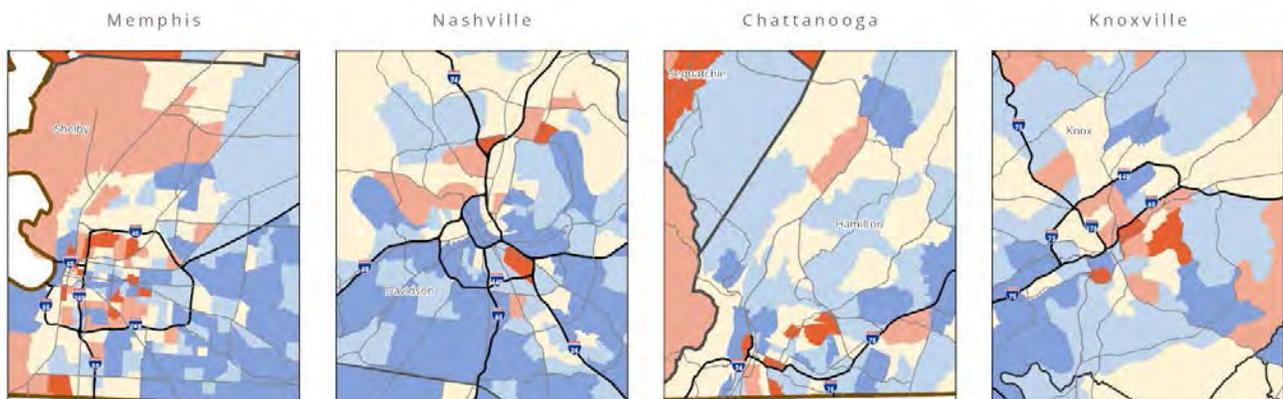
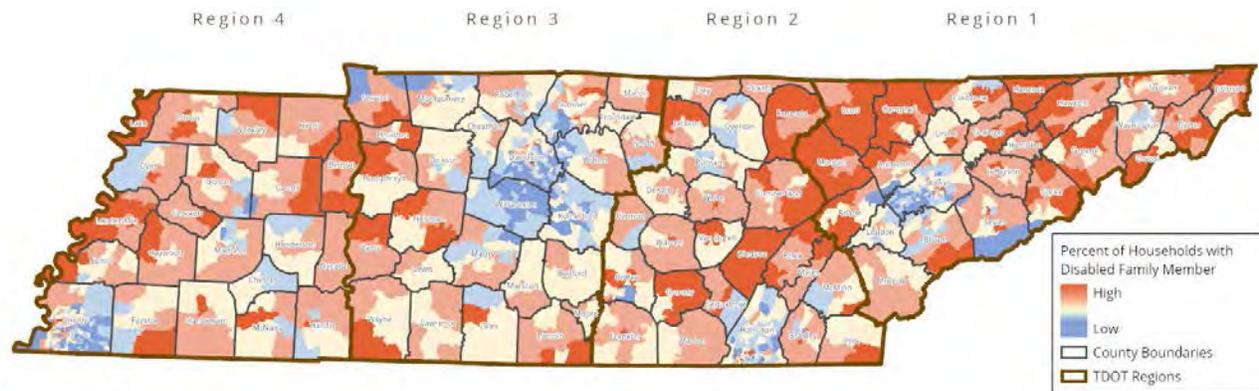
Populations with Hispanic origin are defined by the Census Bureau as those people who identify as being of Hispanic, Latino, or Spanish decent. Approximately 5.2% of the state’s population identify as being of Hispanic origin compared to a national average of 17.6%. The majority of Tennessee’s Hispanic population lies in and around the major urban areas as well as in a few rural locations as shown in the map above.



Senior Populations Across Tennessee Map

Senior

Senior populations are generally defined as people above the age of 65. Recognizing high geographical concentrations of seniors, while not called out as an Environmental Justice population, is beneficial to the identification of specific transportation needs for this group. Approximately 15.4% of Tennessee’s population is over the age of 65 compared to the national average of 14.9%, with the majority of these residents located in rural areas of the state as shown in the map above.



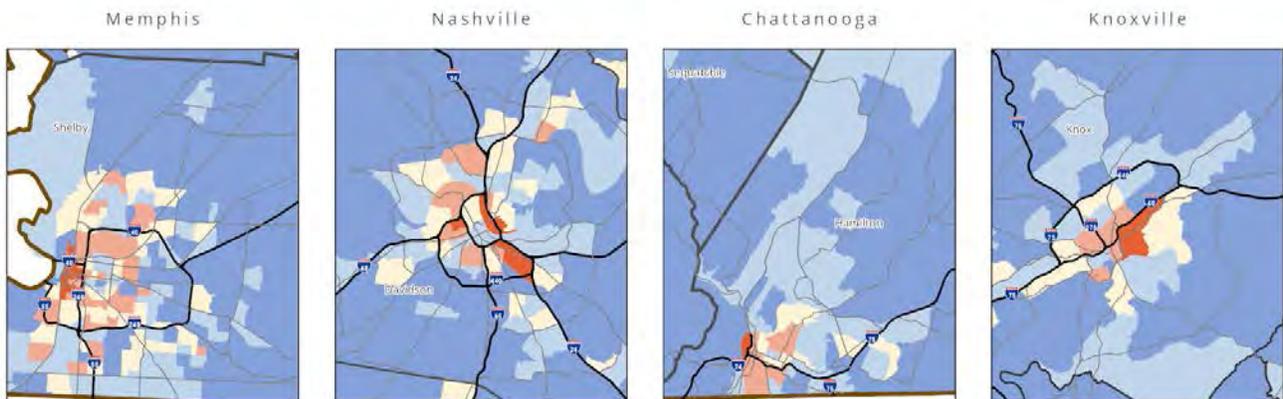
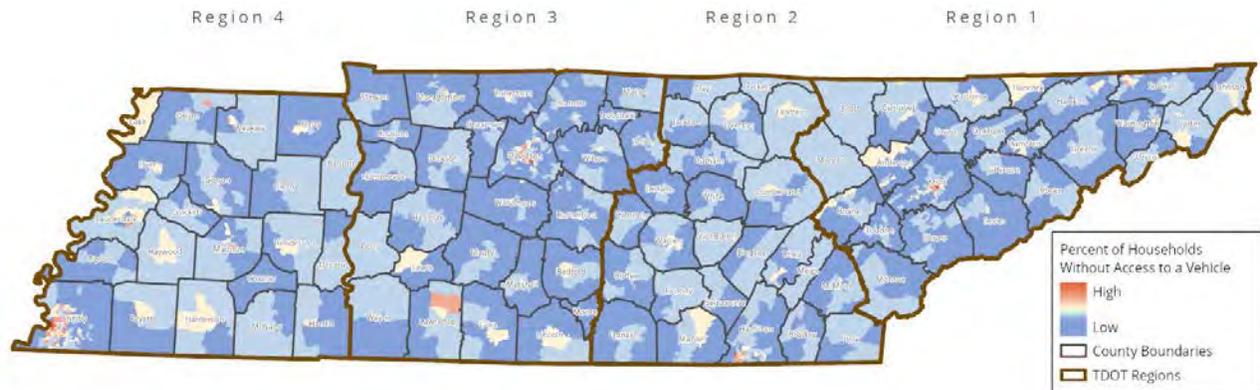
Disabled Populations Across Tennessee Map

Disabled

The ACS's definition of disability is defined by someone having any of the following:

- Hearing difficulty - deaf or having serious difficulty hearing
- Vision difficulty - blind or having serious difficulty seeing, even when wearing glasses
- Cognitive difficulty - Because of a physical, mental, or emotional problem, having difficulty remembering, concentrating, or making decisions
- Ambulatory difficulty - Having serious difficulty walking or climbing stairs
- Self-care difficulty - Having difficulty bathing or dressing
- Independent living difficulty - Because of a physical, mental, or emotional problem, having difficulty doing errands alone such as visiting a doctor's office or shopping

In Tennessee, approximately 29.8% of households report having a family member who is disabled by the above definition compared to a national average of 23.8%. As shown in the map above, these households are well dispersed across the state.



Zero-Auto Households Across Tennessee Map

No Vehicle

It is important to understand the geographic distribution of households without access to a vehicle as it can impact the need for and potential benefit of multimodal investments. Across Tennessee, approximately 6.1% of households have no access to a vehicle compared to the national average of 8.8%. The distribution of these households is pretty equal across the state with higher concentrations seen in urban areas as seen in the map below. The aforementioned statistics are shown in the table below along with companion information as it relates to each TDOT Region.

Population/Household	National Average	State Average	Region 1	Region 2	Region 3	Region 4
Minority	27%	22.2%	5.3%	9.6%	17.2%	41.8%
Low Income	16.4%	19.4%	21.3%	22.0%	14.7%	22.4%
Hispanic	17.6%	5.2%	3.5%	4.5%	6.9%	4.9%
Senior	14.9%	15.4%	18.2%	18.1%	12.7%	14.3%
Disabled	23.8%	29.8%	17.9%	17.5%	13.0%	14.8%
No Vehicle	8.8%	6.1%	5.8%	6.0%	5.0%	8.1%

Zero-Auto Households Across Tennessee Chart

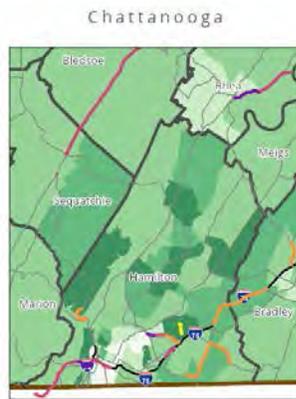
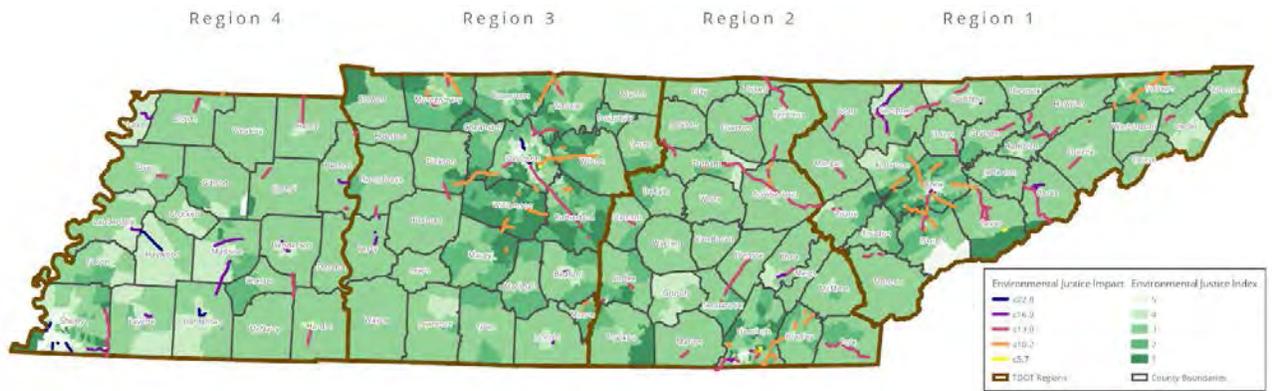
Analysis of STIP Projects

The projects included in the 2017-2020 STIP were analyzed using GIS software. This provided the ability to examine the areas where transportation investments are planned to determine the characteristics of these areas and the effects that the projects could potentially have on traditionally underserved populations. The following table shows the demographic and socioeconomic profiles for the census block groups likely to be affected by the projects contained in the 2017-2020 STIP. It should be noted that the following table relates to all TDOT-led projects located in the rural areas of the state (contained in this STIP) as well as those located in the urban areas (located in the MPO TIPs).

Population/Household	Statewide	Region 1	Region 2	Region 3	Region 4
Minority	16.9%	6.6%	8.6%	17.2%	46.0%
Low Income	17.6%	22.4%	16.6%	10.9%	22.0%
Hispanic	4.5%	3.8%	4.2%	5.2%	4.9%
Senior	14.7%	16.9%	18.7%	11.0%	14.8%
Disabled	31.2%	37.0%	35.7%	22.8%	31.5%
No Vehicle	6.1%	6.3%	4.6%	3.8%	12.2%

Population/ Households Across Tennessee Chart

The statistics shown in the table above are not necessarily correlated with the impacts of these projects on traditionally underserved populations. For instance, project details such as right-of-way acquisition, design details of roadway projects, environmental impacts, utility relocations, etc. cannot be ascertained at the STIP level. As such, an analysis of benefits and burdens associated with each project will need to be examined individually with each project as it progresses throughout the project development process.



STIP Analysis Map

Chapter 3: Development of the STIP

Stakeholder Involvement

TDOT is the principal agency in the State for development, implementation, administration, consolidation, and coordination of State transportation policies, plans, and programs. These policies, plans, and programs are developed in cooperation with the public and a variety of transportation partners through a variety of efforts. A number of factors shape the development of TDOT policies and programs. These include safety, the integration of transportation modes, service and investment preservation, customer focus, economic development, technology, environment partnerships, and federal actions. One such program that aids in the development of the STIP is the Comprehensive Multimodal Program.

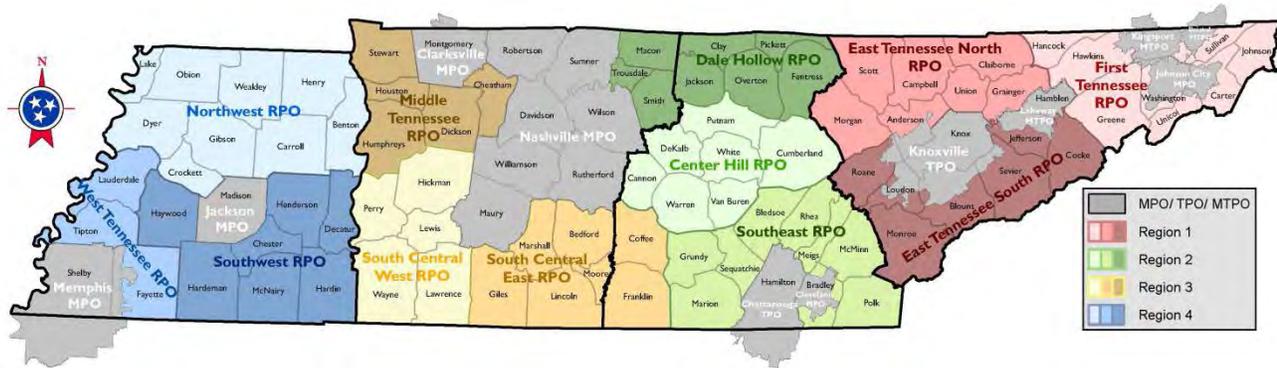
The Comprehensive Multimodal Program is developed annually to capture the most current needs from communities and organizations across the State of Tennessee. This program contains components for highway projects, bridges, transit, rail, waterway, and aviation projects, as well as statewide initiatives in support of the Department's budget. The process for programming capital and non-capital transportation projects include a data driven method in which each of the performance measures (Safety, Pavement Condition, Bridge Condition, NHS Travel Time Reliability, Freight Reliability, Traffic Congestion and Total Emissions Reduction) is considered. Each project and/or program endures a stringent vetting process, not only through our data software (TD-1), but through each local municipality, Rural Planning Organization, Metropolitan Planning Organization and Regional Leadership. These investments are critical in providing a safe and reliable transportation network for our citizens and for increasing economic development opportunities across the state.

The STIP is a focal point for implementing strategic goals from both the federal and state levels, which pursue the desire for improving our transportation network while preserving the livability of our communities. Within TDOT, the Program Development and Administration Division is tasked with the development of the STIP. The STIP is developed through a consultation process between TDOT headquarters, TDOT Region offices, the MPOs, RPOs, various interested parties, and FLMAs.

MPOs, comprised of various urban local officials, have a structure to identify and prioritize proposed transportation improvements. MPOs have federal funds available to them for transportation improvements and a process for developing a TIP. Priority lists from each MPO for state funded projects are submitted to TDOT for consideration during the STIP preparation process. Each MPO is provided a copy of the public comment notification and is encouraged to solicit input from their organization members. TIPs, once adopted by the respective MPO, are forwarded to TDOT for inclusion, by reference into the STIP. Each MPO has an adopted public involvement plan, which it uses to develop its TIP. The 11 MPOs in Tennessee are shown in the figure below with contact information for each provided in the appendices. It should be noted that projects selected by TDOT which fall within a MPO boundary are not listed in this document, but rather are listed within the TIP for the respective MPO where the project is located.

The RPOs, comprised of various rural local officials, also have a structure to identify and prioritize proposed transportation improvements. While MPOs receive an allocation of federal funds, the RPOs do not receive a direct allocation. Each RPO receives a copy of the Draft STIP, along with the public comment notification, and is encouraged to solicit input from their organization members. TDOT will respond to each comment. The 12 RPOs in Tennessee are shown in the figure below with contact information for each provided in the appendices.

Public Engagement



TDOT Region/MPO/RPO Map

Interested parties for the STIP include citizens, affected public agencies, representatives of public transportation employees, public ports, freight shippers, providers of freight transportation services, private providers of transportation (including intercity bus operators, employer-based commuting programs, such as a carpool program, vanpool program, transit benefit program, parking cash-out program, shuttle program, or telework program), representatives of users of public transportation, representatives of users of pedestrian walkways and bicycle transportation facilities, representatives of the disabled, and other interested parties. Many of these stakeholders are already involved in TDOT’s STIP process through their participation in MPO and RPO activities as well as the State’s Freight Advisory Committees (FACs). Additionally, various individuals and representative groups of these stakeholders meet personally with, or write letters to or email the Department throughout the year to offer input to the Department’s transportation priorities. Comments received from these are considered during the STIP development process. Written comments to the Department, along with the Department’s response, are archived by TDOT’s Community Relations Division. To solicit comments on the STIP from citizens, an advertisement providing the opportunity to comment is placed in newspapers statewide.

As part of the development and publication of the STIP, TDOT complies with Executive Order 12898, Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations along with the Department of Transportation Order 5610.2(a), which defines three fundamental principles:

- To avoid, minimize, and/or mitigate disproportionately high and adverse human health and environmental effects, including social and economic effects on minority and low-income populations.

- To ensure the full and fair participation by all potentially affected communities in the transportation decision-making process.
- To prevent the denial of, reduction in, or significant delay in the receipt of benefits by minority and low-income populations.

TDOT has established a process to ensure the appropriate application of these principles in the decision-making process. Project selection for the STIP is guided by the policies and programs outlined in the 25-Year Long-Range Transportation Plan. As part of the 25-Year Plan development, the Department determined and implemented interactive engagement efforts that were used specifically to reach traditionally underserved populations. TDOT defines these traditionally underserved populations to include these citizens:

- Environmental justice populations (minority and low income populations)
- Elderly
- Disabled
- Public Transportation dependent (those listed by the Census as “occupied units with no vehicle”)
- Low literacy (those with Level 1 literacy)
- Those with limited English proficiency

This process ensured that all populations were engaged in determining the policy direction of the 25-Year Plan, and thereby, the projects programmed in the STIP. Similar to the 25-Year Plan, the Draft STIP is accessible to the public and local officials through various resources. TDOT advertised the availability of the Draft STIP for public comment statewide through newspapers in major cities as well as various Minority and Hispanic newspapers throughout the state. Specifically, TDOT advertised the Draft STIP for public comment statewide through the following major newspapers: The Knoxville News-Sentinel, Chattanooga Times- Free Press, The Tennessean, The Commercial Appeal, The Kingsport Times-News, The Daily News Journal, and The Leaf-Chronicle. Advertisement was also included in the following minority papers: The Knoxville Enlightener, The Chattanooga Courier, Nashville Pride, Tri-State Defender, Murfreesboro Vision, El Crucero de Tennessee, and The Clarksville Press. This notice specified locations, dates, and times the document could be reviewed, as well as a contact person, mailing address, and phone number. TDOT provided copies at the TDOT Region offices (location and contact information provided later in this document). Comment cards were provided at these locations to facilitate participation in the process. The public comment period is stipulated as a minimum of 30 calendar days and was held from August 1, 2019 to September 1, 2019.

TDOT staff delivered the Draft STIP to the TDOT Region Offices and provided training to appropriate staff in each location. During the Public Comment period, TDOT staff held Public Meetings at designated places and times to further engage the public and answer questions concerning the Draft STIP. At these Public Meetings, TDOT used visualization techniques such as graphs and charts to enhance the public’s understanding of the Draft STIP document. These meetings were appropriately advertised. Elements of this public engagement process are included in the Appendix, which includes participation lists for the public meetings and examples of the displays used in the public meetings.

The Draft STIP is also accessible via TDOT’s web page <https://www.tn.gov/tdot/program-development-and-administration-home/program-development-and-administration-state-programs.html> . Lists of TDOT-led projects in the MPO areas are distributed to those organizations for inclusion in the respective TIPs and for public review and comment through the MPO’s public involvement process. A list defining each MPO Coordinator and contact information is included within this document. The Department will respond to all comments individually and collectively. Comments are summarized, along with their disposition, in the Appendices included in the Final STIP. If the Department receives a comment(s) that is deemed a viable change, said revision will follow the procedures established for an amendment or an administrative modification to the STIP. A more detailed discussion of the STIP/TIP Amendment/Administrative Modification procedures is described below.

STIP Revision Process

Occasionally, revisions will need to be made to the STIP as elements of a project can change for a variety of reasons. The type of change determines what type of revisions is necessary – an amendment or administrative modification. For changes to the cost of projects (excluding groupings), a sliding scale, outlined in the table below, is used to determine which category of revision is required. All measurements for these cost changes are made from the last approved STIP or STIP amendment/administrative modification to account for incremental changes.

Project Cost Change Thresholds (Table A)

Total project cost of all phases shown within the approved STIP	Amendment	Administrative Modification
Up to \$2 million	> 75%	< 75%
\$2 million to \$15 million	> 50%	< 50%
\$15 million to \$75 million	> 40%	< 40%
\$75 million and above	> 30%	< 30%

Amendments

An amendment is a revision to the STIP that involves major changes to a project or the overall program and must meet the requirements of 23 CFR 450.216 and 450.326 regarding public review and comment, re-demonstration of fiscal constraint, and transportation conformity. Amendments may be made to the STIP at any time during the life of the STIP and require federal approval by FHWA or FTA. An amendment is required when changes to the STIP include:

- A major change in the total project cost (excluding groupings) (see discussion on project cost change thresholds with Table A); or
- Adding a new project or deleting a project from the STIP/TIP; or
- A major change of project scope; examples include, but are not limited to, changing the number of through-lanes, adding/deleting non-motorized facilities, changing mode (e.g., rolling stock or facility type for transit), changing capital category (i.e., transit funding), or changing termini; or
- Any change requiring a new regional air quality conformity finding (including a grouping); or
- Moving funds between a Metropolitan Planning Organization (MPO) TIP and STIP unless a written agreement exists between the MPO and the Tennessee Department of Transportation (TDOT) that such an action may be processed as an administrative modification; or
- Moving funds between an MPO's TIP and another MPO's TIP unless a written agreement exists between each MPO and TDOT that such an action may be processed as an administrative modification.

Amendments to the approved STIP are posted on the Department's web page for review and a 15 calendar day public comment period. The public comment period is advertised in a local newspaper for county specific amendments and in various regional newspapers for statewide/region amendments. The advertisement directs respondents to the Department's web page and provides the name and contact information for appropriate Department personnel. Additionally, notification of an amendment is sent to agencies or interested parties along with the federal land management agencies. There is no requirement for a Public Meeting on amendments. The Department responds to all comments individually and collectively. If the Department receives a comment(s) on an amendment that is deemed a viable change, said revision will again follow this amendment procedure. Amendments will remain on the Department's web page for the life of the STIP.

Administrative Modifications

A STIP administrative modification is a minor change from the approved STIP. Administrative modifications must be consistent with 23 CFR 450, but they do not require public review and comment, or a conformity determination in non-attainment or maintenance areas. Administrative modifications are defined as follows:

- A minor change in the total project cost (see Table A)
- A minor change in project description that does not change the air quality conformity finding in maintenance and/or non-attainment areas; or
- A minor change in project description/termini that is for clarification and does not change the project scope such as a length change for reasonable transition purposes or to correct minor clerical errors or discrepancies; or
- Shifting funds between projects or groupings within the STIP/TIP (i.e., funding sources and projects already identified in the STIP/TIP) if the change does not result in a cost increase greater than the amendment threshold (see Table A) for the total project cost of all phases shown within the approved STIP/TIP; or

- Adding an amount of funds already identified in the STIP/TIP for the current or previous year(s) if:
 - The funds are currently identified in the STIP/TIP either in an existing project or as available funds and
 - The change does not result in a cost increase greater than the amendment threshold (project cost change thresholds listed in Table A) for the total project cost of all phases shown within the approved STIP/TIP; or
- Moving project phases or funding from year to year within an approved STIP/TIP, except those that cross air quality horizon years of the project; or
- Adding any phase (if total project cost includes all phases), such as environmental or location study, preliminary engineering, right-of-way, or construction to a project in the STIP/TIP so long as such a change does not result in a cost increase greater than the amendment threshold (see Table A) for the total project cost of all phases shown within the approved/amended STIP/TIP; or
- Changes required to follow FHWA or FTA instructions as to the withdrawal of funds or re-establishment of funds withdrawn at the request of FHWA or FTA; or
- Moving funds between similarly labeled groupings, regardless of percent of change; or
- Adjustments in revenue to match actual revenue receipts; or
- Adding a project with 100% state or non-federal funding for all phases that does not change the air quality conformity finding in maintenance and/or non-attainment areas; or
- Adding a funding source as long as the change does not result in a cost increase greater than the amendment threshold (see Table A);

Administrative modifications may be made to the STIP at any time during the life of the STIP and do not require federal approval by FHWA or FTA, but will be submitted to FHWA and FTA for informational purposes prior to requesting federal authorization of the affected project.

Chapter 4: Execution of the STIP

The STIP is fiscally constrained and includes project phases that have a reasonable expectation of being ready for implementation by the year listed. Projects are subject to many considerations and actions from conception to completion that may impede or accelerate their progress. These considerations may include policy decisions; changes in design requirements; conflicts with other scheduled activities; and unforeseen circumstances such as cutbacks in funding, shortage of manpower, and inflation of project costs. Project cost estimates are based on best available engineering estimates at the time the STIP is developed, and projects listed beyond 2020 are inflated to year of expenditure dollars. Final cost for the actual projects may differ, as the projects are refined in the project development process prior to construction. When a project is adversely affected by any of the above-mentioned factors, the projected fiscal year dates will be modified accordingly.

STIP Projects, Programs, and Policies

Major transportation improvement projects have been identified in the STIP for funding authorization for the various federal highway and transit programs. Projects with small-scale impacts are those which do not induce significant impacts to planned growth or land use for the area; do not require the relocation of significant numbers of people; do not have a significant impact on any natural, cultural, recreational, historic or other resource; do not involve significant air, noise, or water quality impacts; do not have significant impacts on travel patterns; or do not otherwise, either individually or cumulatively, have any significant environmental impacts, are air-quality neutral, and non-regionally significant. These projects with small-scale impacts, (e.g., pavement resurfacing, bridge replacement and rehabilitation, seismic retrofit, bridge inspection, freeway service patrols, rail/highway grade crossings, hazard elimination, lighting, signalization, etc.) as well as multimodal projects are grouped into statewide groupings. These groupings are explained in more detail in the Appendices of this document.

Revenue Inflation and Cost Estimation

The FAST Act is the current 5-year federal Surface Transportation legislation that authorizes \$305 billion over fiscal years 2016 through 2020 for highway, highway and motor vehicle safety, public transportation, motor carrier safety, hazardous materials safety, rail, and research, technology, and statistics programs. With the FAST Act expiring in 2020 and the uncertainty of a future Transportation Bill, the Tennessee Department of Transportation (TDOT) projected a modest funding growth of 5%. This growth rate was calculated utilizing the existing funding distribution and assumptions from when funds were increased from MAP 21 (the previous Surface Transportation Act) to the current FAST Act. The 5% is a conservative increase that may be adjusted moving forward.

Due to the limited funding availability, TDOT has modified its cost estimating methodology. This methodology begins in the early planning phase. When a TIR (Transportation Investment Report) is developed for any transportation project, TDOT develops cost projections with the knowledge that the average transportation project takes 5-7 years to get to construction. Planning documents now include today's cost, a 5-year projection, and a 10-year projected cost. These new projections will assist the Department in getting a handle on future funding needs. Additionally, the Programming and Administration Division adds 10%-15% to the estimates for contingencies - 10% for >\$15.0 million and 15% for <\$15.0 million.

TDOT considers several factors when developing estimated revenues in the STIP. The Department starts with actual revenue from the current Transportation Bill, then adds the estimated cost for phases of projects that have not been obligated. This increases the estimated revenue to cover all previously programmed projects (projects that were budgeted in a previous or existing STIP, but funds were not authorized). It is vital to include these unobligated project phases, as the Department is working out of an active STIP while developing a new Draft STIP. This duplication of projects provides the Department flexibility through this planning and programming document.

Maintenance

Maintenance activities are those that occur primarily in reaction to situations that have an immediate or imminent adverse impact on the safety or availability of transportation facilities such as snow removal, pavement resurfacing and marking, bridge repair, guardrail and sign replacement, etc. While maintenance activities are not funded through or scheduled in the STIP, they are included here for informational purposes. The Tennessee General Assembly appropriates approximately \$360,000,000 annually in state funds for maintenance and operation of the Interstate and State Route Systems, which is based on calculations of existing needs through the Department's Maintenance Management System (MMS). Actual figures for fiscal years 2021, 2022, and 2023 will be determined and submitted to the Legislature in future budget requests. Federal funding for resurfacing, restoring, or rehabilitating (3R) type projects on the Interstate and State Route Systems are listed in this document. The maintenance activities for facilities under the jurisdiction of local governments are the responsibility of each entity. Each local entity represented in this document appropriates adequate funding in their respective budgets to operate and maintain facilities under their jurisdiction.

Bicycle and Pedestrian

TDOT has a Multimodal Access policy, adopted on July 31, 2015, designed to encourage safe access and mobility for roadway users of all ages and abilities. TDOT is committed to the development of a transportation system that improves conditions for bicycling and walking through the following actions:

- 1) Provisions for multimodal transportation shall be given full consideration in new construction, reconstruction, and retrofit roadway projects through design features appropriate for the context and function of the transportation facility.
- 2) The planning, design, and construction of new facilities shall give full consideration to likely future demand for multimodal facilities and not preclude the provision of future improvements. If all feasible roadway alternatives have been explored and suitable multimodal facilities cannot be provided within the existing or proposed right of way due to economic or environmental constraints, an alternate route that provides continuity and enhances the safety and accessibility of multimodal travel shall be considered.
- 3) Existing multimodal provisions along roadways shall not be made more difficult or impossible by roadway improvements or routine maintenance projects.
- 4) Intersections and interchanges shall be designed (where appropriate based on context) to accommodate the mobility of bicyclists and pedestrians to cross corridors as well as travel along them in a manner that is safe, accessible, and convenient.
- 5) While it is not the intent of resurfacing projects to expand existing facilities, opportunities to provide or enhance bicycle and pedestrian facilities shall be given full consideration during the program development stage of resurfacing projects.
- 6) Pedestrian facilities shall be designed to accommodate persons with disabilities in accordance with the access standards required by the Americans with Disabilities Act (ADA). Sidewalks, shared use paths, street crossings (including over- and under-crossings) and other infrastructure shall be constructed so that all pedestrians, including those with disabilities, can travel independently.
- 7) Provisions for transit riders, pedestrians, and bicyclist shall be considered when closing roads, bridges or sidewalks for construction projects where pedestrian, bicycle, or transit traffic is documented or expected.

There are conditions where it is generally inappropriate to provide multimodal facilities. These conditions include, but are not limited to:

- 1) Facilities where non-motorized users are prohibited from using the roadway. In this instance, a greater effort may be necessary to accommodate these users elsewhere within the same transportation corridor.
- 2) The cost of accommodations would be excessively disproportionate to the need and probable use. Excessively disproportionate is defined as exceeding twenty percent (20%) of the cost of the project. The twenty percent figure should be used in an advisory rather than an absolute sense, especially in instances where the cost may be difficult to quantify. Compliance with ADA requirements may require greater than 20% of project cost to accommodate multimodal access. Costs associated with ADA requirements are NOT an exception.
- 3) Areas in which the population and employment densities or level of transit service around the facility, both existing and future, does not justify the incorporation for multimodal alternatives.
- 4) Inability to negotiate and enter into an agreement with a local government to assume the operational and maintenance responsibility of the facility.

- 5) Other factors where there is a demonstrated absence of need or prudence, or as requested by the Commissioner of the Department of Transportation.

Exceptions for not accommodating bicyclists and pedestrians in accordance with this policy will be documented describing the basis for the exception. For exceptions on Federal-aid highway projects, concurrence from FHWA must be obtained.

Pedestrian, Bicycle, and Shared Use Path design standards have been developed by TDOT and approved by FHWA. These design standards should be followed by local, regional and state agencies when considering including bicycle and pedestrian features on an existing facility or new construction.

Advance Construction

Advance Construction (AC) is a technique which allows a State to initiate a project using non-federal funds while preserving eligibility for future Federal-Aid funds. Eligibility means that FHWA has determined that the project technically qualifies for Federal- Aid; however, no present or future Federal funds are committed to the project. After an AC project is authorized, the state may convert the project to regular Federal-Aid funding provided Federal funds are made available for the project.

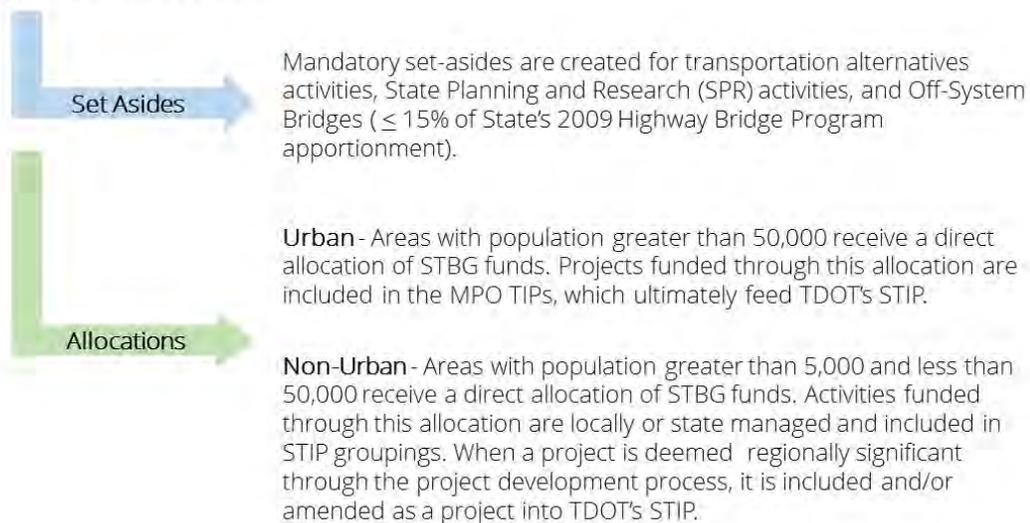
An AC project must meet the same requirements and be processed in the same manner as a regular Federal-Aid project. All phases of a project must meet federal requirements for the National Environmental Policy Act (NEPA), Uniform Relocation Assistance and Real Property Acquisition Policies Act (Uniform Act), etc. when any phase is implemented with Federal-Aid funds. All projects advanced as an AC project will have 'AC' preceding the funding code (e.g., ACNHPP, ACSTBG, etc.).

Funding Programs Used in the STIP

At the discretion of the State, a portion of the State's Federal Surface Transportation Block Grant Program (STBG) funds are made available to small urban areas between 5,000 and 50,000 population based on the latest available census figures. The majority of these funds are utilized on minor types of improvement such as resurfacing, signalization, lighting, intersection improvements, bridge replacements, signing and pavement marking, sidewalks, bikeways, grade crossings, etc. Additionally, a portion of the state's federal STBG funds are sub allocated to urban areas with populations between 50,000 and 200,000. Those funds are included in the TIPs of the Bristol, Clarksville, Cleveland, Jackson, Johnson City, Kingsport, and Lakeway MPOs. STBG funds that are sub allocated in the federal highway act for urbanized areas with populations greater than 200,000 are included in the TIPs of Chattanooga, Knoxville, Memphis, and Nashville MPO(s). The state is required to set aside a portion of funds (not from the sub-allocated amounts) for bridges not on the Federal-Aid highway system (off-

system bridges). The amount is to be not less than 15% of the State’s FY 2009 Bridge Program apportionment. The remaining funds of this type are included in the statewide category. The diagram below is used to illustrate this flow of highway-related dollars as it relates to the programming of state and locally-managed projects.

Tennessee’s Apportionment of Funds



Highway Funding Program Descriptions

Federal Highway Administration Funds are allocated to the state in numerous categories. The major funding categories are described below:

Appalachia Development Highway System (APD) - This program provides funding for routes with remaining work deemed eligible as approved by the Appalachian Regional Commission in the most recent APD Cost Estimate. This funding program was eliminated under MAP-21. The FAST Act continues that precedent with the elimination of a standalone funding category for APD. APD projects may be eligible for FAST Act STBG and NHPP funds. Unexpended APD funds remain in place under pre-FAST Act rules and regulations. States can apply for 100% Federal share using APD or other apportioned funds (e.g., NHPP or STBG) for highways and access roads on the Appalachia Development Highway System.

Bridge Replacement and Rehabilitation (BRR) - This program provides funding for bridge replacement, or to rehabilitate aging or substandard bridges based on bridge sufficiency ratings. This funding program was eliminated under MAP-21. The FAST Act continues that precedent with the elimination of a standalone funding category for BRR. All projects previously authorized under the BRR Program and carry-over funds will still be administered under previous rules and regulations of the BRR Program.

Congestion Mitigation and Air Quality (CMAQ) - The CMAQ program provides a flexible funding source to State and local governments for transportation projects and programs to help meet the requirements of the Clean Air Act. Funding is available to reduce congestion and improve air quality for areas that do not meet the National Ambient Air Quality Standards for ozone, carbon monoxide, or particulate matter (non-attainment areas) and for former non-attainment areas that are now in compliance (maintenance areas). Funds may be used for transportation projects likely to contribute to the attainment or maintenance of a national ambient air quality standard, have a high level of effectiveness in reducing air pollution, and be included in the MPO's current transportation plan and TIP or the current STIP in areas without an MPO. Currently, all non-attainment and maintenance areas in the state are located within an MPO area, meaning that air quality conformity must be demonstrated in the MPO long-range transportation plans.

Counties Eligible for CMAQ Funding



Some specific eligible activities are described below:

- Establishment or operation of a traffic monitoring, management, and control facility, including advanced truck stop electrification systems, if it contributes to attainment of an air quality standard.
- Projects that improve traffic flow, including projects to improve signalization, construct HOV lanes, improve intersections, add turning lanes, improve transportation systems management and operations that mitigate congestion and improve air quality, and implement ITS solutions and other CMAQ-eligible projects, including projects to improve incident and emergency response or improve mobility, such as real-time traffic, transit, and multimodal traveler information.
- Purchase of integrated, interoperable emergency communications equipment.
- Projects that shift traffic demand to non-peak hours or other transportation modes, increase vehicle occupancy rates, or otherwise reduce demand.
- Purchase of diesel retrofits or conducts of related outreach activities.
- Facilities serving electric or natural gas-fueled vehicles (except where this conflicts with prohibition on rest area commercialization) are explicitly eligible.
- Some expanded authority to use funds for transit operations.
- Workforce development, training, and education activities are also an eligible use of CMAQ funds.

The time period for use of CMAQ funds is limited, specifically with the funds used for the operation of services and facilities. Previously, the operating assistance funds were allowable for use over a period of three years;

however, those funds may now be tapered for use over the course of a five-year period in an effort to allow for an incremental reduction in assistance.

Demonstration Set Aside of the STP (DEMO) - This program provides special funding for certain projects.

Enhancement Activity Set Aside of the STP (ENH) - This program provides funding for pedestrian and bicycle facilities; pedestrian and bicycle safety and educational activities; acquisition of scenic easements and scenic or historic sites; scenic or historic highway programs; landscaping and other scenic beautification activities; historic preservation; rehabilitation of historic transportation buildings, structures, or facilities; preservation of abandoned railway corridors; control and removal of outdoor advertising; archaeological planning and research; environmental mitigation to address water pollution due to highway runoff or reduce vehicle-caused wildlife mortality while maintaining habitat connectivity; establishment of transportation museums. Under the Tennessee Roadscapes grant program, types of work will include landscaping, irrigation, benches, trash cans, paths, and signage. This funding program was eliminated under MAP-21. All projects previously authorized under the ENH Program and carry-over funds will still be administered under previous rules and regulations of the ENH Program.

Federal Lands Access Program (FLAP) - This program provides funds for projects on Federal Lands Access Transportation Facilities that are located on, adjacent to, or that provide access to Federal lands. A Federal Lands access transportation facility is defined as a public highway, road, bridge, trail, or transit system that is located on, adjacent to, or provides access to Federal lands for which title or maintenance responsibility is vested in a State, county, town, township, tribal, municipal, or local government. Funds are distributed by formula among States that have Federal Lands managed by the National Park Service, the U.S. Forest Service, the U.S. Fish and Wildlife Service, the Bureau of Land Management, and the U.S. Army Corps of Engineers. The Eastern Federal Lands Highway Division is responsible for administering the FLAP and serving the transportation engineering needs of these Federal Land Management agencies in Tennessee. Eligible Activities include but are not limited to: Transportation planning, research, engineering, preventive maintenance, rehabilitation, restoration, construction, and reconstruction of Federal Lands located on or adjacent to, or that: provide access to a Federal land (adjacent vehicular parking areas; acquisition of necessary scenic easements and scenic or historic sites; provisions for pedestrians and bicycles; environmental mitigation in or adjacent to Federal land to improve public safety and reduce vehicle-caused wildlife mortality while maintaining habitat connectivity; construction and reconstruction of roadside rest areas, including sanitary and water facilities); operation and maintenance of transit facilities.

Tennessee Federal Lands Map



Forest Highway/Public Lands (FH/PL) - This funding program was eliminated under MAP-21. All projects previously authorized under the Forest Highway Program and carry-over funds from SAFETEA-LU will still be administered under previous rules and regulations of the Forest Highway Program.

High Priority Projects (HPP and HPP-L) - This program provides designated funding to the state (HPP) and Local Agencies (HPP-L) for specific projects identified by Congress.

Highway Safety Improvement Program (HSIP) - The FAST Act continues the Highway Safety Improvement Program (HSIP) to achieve a significant reduction in traffic fatalities and serious injuries on all public roads, including non-State-owned public roads and roads on tribal lands. The HSIP requires a data-driven, strategic approach to improving highway safety on all public roads that focuses on performance. The FAST Act continues the overarching requirement that HSIP funds be used for safety projects that are consistent with the State's Strategic Highway Safety Plan (SHSP) and that correct or improve a hazardous road location or feature or address a highway safety problem. Under MAP-21, the HSIP statute listed a range of eligible HSIP projects. However, the list was non-exhaustive, and a State could use HSIP funds on any safety project (infrastructure-related or non-infrastructure) that met the overarching requirement. In contrast, the FAST Act limits HSIP eligibility to only those listed in 23 U.S.C. § 148—most of which are infrastructure-safety related.

High Risk Rural Road (HRRR) - A HRRR is any rural major or minor collector or a rural local road with significant safety risks, as defined by a State in accordance with an updated SHSP. This funding program was eliminated as a separate program under MAP-21. All projects previously authorized under the HRRR Program and carry-over funds will still be administered under previous rules and regulations of the HRRR Program.

Interstate Maintenance (IM) - This program provides funding to rehabilitate, restore, and resurface the Interstate System. Reconstruction is also eligible if it does not add capacity, and High-Occupancy-Vehicle (HOV) lanes can be added. This funding program was eliminated under MAP-21. All projects previously authorized under the IM Program and carry-over funds will still be administered under previous rules and regulations of the IM Program.

National Highway Freight Program (NHFP) - The FAST Act established a new National Highway Freight Program to improve the efficient movement of freight on the National Highway Freight Network (NHFN) and support several goals. Generally, NHFP funds must contribute to the efficient movement of freight on the NHFN and be identified in a freight investment plan included in the State's freight plan (required in FY 2018 and beyond). In addition, a State may use not more than 10% of its total NHFP apportionment each year for freight intermodal or freight rail projects. Eligible uses of program funds are as follows:

- Development phase activities, including planning, feasibility analysis, revenue forecasting, environmental review, preliminary engineering and design work, and other preconstruction activities.
- Construction, reconstruction, rehabilitation, acquisition of real property (including land relating to the project and improvements to land), construction contingencies, acquisition of equipment, and operational improvements directly relating to improving system performance.

- Intelligent transportation systems and other technology to improve the flow of freight, including intelligent freight transportation systems.
- Efforts to reduce the environmental impacts of freight movement.
- Environmental and community mitigation for freight movement.
- Railway-highway grade separation.
- Geometric improvements to interchanges and ramps.
- Truck-only lanes.
- Climbing and runaway truck lanes.
- Adding or widening of shoulders.
- Truck parking facilities eligible for funding under section 1401 (Jason's Law) of MAP-21.
- Real-time traffic, truck parking, roadway condition, and multimodal transportation information systems.
- Electronic screening and credentialing systems for vehicles, including weigh-in-motion truck inspection technologies.
- Traffic signal optimization, including synchronized and adaptive signals.
- Work zone management and information systems.
- Highway ramp metering.
- Electronic cargo and border security technologies that improve truck freight movement.
- Intelligent transportation systems that would increase truck freight efficiencies inside the boundaries of intermodal facilities.
- Additional road capacity to address highway freight bottlenecks.
- Physical separation of passenger vehicles from commercial motor freight.
- Enhancement of the resiliency of critical highway infrastructure, including highway infrastructure that supports national energy security, to improve the flow of freight.
- A highway or bridge project, other than a project described above, to improve the flow of freight on the NHFN.
- Any other surface transportation project to improve the flow of freight into and out of an eligible intermodal freight facility.
- Diesel retrofit or alternative fuel projects under the Congestion Mitigation and Air Quality Improvement program (CMAQ) for class 8 vehicles.
- Conducting analyses and data collection related to the NHFP, developing and updating freight performance targets to carry out section 167 of title 23, and reporting to the Administrator to comply with the freight performance target under section 150 of title 23.

National Highway Performance Program (NHPP) - The FAST Act continues the NHPP, which provides support for the condition and performance of the National Highway System (NHS), for the construction of new facilities on the NHS, and to ensure that investments of Federal-aid funds in highway construction are directed to support progress toward the achievement of performance targets established in a State's asset management plan for the NHS.

NHPP projects must be on an eligible facility and support progress toward achievement of national performance goals for improving infrastructure condition, safety, mobility, or freight movement on the NHS, and be consistent with Metropolitan and Statewide planning requirements. Eligible activities include:

- Construction, reconstruction, resurfacing, restoration, rehabilitation, preservation, or operational improvements of NHS segments.
- Construction, replacement (including replacement with fill material), rehabilitation, preservation, and protection (including scour countermeasures, seismic retrofits, impact protection measures, security countermeasures, and protection against extreme events) of NHS bridges and tunnels.
- Bridge and tunnel inspection and evaluation on the NHS and inspection and evaluation of other NHS highway infrastructure assets.
- Training of bridge and tunnel inspectors.
- Construction, rehabilitation, or replacement of existing ferry boats and facilities, including approaches that connect road segments of the NHS.
- Construction, reconstruction, resurfacing, restoration, rehabilitation, and preservation of, and operational improvements for, a Federal-aid highway not on the NHS, and construction of a transit project eligible for assistance under chapter 53 of title 49, if the project is in the same corridor and in proximity to a fully access-controlled NHS route, if the improvement is more cost-effective (as determined by a benefit-cost analysis) than an NHS improvement, and will reduce delays or produce travel time savings on the NHS route and improve regional traffic flow.
- Bicycle transportation and pedestrian walkways.
- Highway safety improvements on the NHS.
- Capital and operating costs for traffic and traveler information, monitoring, management, and control facilities and programs.
- Development and implementation of a State Asset Management Plan for the NHS including data collection, maintenance and integration, software costs, and equipment costs.
- Infrastructure-based ITS capital improvements.
- Environmental restoration and pollution abatement.
- Control of noxious weeds and establishment of native species.
- Environmental mitigation related to NHPP projects.
- Construction of publicly owned intracity or intercity bus terminals servicing the NHS.
- Workforce development, training, and education activities are also an eligible use of NHPP funds.
- Installation of vehicle-to-infrastructure communication equipment.
- Reconstruction, resurfacing, restoration, rehabilitation, or preservation of a bridge on a non-NHS Federal-aid highway (if Interstate System and NHS Bridge Condition provision requirements are satisfied).
- A project to reduce the risk of failure of critical NHS infrastructure (defined to mean a facility, the incapacity or failure of which would have a debilitating impact in certain specified areas).
- At a State's request, the U.S. DOT may use the State's STBG funding to pay the subsidy and administrative costs for TIFIA credit assistance for an eligible NHPP project or group of projects.

National Highway System (NHS) - This Program provides funding for major roads including the Interstate System, a large percentage of urban and rural principal arterials, the Strategic Defense Highway Network (STRAHNET), and strategic highway connectors. This funding program was eliminated under MAP-21. All projects previously authorized under the NHS Program and carry-over funds will still be administered under previous rules and regulations of the NHS Program.

National Scenic Byways Program (NSBP) - This Program provides funding for roads having outstanding scenic, historic, cultural, natural, recreational, and archaeological qualities and provides for designation of these roads as National Scenic Byways, All-American Roads or America's Byways. This funding program was eliminated under MAP-21. All projects previously authorized under the NSBP Program and carry-over funds will still be administered under previous rules and regulations of the NSBP Program.

Recreational Trails (RTP) - This program provides funding for the creation, rehabilitation and maintenance of multi-use recreational trails. The FAST Act reauthorized the RTP for Federal fiscal years 2016-2020 as a set-aside of funds from the Transportation Alternatives Set-Aside (TA) under the Surface Transportation Block Grant Program (STBG).

Repurposed Funds (R-) - This acronym does not represent a specific funding program, but rather the re-designation of funds. The Consolidated Appropriations Act of 2016 allows States to repurpose certain funds originally earmarked for specific projects more than 10 years ago. The limitations in the provision are to ensure the projects are obligated promptly and used in the same geographic area as the original earmark to provide funding for other needed projects eligible under the Surface Transportation Block Grant Program. In this STIP, repurposed earmark funds are designated with 'R-' preceding the funding code (i.e. R-NHPP, R-HSIP, and R-PHSIP).

Safe Routes to School (SRTS) - This program provides funding for empowering communities to make walking and bicycling to school a safe and routine activity. This funding program was eliminated under MAP-21, with eligibilities initially carried forward under the Transportation Alternatives Program (TAP). Under the FAST Act, SRTS activities are carried forward under the Surface Transportation Alternatives (TA) under the Surface Transportation Block Grant Program (STBG). All projects previously authorized under the SRTS Program and carry-over funds will still be administered under previous rules and regulations of the SRTS Program.

Section 154 - This Program provides funding to help save lives, prevent injuries, and reduce economic costs due to road traffic crashes, through education, research, safety standards, and enforcement activity.

State (STA) - This program provides 100% state funding by the state for various projects on the State Highway System.

Surface Transportation Block Grant Program (STBG) - The FAST Act converted the long-standing Surface Transportation Program into the Surface Transportation Block Grant Program (STBG). The new STBG Program provides flexible funding that may be used by States and localities for projects to preserve and improve the conditions and performance on any Federal-aid highway, bridge and tunnel projects on any public road, pedestrian and bicycle infrastructure, and transit capital projects, including intercity bus terminals. Eligible activities include, but are not limited to:

- Construction, reconstruction, rehabilitation, resurfacing, restoration, preservation, or operational improvements for highways, including designated routes of the Appalachian Development Highway System (ADHS) and local access roads under 40 U.S.C. 14501.
- Replacement, rehabilitation, preservation, protection, and anti-icing/deicing for bridges and tunnels on any public road, including construction or reconstruction necessary to accommodate other modes.
- Construction of new bridges and tunnels on a Federal-aid highway.
- Inspection and evaluation of bridges, tunnels and other highway assets as well as training for bridge and tunnel inspectors.
- Capital costs for transit projects eligible for assistance under chapter 53 of title 49, including vehicles and facilities used to provide intercity passenger bus service.
- Carpool projects, fringe and corridor parking facilities and programs, including electric and natural gas vehicle charging infrastructure, bicycle transportation and pedestrian walkways, and ADA sidewalk modification.
- Highway and transit safety infrastructure improvements and programs, installation of safety barriers and nets on bridges, hazard eliminations, mitigation of hazards caused by wildlife, railway-highway grade crossings.
- Highway and transit research, development, technology transfer.
- Capital and operating costs for traffic monitoring, management and control facilities and programs, including advanced truck stop electrification.
- Surface transportation planning.
- Transportation alternatives - newly defined with set-aside, includes most transportation enhancement eligibilities.
- Transportation control measures.
- Development and establishment of management systems.
- Environmental mitigation efforts (as under National Highway Performance Program).
- Intersections with high accident rates or levels of congestion.
- Infrastructure-based ITS capital improvements.
- Environmental restoration and pollution abatement.
- Control of noxious weeds and establishment of native species.
- Congestion pricing projects and strategies, including electric toll collection and travel demand management strategies and programs.
- Recreational trails projects.
- Construction of ferry boats and terminals.
- Border infrastructure projects.
- Truck parking facilities.
- Development and implementation of State asset management plan for the NHS, and similar activities related to the development and implementation of a performance based management program for other public roads.
- Surface transportation infrastructure modifications within port terminal boundaries, only if necessary to facilitate direct intermodal interchange, transfer, and access into and out of the port.

- Construction and operational improvements for a minor collector in the same corridor and in proximity to an NHS route if the improvement is more cost-effective (as determined by a benefit-cost analysis) than an NHS improvement and will enhance NHS level of service and regional traffic flow.
- Workforce development, training, and education activities are also an eligible use of STBG funds.
- Create and operate a State office to help design, implement, and oversee public-private partnerships (P3) eligible to receive Federal highway or transit funding, and to pay a stipend to unsuccessful P3 bidders in certain circumstances.
- At a State's request, the U.S. DOT may use the State's STBG funding to pay the subsidy and administrative costs for TIFIA credit assistance for an eligible STBG project or group of projects.

Similar to the past STP funding, STBG projects may not be on local or rural minor collectors. However, there are a number of exceptions to this requirement. A State may use up to 15% of its rural sub allocation on minor collectors. Other exceptions include: bridge and tunnel projects; safety projects; fringe and corridor parking facilities/programs; recreational trails, pedestrian and bicycle projects, and safe routes to school projects; boulevard/roadway projects largely in the right-of-way of divided highways; inspection/evaluation of bridges, tunnels, and other highway assets; port terminal modifications; and projects within the pre-FAST Act title 23 definition of "transportation alternatives".

The FAST Act continues the MAP-21 set-aside of a share of each State's STBG apportionment for use on bridges not on Federal-aid highways ("off-system bridges"). The amount is to be not less than 15% of the State's FY 2009 Highway Bridge Program apportionment. The Secretary, after consultation with State and local officials, may reduce a State's set-aside requirement if the State has insufficient off-system bridge needs. For wholly State/locally funded projects to replace or rehabilitate deficient off-system bridges, any amounts spent that are in excess of 20% of project costs may be credited to the non-Federal share of eligible bridge projects in the State.

Transportation Alternatives Set-Aside (TA) The FAST Act eliminated the MAP-21 Transportation Alternatives Program (TAP) and replaced it with a set-aside of Surface Transportation Block Grant (STBG) program funding for transportation alternatives. These set-aside funds include all projects and activities that were previously eligible under TAP, encompassing a variety of smaller-scale transportation projects such as pedestrian and bicycle facilities, recreational trails, safe routes to school projects, community improvements such as historic preservation and vegetation management, and environmental mitigation related to storm water and habitat connectivity.

Generally, transportation alternatives eligibilities are the same as those under the prior TAP and consist of:

- Transportation Alternatives as defined in 23 U.S.C. 101(a)(29) (MAP-21 1103):
 - Construction, planning, and design of on-road and off-road trail facilities for pedestrians, bicyclists, and other non-motorized forms of transportation, including sidewalks, bicycle infrastructure, pedestrian and bicycle signals, traffic calming techniques, lighting and other safety-related infrastructure, and transportation projects to achieve compliance with the Americans with Disabilities Act of 1990.

- Construction, planning, and design of infrastructure-related projects and systems that will provide safe routes for non-drivers, including children, older adults, and individuals with disabilities to access daily needs.
- Conversion and use of abandoned railroad corridors for trails for pedestrians, bicyclists, or other non-motorized transportation users.
- Construction of turnouts, overlooks, and viewing areas.
- Community improvement activities, including;
 - Inventory, control, or removal of outdoor advertising;
 - Historic preservation and rehabilitation of historic transportation facilities;
 - Vegetation management practices in transportation rights-of-way to improve roadway safety, prevent against invasive species, and provide erosion control; and
 - Archaeological activities relating to impacts from implementation of transportation project eligible under title 23.
- Any environmental mitigation activity, including pollution prevention and pollution abatement activities and mitigation to;
 - Address storm water management, control, and water pollution prevention or abatement related to highway construction or due to highway runoff, including activities described in sections 133(b)(11), 328(a), and 329 of title 23; or
 - Reduce vehicle-caused wildlife mortality or to restore and maintain connectivity among terrestrial or aquatic habitats.
- The Recreational Trails Program under Section 206 of Title 23. This program is administered by the Tennessee Department of Environment and Conservation – Recreation Educational Services
- The Safe Routes to School Program under section 1404 of the SAFETEA-LU.
 - Infrastructure-related projects -planning, design, and construction of infrastructure-related projects on any public road or any bicycle or pedestrian pathway or trail in the vicinity of schools that will substantially improve the ability of students to walk and bicycle to school, including sidewalk improvements, traffic calming and speed reduction improvements, pedestrian and bicycle crossing improvements, on-street bicycle facilities, off-street bicycle and pedestrian facilities, secure bicycle parking facilities, and traffic diversion improvements in the vicinity of schools.
 - Non-infrastructure-related activities to encourage walking and bicycling to school, including public awareness campaigns and outreach to press and community leaders, traffic education and enforcement in the vicinity of schools, student sessions on bicycle and pedestrian safety, health, and environment, and funding for training, volunteers, and managers of safe routes to school programs.
 - Safe Routes to School coordinator.
- Planning, designing, or constructing boulevards and other roadways largely in the right-of-way of former Interstate System routes or other divided highways.
- Ineligible Activities: Section 1103 of MAP-21 eliminated the definition of transportation enhancement activities in section 104 of title 23 and inserted in its place a definition of transportation alternatives, which does not include eligibility for certain activities that were previously eligible as transportation enhancements:

- Safety and educational activities for pedestrians and bicycles.
 - Exception: Activities targeting children in Kindergarten through 8th grade are eligible under SRTS (an eligible activity under TAP funding).
 - Note: Some of these activities may be eligible under HSIP. Non-construction projects for bicycle safety remain broadly eligible for STBG funds.
- Acquisition of scenic easements and scenic or historic sites.
- Scenic or historic highway programs (including visitor and welcome centers).
 - Note: A few specific activities under this category (construction of turnouts, overlooks, and viewing areas) remain eligible under Section 101(a)(29)(D) of title 23.
- Historic preservation as an independent activity unrelated to historic transportation facilities. Note: Historic preservation and rehabilitation of historic transportation facilities are permitted as one type of community improvement activity; see Section 101(a)(29)(E).
- Operation of historic transportation facilities.
- Archaeological planning and research undertaken for proactive planning. This category now must be used only as mitigation for highway projects.
- Transportation museums.

The FAST Act allows an urbanized area with a population of more than 200,000 to use up to 50% of its sub-allocated TA funds for any STBG-eligible purpose (but still subject to the TA-wide requirement for competitive selection of projects). Additionally, it eliminated TAP’s “Flexibility of Excess Reserved Funding” provision, which allowed the use of excess TAP funds for any TAP-eligible activity or for projects eligible under the CMAQ Program.

Transit Funding Program Descriptions

The Federal Transit Program provides funding for planning, capital and operating assistance, major capital needs such as a light or commuter rail system development, large bus or rail fleet purchases, construction of transit facilities, passenger equipment for special needs, intercity bus programs, and state administration of projects of a transit nature.

There are a total of 27 agencies that provide transportation in Tennessee. TDOT’s Multimodal Transportation Resources Division provides financial assistance for the operation of public transit systems serving all the counties in the state. The services provided by Tennessee’s transit systems vary depending on the specific needs of the community and include fixed route bus, incline rail, streetcar/trolleys, commuter rail, demand response, and paratransit services to meet ADA requirements. The Division also provides capital assistance to public and private non-profit organizations that provide specialized transportation services to the elderly and people with disabilities.

There are 15 urban public transit providers in the state. The urban agencies provide public transportation services in large and small urbanized areas of Tennessee and are listed below:

- Bristol Transit
- Chattanooga Area Regional Transportation Authority
- Clarksville Transit
- Cleveland Transit (Southeast Tennessee Human Resource Agency)
- Franklin Transit
- Jackson Transit
- Johnson City Transit
- Kingsport Area Transit System
- Knoxville Area Transit
- Knox County Community Action Committee
- Memphis Area Transit Authority
- Morristown (East Tennessee Human Resource Agency)
- Murfreesboro Transit
- Nashville Metropolitan Transportation Authority
- Regional Transportation Authority of Middle Tennessee

There are 9 rural public transit providers in the state. The rural agencies provide public transportation services on a more regional basis in every county of the state. These providers are listed below:

- Delta Human Resource Agency (DHRA): online resource for Fayette, Lauderdale, Tipton and non-urban Shelby Counties
- Northwest Tennessee Human Resource Agency (Nwthra): online resource for Benton, Carroll, Crockett, Dyer, Gibson, Henry, Lake, Obion, and Weakley Counties
- Southwest Human Resource Agency (SWHRA): online resource for Chester, Decatur, Hardeman, Hardin, Haywood, Henderson, Madison, and McNairy Counties
- Mid-Cumberland Human Resource Agency (MCHRA): online resource for Cheatham, Davidson, Dickson, Houston, Humphreys, Montgomery, Robertson, Rutherford, Stewart, Sumner, Trousdale, Williamson, and Wilson Counties
- South Central Tennessee Development District (SCTDD): online resource for Bedford, Coffee, Franklin, Giles, Hickman, Lawrence, Lewis, Lincoln, Marshall, Maury, Moore, Perry, and Wayne Counties
- Upper Cumberland Human Resource Agency (UCHRA): online resource for Cannon, Clay, Cumberland, DeKalb, Fentress, Jackson, Macon, Overton, Pickett, Putnam, Smith, Van Buren, Warren, and White Counties
- Southeast Tennessee Human Resource Agency (SETHRA): online resource for Bledsoe, Bradley, Grundy, Marion, McMinn, Meigs, Polk, Rhea, and Sequatchie Counties
- East Tennessee Human Resource Agency (ETHRA): online resource for Anderson, Blount, Campbell, Claiborne, Cocke, Grainger, Hamblen, Jefferson, Knox, Loudon, Monroe, Morgan, Roane, Scott, Sevier and Union Counties
- Northeast Tennessee Rural Public Transit (NET Trans): online resource for Carter, Greene, Hancock, Hawkins, Johnson, Sullivan, Unicoi and Washington Counties (operated by First Tennessee Human Resource Agency (FTHRA))

Additional transportation is provided in Sevier and Blount counties by 3 other agencies that are listed below:

- Gatlinburg Transit
- Great Smokey Mountain Heritage Center
- Pigeon Forge Fun-Time Trolleys

TDOT serves as the recipient and administrator of federal transit assistance funds for all rural agencies and some specific programs for small urbanized agencies. To receive funds, TDOT submits electronic applications to the FTA, administers contracts with state providers and monitors their compliance with federal and state regulations. A description of the federal programs administered by this Division is provided below:

Section 5303-Metropolitan Transportation Planning - This program provides funds to MPOs for multimodal transportation planning.

Section 5304-Statewide Transportation Planning - This program provides funds to make transportation investment decisions in support of cooperative, continuous and comprehensive planning needs.

Section 5309-Discretionary Capital Investment Grants - This program provides discretionary capital assistance for the purchase of new equipment, acquisition of property and the construction of public transit facilities.

Section 5310-Formula Funding for Elderly and Persons with Disabilities Capital/Grants - This program provides transit capital assistance, through the state, to private non-profit organizations and public bodies that provide specialized transportation services to elderly and/or persons with disabilities. Annually, eligible applicants apply to the Division for funding to acquire equipment to provide specialized transportation needs of elderly and/or persons with disabilities. Funds apportioned after 2013 are also eligible for SAFETEA-LU 5317 activities. These activities address the transportation needs of persons with disabilities that go beyond the requirements of the Americans with Disabilities Act.

Section 5311-Non-Urbanized Area Formula Grants - This formula grant program provides funds to support public transportation in rural areas with a population less than 50,000. The funds are used for transit capital, operating and administrative expenses for projects that meet the needs of the rural communities. Specialized program areas of the 5311 grant include.

U5311 (f)-Intercity Bus Service - The State must use 15% of its annual apportionment to support this service unless the Governor certifies that the needs of the state are adequately met.

U5311 RTAP-Rural Transit Assistance Program - This program provides funds to assist in the design and implementation of training, technical assistance, research, and other support services to meet the needs of transit operators in the non-urbanized areas.

U5311 Appalachian -Appalachian Development Public Transportation Assistance Program - This program provides funds to carry out a public transportation assistance program in the Appalachian Region.

Section 5316-Job Access Reverse Commute Grants - This program provides capital, operating and administrative transit expenses for projects that are used to assist low-income and welfare recipients to transition from welfare to work by transportation services to reach needed job training activities, employment, and childcare. Only residual funds are available for projects as the last 5316 program apportionment was made in 2012. Once residual funds are used, the program will be removed from the STIP program list.

Section 5317-New Freedom Grants - This program provides capital, operating and administrative transit expenses for projects that are used to support new public transportation services and transportation alternatives for individuals with disabilities beyond those required by the ADA of 1990. Only residual funds are available for projects as the last 5317 program apportionment was made in 2012. Once residual funds are used, the program will be removed from the STIP program list.

Section 5329-State Safety Oversight Program - This grant program provides funding for states to oversee safety at rail transit systems in their jurisdiction.

Section 5339-Bus and Bus Facilities Grants - This program provides for capital funding to replace, rehabilitate and purchase buses, vans, and related equipment and to construct bus related facilities.

The tables in the following pages document the transit funds allocated by fiscal year. Note that these funds are only those allocated in rural counties. FTA funding for urban areas designated as a MPO is not listed in this document, but is included in each respective MPO's TIP.

Highway Funding Project Phases in the STIP

The following project phases have been identified and are provided with funding allocations for the aforementioned highway funding programs.

- **CONST (Construction)** - Work by the agency or contractor(s) to construct the project, possibly including utility relocation.
- **ITS (Intelligent Transportation Systems)** - Procuring, developing, or integrating technology to manage transportation facilities, improve safety, or mobility.
- **OPERATIONS or OP** - Operating the transportation system such as incurring costs related to the day-to-day operations or maintenance of transit vehicle systems, traffic signal systems, or intelligent transportation systems.
- **PE-N (Preliminary Engineering – NEPA)** - Includes activities from the inception of the project, fulfilling the requirements of the National Environmental Policy Act of 1969 and all applicable legislation, regulations, executive orders, and directives, up to the approval of the environmental document.
- **PE-D (Preliminary Engineering – Design)** - Preliminary engineering design work, according to accepted engineering practices, after approval of the environmental document.
- **PLANNING** – Planning costs.

- **PURCHASE/ACQUIRE** - Procuring equipment, software, or vehicles.
- **ROW (Right-of-Way)** - Work from the distribution of ROW plans up to advertising for bids or commencement of work by the Agency, dealing with real property acquisition, temporary and permanent easements, and utility relocation.
- **TRAINING** – Training activities.

Federal, State, and Local Match Requirements by Funding Program Chart

Program		Funding Match Requirements
APD	Appalachian Development Highway System Projects	100% Federal
BRR	Bridge Replacement and Rehabilitation	80% Federal, 20% State
CMAQ	Congestion Mitigation Air Quality	80% Federal, 20% State/Local
DEMO	Demonstration project, special set aside	100% Federal or 80% Federal, 20% State/Local
ENH	Enhancement Activity set aside of the STP	80% Federal, 20% Local/State
FH/PL	Forest Highway/Public Lands Projects	100% Federal or 80% Federal, 20% State
FLAP	Federal Lands Access Program	100% Federal or 80% Federal, 20% State
HPP	High Priority Projects	80% Federal, 20% State
HPP-L	High Priority Projects (Local)	80% Federal, 20% Local
HRRR	High Risk Rural Roads	90% Federal, 10% State/Local
HSIP	Highway Safety Improvement Program	90% Federal, 10% State
IM	Interstate Maintenance Projects	90% Federal, 10% State
IMD	Interstate Maintenance Discretionary Funds	90% Federal, 10% State/Local
NHFP	National Highway Freight Program	90% Federal, 10% State or 80% Federal, 20% State
NHPP	National Highway Performance Program	80% Federal, 20% State or 90% Federal, 10% State
NHS	National Highway System Program	80% Federal, 20% State
NSBP	National Scenic Byways Program	100% Federal or 80% Federal, 20% State/Local
PLHD	Public Lands Highways Discretionary Funds	100% Federal
RTP	Recreational Trails Projects	80% Federal, 20% State
Sect 130 (HSP-R)	Highway-Railroad Crossing Program	90% Federal, 10% State
Sect 154	Section 154	100% Federal
SRTS	Safe Routes to School	100% Federal
STA	State Funds	100% State
STBG	Surface Transportation Block Grant Program	80% Federal, 20% State/Local
TAP	Transportation Alternatives Program	80% Federal, 20% State/Local

Development of STIP Funding Levels

Fiscal Year (FY) 2020 (October 1, 2019-September 30, 2020) funds represent both remaining funds from prior years, plus projected revenue estimates for FY 2020. Due to the FAST Act expiring in FY 2020, the Department utilized similar funding projections from the FAST Act to determine the appropriate estimated revenues for each fund code. Projects using one hundred percent (100%) State funds are based on funding levels appropriated by the Tennessee General Assembly. For 100% State funds, the FY 2020 revenue figures are obtained from actual data, while Federal funds are estimated.

In non-attainment and maintenance areas, projects included in the first two years of the STIP are limited to those for which funds are available or committed. For the remaining projects and remaining years of FY 2022-2023, the figures are projections based on trends and statewide allocations outlined in the FAST Act. The amount used for federally funded projects is based on projected funds anticipated to be appropriated by congressional action each year. TDOT updates cost estimates to the year of expenditure for each project when appropriate. The data is derived from TDOT statewide cost estimates for planning purposes and is cooperatively developed by the State, MPOs, and others. TDOT further monitors estimated project cost with a defined process for updating project estimates during the Engineering and Right-of-Way phases throughout the life of each project individually and collectively on a balance sheet to maintain the fiscal constraint of the STIP, as required by law. Other types of funds as noted in this document include congressional earmark funds (HPP and HPP-L) and demonstration funds (DEMO).

The tables in the following pages document the highway funds allocated by fiscal year. Note that these funds are only those allocated in rural counties. Funding for projects located within urban areas designated as a MPO are not listed in this document, but is included in each respective MPO's TIP.

**State Transportation Improvement Program
Estimated Available and Estimated Costs for Fiscal Year 2020**

Fund Code	FY	Estimated Revenue	Programmed Cost	Federal Funds	State Funds	Local Funds	Balance	Allocation 2021	Revenue Brought Fwd. to 2021
ACNHPP	2020	\$123,500,000	\$103,500,000	\$0	\$103,500,000	\$0	\$20,000,000		\$20,000,000
ACSTBG	2020	\$0	\$0	\$0	\$0	\$0	\$0		\$0
APD	2020	\$29,200,000	\$29,200,000	\$29,200,000	\$0		\$0	\$0	\$0
CMAQ	2020	\$89,416,369	\$22,350,000	\$17,880,000	\$4,470,000	\$0	\$67,066,369	\$41,929,751	\$108,996,120
HIP	2020	\$45,000,000	\$0	\$0	\$0	\$0	\$45,000,000	\$47,250,000	\$92,250,000
HPP	2020	\$22,749,381	\$22,749,381	\$18,199,505	\$4,549,876		\$0	\$0	\$0
HRRR	2020	\$1,066,112	\$1,066,112	\$959,500	\$106,611		\$0	\$0	\$0
HSIP	2020	\$75,010,786	\$61,000,990	\$54,900,891	\$6,100,099		\$14,009,796	\$54,553,790	\$68,563,586
HSIP-R	2020	\$20,406,367	\$765,170	\$688,653	\$76,517		\$19,641,197	\$5,543,895	\$25,185,092
NHFP	2020	\$62,724,928	\$0	\$0	\$0	\$0	\$62,724,928	\$32,970,000	\$95,694,928
NHPP	2020	\$1,290,367,000	\$1,047,066,214	\$856,111,371	\$190,954,843	\$0	\$243,300,786	\$644,140,350	\$887,441,136
PHSIP	2020	\$12,000,000	\$5,330,750	\$5,330,750	\$0		\$6,669,250	\$12,600,000	\$19,269,250
RTP	2020	\$11,410,275	\$1,500,000	\$1,200,000	\$300,000		\$9,910,275	\$11,980,789	\$21,891,064
STA	2020	\$300,000,000	\$109,300,000	\$0	\$109,300,000		\$190,700,000	\$300,000,000	\$300,000,000
STBG	2020	\$480,568,444	\$334,750,584	\$268,013,467	\$65,387,892	\$1,349,224	\$145,817,860	\$231,682,500	\$377,500,360
STBG-L	2020	\$236,584	\$236,584	\$189,267	\$47,317	\$0	\$0	\$0	\$0
TAP	2020	\$8,701,500	\$8,701,500	\$6,961,200	\$1,740,300		\$0	\$9,136,575	\$9,136,575

Estimated Available and Estimated Costs for Fiscal Year 2020

**State Transportation Improvement Program
Estimated Available and Estimated Costs for Fiscal Year 2021**

Fund Code	FY	Estimated Revenue	Programmed Cost	Federal Funds	State Funds	Local Funds	Balance	Allocation 2022	Revenue Brought Fwd. to 2022
ACNHPP	2021	\$20,000,000	\$0	\$0	\$0	\$0	\$20,000,000	\$0	\$20,000,000
ACSTBG	2021	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
APD	2021	\$87,500,000	\$87,500,000	\$87,500,000			\$0	\$0	\$0
CMAQ	2021	\$108,996,120	\$4,000,000	\$3,200,000	\$800,000	\$0	\$104,996,120	\$44,026,238	\$149,022,358
HIP	2021	\$92,250,000	\$0	\$0	\$0	\$0	\$92,250,000	\$49,612,500	\$141,862,500
HPP	2021	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
HRRR	2021	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
HSIP	2021	\$68,563,586	\$59,493,490	\$53,544,141	\$5,949,349		\$9,070,096	\$57,281,479	\$66,351,575
HSIP-R	2021	\$25,185,092	\$718,400	\$646,560	\$71,840		\$24,466,692	\$5,821,090	\$30,287,782
NHFP	2021	\$95,694,928	\$0	\$0	\$0	\$0	\$95,694,928	\$34,618,500	\$130,313,428
NHPP	2021	\$887,441,136	\$461,700,000	\$370,850,000	\$90,850,000		\$425,741,136	\$676,347,368	\$1,102,088,504
PHSIP	2021	\$19,269,250	\$5,275,626	\$5,275,626	\$0		\$13,993,624	\$13,230,000	\$27,223,624
RTP	2021	\$21,891,064	\$1,500,000	\$1,200,000	\$300,000		\$20,391,064	\$12,579,828	\$32,970,892
STA	2021	\$300,000,000	\$5,000,000		\$5,000,000		\$295,000,000	\$300,000,000	\$300,000,000
STBG	2021	\$377,500,360	\$232,790,000	\$186,317,000	\$45,613,000	\$860,000	\$144,710,360	\$243,266,625	\$387,976,985
STBG-L	2021	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
TAP	2021	\$9,136,575	\$8,701,500	\$6,961,200	\$1,740,300		\$435,075	\$9,593,404	\$10,028,479

Estimated Available and Estimated Costs for Fiscal Year 2021

**State Transportation Improvement Program
Estimated Available and Estimated Costs for Fiscal Year 2022**

Fund Code	FY	Estimated Revenue	Programmed Cost	Federal Funds	State Funds	Local Funds	Balance	Allocation 2023	Revenue Brought Fwd. to 2023
ACNHPP	2022	\$20,000,000	\$0	\$0	\$0	\$0	\$20,000,000	\$0	\$20,000,000
ACSTBG	2022	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
APD	2022	\$20,000,000	\$20,000,000	\$20,000,000			\$0	\$0	\$0
CMAQ	2022	\$149,022,358	\$4,000,000	\$3,200,000	\$800,000	\$0	\$145,022,358	\$46,227,550	\$191,249,908
HIP	2022	\$141,862,500	\$0	\$0	\$0	\$0	\$141,862,500	\$52,093,125	\$193,955,625
HPP	2022	\$2,400,000	\$2,400,000	\$1,920,000	\$480,000		\$0	\$0	\$0
HRRR	2022	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
HSIP	2022	\$66,351,575	\$56,978,490	\$51,280,641	\$5,697,849		\$9,373,085	\$60,145,553	\$69,518,638
HSIP-R	2022	\$30,287,782	\$593,600	\$534,240	\$59,360		\$29,694,182	\$6,112,144	\$35,806,326
NHFP	2022	\$130,313,428	\$0	\$0	\$0	\$0	\$130,313,428	\$36,349,425	\$166,662,853
NHPP	2022	\$1,102,088,504	\$367,004,000	\$301,558,200	\$65,445,800		\$735,084,504	\$710,164,736	\$1,445,249,239
PHSIP	2022	\$27,223,624	\$5,165,376	\$5,165,376	\$0		\$22,058,248	\$13,891,500	\$35,949,748
RTP	2022	\$32,970,892	\$1,500,000	\$1,200,000	\$300,000		\$31,470,892	\$13,208,820	\$44,679,712
STA	2022	\$300,000,000	\$5,000,000		\$5,000,000		\$295,000,000	\$300,000,000	\$300,000,000
STBG	2022	\$387,976,985	\$224,945,000	\$180,041,000	\$44,904,000		\$163,031,985	\$255,429,956	\$418,461,941
STBG-L	2022	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
TAP	2022	\$10,028,479	\$8,701,500	\$6,961,200	\$1,740,300		\$1,326,979	\$10,073,074	\$11,400,053

Estimated Available and Estimated Costs for Fiscal Year 2022

**State Transportation Improvement Program
Estimated Available and Estimated Costs for Fiscal Year 2023**

Fund Code	FY	Estimated Revenue	Programmed Cost	Federal Funds	State Funds	Local Funds	Balance	Allocation 2024	Revenue Brought Fwd. to 2024
ACNHPP	2023	\$20,000,000	\$0	\$0	\$0	\$0	\$20,000,000	\$0	\$20,000,000
ACSTBG	2023	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
APD	2023	\$69,000	\$69,000	\$69,000			\$0	\$0	\$0
CMAQ	2023	\$191,249,908	\$4,000,000	\$3,200,000	\$800,000	\$0	\$187,249,908	\$48,538,928	\$235,788,836
HIP	2023	\$193,955,625	\$0	\$0	\$0	\$0	\$193,955,625	\$54,697,781	\$248,653,406
HPP	2023	\$0					\$0	\$0	\$0
HRRR	2023	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
HSIP	2023	\$69,518,638	\$54,970,990	\$49,473,891	\$5,497,099		\$14,547,648	\$63,152,831	\$77,700,478
HSIP-R	2023	\$35,806,326	\$546,800	\$492,120	\$54,680		\$35,259,526	\$6,417,751	\$41,677,277
NHFP	2023	\$166,662,853	\$0	\$0	\$0	\$0	\$166,662,853	\$38,166,896	\$204,829,749
NHPP	2023	\$1,445,249,239	\$198,235,000	\$158,728,000	\$39,507,000		\$1,247,014,239	\$745,672,973	\$1,992,687,212
PHSIP	2023	\$35,949,748	\$5,110,250	\$5,110,250	\$0		\$30,839,498	\$14,586,075	\$45,425,573
RTP	2023	\$44,679,712	\$1,500,000	\$1,200,000	\$300,000		\$43,179,712	\$13,869,261	\$57,048,972
STA	2023	\$300,000,000	\$6,000,000		\$6,000,000		\$294,000,000	\$300,000,000	\$300,000,000
STBG	2023	\$418,461,941	\$121,957,500	\$97,651,000	\$24,306,500	\$0	\$296,504,441	\$268,201,454	\$564,705,895
STBG-L	2023	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
TAP	2023	\$11,400,053	\$8,701,500	\$6,961,200	\$1,740,300		\$2,698,553	\$10,576,728	\$13,275,280

Estimated Available and Estimated Costs for Fiscal Year 2023

Transit Funding Project Phases in the STIP

The following project phases have been identified and are provided with funding allocations for the aforementioned transit funding programs:

- *ACQ* – Acquisition of vehicles or equipment.
- *AD* – Administrative costs.
- *CA or Capital* – Capital expenditures.
- *OPERATIONS or OP* – Operating the transportation system such as incurring costs related to the day-to-day operations or maintenance of transit vehicle systems.
- *PLANNING* – Planning costs.
- *SAFETY/OVERSIGHT* – Safety oversight activities of the State’s public transportation systems.
- *TRNG or TRAINING* – Training activities.

**State Transportation Improvement Program
Transit Estimated Available and Estimated Costs for Fiscal Year 2020**

Fund Code	FY	Estimated Available Funds	Estimated Total Costs	Federal	State	Local
5303	2020	\$2,354,200	\$2,354,200	\$1,883,000	\$235,600	\$235,600
5304	2020	\$3,407,531	\$3,407,531	\$2,725,731	\$681,800	\$0
5309	2020	\$805,200	\$805,200	\$641,200	\$82,000	\$82,000
5310	2020	\$20,058,400	\$20,058,400	\$16,034,400	\$2,012,000	\$2,012,000
5311	2020	\$107,490,000	\$107,490,000	\$53,744,000	\$26,873,000	\$26,873,000
5311(f)	2020	\$0	\$0	\$0	\$0	\$0
5311 RTAP	2020	\$1,664,100	\$1,664,100	\$1,664,100	\$0	\$0
5311 App	2020	\$5,162,800	\$5,162,800	\$2,579,800	\$1,291,500	\$1,291,500
5316	2020	\$841,000	\$841,000	\$419,000	\$211,000	\$211,000
5317	2020	\$3,654,000	\$3,654,000	\$1,826,000	\$914,000	\$914,000
5329	2020	\$2,309,000	\$2,309,000	\$1,843,000	\$233,000	\$233,000
5339	2020	\$17,756,000	\$17,756,000	\$14,200,000	\$1,778,000	\$1,778,000
5339(b)	2020	\$4,065,000	\$4,065,000	\$2,479,000	\$1,179,000	\$407,000
Totals	2020	\$169,567,231	\$169,567,231	\$100,039,231	\$35,490,900	\$34,037,100

Transit Estimated Available and Estimated Costs for Fiscal Year 2020

**State Transportation Improvement Program
Transit Estimated Available and Estimated Costs for Fiscal Year 2021**

Fund Code	FY	Estimated Available Funds	Estimated Total Costs	Federal	State	Local
5303	2021	\$0	\$0	\$0	\$0	\$0
5304	2021	\$462,500	\$462,500	\$370,000	\$92,500	\$0
5309	2021	\$0	\$0	\$0	\$0	\$0
5310	2021	\$4,721,000	\$4,721,000	\$3,775,000	\$473,000	\$473,000
5311	2021	\$42,120,000	\$42,120,000	\$21,060,000	\$10,530,000	\$10,530,000
5311(f)	2021	\$7,432,000	\$7,432,000	\$3,716,000	\$1,858,000	\$1,858,000
5311 RTAP	2021	\$400,000	\$400,000	\$400,000	\$0	\$0
5311 App	2021	\$2,591,000	\$2,591,000	\$1,295,000	\$648,000	\$648,000
5316	2021	\$0	\$0	\$0	\$0	\$0
5317	2021	\$0	\$0	\$0	\$0	\$0
5329	2021	\$459,000	\$459,000	\$367,000	\$46,000	\$46,000
5339	2021	\$4,825,000	\$4,825,000	\$3,859,000	\$483,000	\$483,000
5339(b)	2021	\$0	\$0	\$0	\$0	\$0
Totals	2021	\$63,010,500	\$63,010,500	\$34,842,000	\$14,130,500	\$14,038,000

Transit Estimated Available and Estimated Costs for Fiscal Year 2021

**State Transportation Improvement Program
Transit Estimated Available and Estimated Costs for Fiscal Year 2022**

Fund Code	FY	Estimated Available Funds	Estimated Total Costs	Federal	State	Local
5303	2022	\$0	\$0	\$0	\$0	\$0
5304	2022	\$473,800	\$473,800	\$379,000	\$94,800	\$0
5309	2022	\$0	\$0	\$0	\$0	\$0
5310	2022	\$5,004,000	\$5,004,000	\$4,002,000	\$501,000	\$501,000
5311	2022	\$45,488,000	\$45,488,000	\$22,744,000	\$11,372,000	\$11,372,000
5311(f)	2022	\$8,028,000	\$8,028,000	\$4,014,000	\$2,007,000	\$2,007,000
5311 RTAP	2022	\$432,000	\$432,000	\$432,000	\$0	\$0
5311 App	2022	\$2,799,000	\$2,799,000	\$1,399,000	\$700,000	\$700,000
5316	2022	\$0	\$0	\$0	\$0	\$0
5317	2022	\$0	\$0	\$0	\$0	\$0
5329	2022	\$497,000	\$497,000	\$397,000	\$50,000	\$50,000
5339	2022	\$5,066,000	\$5,066,000	\$4,052,000	\$507,000	\$507,000
5339(b)	2022	\$0	\$0	\$0	\$0	\$0
Totals	2022	\$67,787,800	\$67,787,800	\$37,419,000	\$15,231,800	\$15,137,000

Transit Estimated Available and Estimated Costs for Fiscal Year 2022

**State Transportation Improvement Program
Transit Estimated Available and Estimated Costs for Fiscal Year 2023**

Fund Code	FY	Estimated Available Funds	Estimated Total Costs	Federal	State	Local
5303	2023	\$0	\$0	\$0	\$0	\$0
5304	2023	\$485,000	\$485,000	\$388,000	\$97,000	\$0
5309	2023	\$0	\$0	\$0	\$0	\$0
5310	2023	\$5,305,000	\$5,305,000	\$4,243,000	\$531,000	\$531,000
5311	2023	\$49,128,000	\$49,128,000	\$24,564,000	\$12,282,000	\$12,282,000
5311(f)	2023	\$8,671,000	\$8,671,000	\$4,335,000	\$2,168,000	\$2,168,000
5311 RTAP	2023	\$467,000	\$467,000	\$467,000	\$0	\$0
5311 App	2023	\$3,023,000	\$3,023,000	\$1,511,000	\$756,000	\$756,000
5316	2023	\$0	\$0	\$0	\$0	\$0
5317	2023	\$0	\$0	\$0	\$0	\$0
5329	2023	\$537,000	\$537,000	\$429,000	\$54,000	\$54,000
5339	2023	\$5,319,000	\$5,319,000	\$4,255,000	\$532,000	\$532,000
5339(b)	2023	\$0	\$0	\$0	\$0	\$0
Totals	2023	\$72,935,000	\$72,935,000	\$40,192,000	\$16,420,000	\$16,323,000

Transit Estimated Available and Estimated Costs for Fiscal Year 2023

Chapter 5: Performance Measures

Introduction

Performance measurement provides transportation agencies with a better ability to identify and mitigate issues with their respective transportation networks. The introduction of The Moving Ahead for Progress in the 21st Century Act (MAP-21) and Fixing America's Surface Transportation (FAST) Act requires various transportation agencies to take a performance-based approach to planning and programming activities. TDOT is currently coordinating with regional, State, and Federal partners to maintain compliance with Federal laws and regulations related to performance management. The integration of performance management regulations outlined in MAP-21 and the FAST Act into TDOT's plans and processes is an evolving effort.

- Note: This section was prepared using the most recent transportation act, the FAST Act, and was developed with the intent of including performance metrics mandated by the most recent legislation. The FAST Act did not change national performance goals related to safety, infrastructure condition, congestion reduction, system reliability, freight movement and economic viability, environmental sustainability, and reduced project delivery delays, which were established as part of MAP-21.

Federal Requirements

MAP-21 was developed with the intent of creating a streamlined, performance-based, multimodal program in an effort to address challenges facing the U.S. transportation system. In relation to performance management, MAP-21 aims to increase the accountability and transparency of Federal highway programs by improving transportation investment decision-making through performance-based planning and programming.

As a result of MAP-21, seven (7) national goals were established to address safety, current infrastructure, traffic congestion, efficiency, environment, transportation delays, and project delivery delays:

- 1) Safety - To achieve a significant reduction in traffic fatalities and serious injuries on all public roads.
- 2) Infrastructure Condition - To maintain the highway infrastructure asset system in a state of good repair.
- 3) Congestion Reduction - To achieve a significant reduction in congestion on the National Highway System (NHS).
- 4) System Reliability - To improve the efficiency of the surface transportation system.
- 5) Freight Movement and Economic Vitality - To improve the national freight network, strengthen the ability of rural communities to access national and international trade markets, and support regional economic development.
- 6) Environmental Sustainability - To enhance the performance of the transportation system while protecting and enhancing the natural environment.

- 7) Reduced Project Delivery Delays - To reduce project costs, promote jobs and the economy, and expedite the movement of people and goods by accelerating project completion through eliminating delays in the project development and delivery process, including reducing regulatory burdens and improving agencies' work practices.

The FAST Act continues MAP-21's overall performance approach and ensures that State DOTs invest in projects that collectively make progress toward the National Goals that were established by Congress. The FAST Act made few adjustments to the performance management provisions established by MAP-21. As required under the most recent transportation act, TDOT is required to establish performance targets, and track progress towards target achievement, for the following performance measures:

Safety (PM1)

- 1) Total number of traffic related fatalities on all public roads.
- 2) Rate of traffic related fatalities on all public roads per 100 million VMT.
- 3) Total number of traffic related serious injuries on all public roads.
- 4) Rate of traffic related serious injuries on all public roads per 100 million VMT.
- 5) Total number of non-motorized fatalities and serious injuries on all public roads.

Pavement Condition (PM2)

- 6) Percentage of pavement on the Interstate System in Good condition.
- 7) Percentage of pavement on the Interstate System in Poor condition.
- 8) Percentage of the non-interstate National Highway System in Good condition.
- 9) Percentage of the non-interstate National Highway System in Poor condition.

Bridge Condition (PM2)

- 10) Percentage of National Highway System bridges classified as in Good condition.
- 11) Percentage of National Highway System bridges classified as in Poor condition.

NHS Travel Time Reliability (PM3)

- 12) Percent of the Person-Miles Traveled on the Interstate that is reliable.
- 13) Percent of the Person Miles Traveled on the Non-Interstate National Highway System that is reliable.

Freight Reliability (PM3)

- 14) Percentage of Interstate System mileage providing reliable truck travel time.

Traffic Congestion (PM3)

- 1) Annual hours of peak hour excessive delay per capita.
- 2) Percentage of Non-Single Occupancy Vehicle Travel.

Total Emissions Reduction (PM3)

- 1) Total emissions reductions by applicable pollutants under the CMAQ program.

FY 2020 – FY 2023 STIP and Performance Measures

The Tennessee Department of Transportation (TDOT) acknowledges the importance of implementing performance-based planning within the Statewide Transportation Improvement Program (STIP) that reflects federal, state and regional priorities. The STIP implements the policies and programs of the Tennessee Statewide Long Range Transportation Plan. The Statewide Long Range Plan has seven (7) Guiding Principles supported by a number of initiatives and policies spanning important policy paper topics. Three (3) of these Guiding Principles, which contribute to the implementation of PM1, PM2, and PM3 are:

- Maximize Safety and Security by reducing injuries and fatalities in all modes of transportation, minimizing construction-related safety incidents, and improving disaster preparedness and incident response, and;
- Preserve and Manage the Existing System by protecting existing assets and maintaining the efficiency of the system through cost-effective management and new technologies, and;
- Provide for the Efficient Movement of People and Freight by providing greater access to transportation services for all people and by building better connections among different modes of transportation.

The Statewide Long Range Transportation Plan outlines a variety of priority initiatives that promote TDOT's Vision and Guiding Principles. The following sections detail the three (3) Performance Measure areas and associated targets, and describe policy initiatives undertaken by the Department that serve to implement them.

Safety Performance Measures (PM1)

The Statewide Long Range Transportation Plan's Safety Policy Paper states that the protection of life and property for all transportation modes in Tennessee is the most important element of TDOT's mission.

This protection is generally accomplished through planning (including data collection, analysis, and performance management), regulation, enforcement, management, operations, design, and maintenance. TDOT is responsible for a number of programs related to multimodal transportation safety and supports other agencies in their transportation safety activities.

TDOT utilizes a multidisciplinary group of individuals within different Divisions and Bureaus to ensure that the State's Safety Performance Targets are an important factor in determining project priorities. Each project and project phase receives a full crash data investigation prior to any investment decision making. Additionally, the goals, objectives and countermeasures from the Tennessee Strategic Highway Safety Plan are strongly considered when developing the project scope of work. These targets are consistent with TDOT's Strategic Direction (Mission Statement), align with 23 U.S.C. 150 (Prioritize Safety), and are discussed throughout the STIP (ENH, HSIP, HRRR, NHPP, STBG, TAP, ITS, etc.).

PM1 is a vital part of TDOT’s operational and planning procedures to ensure that roadway crash data trends towards our projected goals and targets.

In order to develop PM1 (Safety) targets, an ongoing Safety PM Working Group was established consisting of staff from TDOT, the Tennessee Department of Safety and Homeland Security, FHWA, and the MPOs. The target setting process consisted of data review, trend analysis, consideration of key factors and assumptions, and review and consensus on draft targets. The Safety PM Working Group provided recommendations to an oversight committee which included Directors from both TDOT and the Tennessee Highway Safety Office (THSO). Finalized 2019 targets were presented to the Executive Leadership at both agencies for review and approval.

The 2019 Baseline and Safety Targets are presented in the chart below. Additional information on TDOT’s Safety Targets can be found on FHWA’s website at the link below:

<https://www.fhwa.dot.gov/tpm/reporting/state/safety.cfm?state=Tennessee>

Safety Performance Measure Targets Chart

Measure	2013-2017 Baseline	2015-2019 Targets
Number of Fatalities*	1,001.2	1022.0
Fatality Rate*	1.332	1.291
Number of Serious Injuries*	7,226.0	7374.6
Serious Injury Rate	9.662	9.324
Number of Non- motorized Serious Injuries and Fatalities	469.2	546.8

Bridge and Pavement Condition Performance Measures (PM2)

The Bridge and Pavement Condition Final Rule established performance measures for the purpose of maintaining highway infrastructure in a state of good repair, and supports TDOT's Transportation Asset Management Plan. The Federal Highway Administration (FHWA) advises state Departments of Transportation to set realistic targets which are based on analysis of trends and projections of future efforts. TDOT strives to comply with this Federal rule, however, it should be noted that these performance measures tell only a partial story of asset condition. Because of this, TDOT continues to make programming and planning decisions using a variety of data, information, and other input related to infrastructure condition.

TDOT utilizes a multidisciplinary group of individuals within different Divisions and Bureaus to ensure that the State's PM2 Performance Targets are an important factor in determining project priorities. PM2 is a vital part of TDOT's operational and planning procedures to ensure that pavement and bridge performance data trends towards our projected goals and targets.

TDOT's budget can be categorized into four (4) general areas – Operation & Management, Maintenance, Highway & Bridge Construction, and Transit, Air, Water, & Rail. Over the past 15 years, the relative share of the budget has shrunk for Highway & Bridge Construction, whereas the Operation & Management and Maintenance categories have increased over time.

TDOT has always been a "fix it first" organization, and places a great amount of emphasis on its ability to identify problem areas that arise in various portions of its infrastructure and respond with a maintenance solution that is often more cost effective than a complete replacement. As the transportation infrastructure continues to age, the maintenance needs of roadways and bridges will continue to grow.

Roadways are TDOT's largest asset and, thereby, one of the most expensive portions of the transportation infrastructure. Each year, approximately \$150 million is spent on average on the pavement systems across the state. These dollars are put towards activities such as hot-mix asphalt resurfacing, which can add an additional 11 to 12 years to the life of a roadway. Other pavement activities include micro-surfacing, surface seals, and crack/joint sealing. The pavement activities on state routes occupy a larger percentage of this budget item compared the Interstate system due to the relative mileage of state routes compared to Interstates.

TDOT has made a strong commitment to keeping Tennessee's bridges in a state of good repair. This is accomplished by fulfilling the federal requirement of inspecting all state-maintained bridges on a 24-month cycle. TDOT consistently meets and exceeds its target of examining 95% of all bridges, which helps to determine where investments need to be made in the repair or replacement of deficient bridges.

Through the Department's Strategic Planning Office, a PM2 Working Group was established consisting of staff from TDOT, FHWA, and the MPOs. The target setting process consisted of data review, trend analysis, consideration of key factors and assumptions, and review and consensus on draft targets. The Working Group provided recommendations to TDOT's Oversight Committee, which in turn presented finalized PM2 targets to the Executive Leadership for review and approval.

The PM2 Baselines and Targets are presented in the chart below. Additional information on TDOT's PM2 Targets can be found on FHWA's website at the link below:

<https://www.fhwa.dot.gov/tpm/reporting/state/state.cfm?state=Tennessee>

PM2 Bridge - TDOT Performance Measure Targets

PM2 Bridge - TDOT Performance Measure Targets			
Measure	Baseline	2-Year Target	4-Year Target
Percent of NHS Bridges by Deck Area in Good condition	39.5%	36.0%	36.0%
Percent of NHS Bridges by Deck Area in Poor condition	4.9%	6.0%	6.0%

PM2 Pavement - TDOT Performance Measure Targets

PM2 Pavement - TDOT Performance Measure Targets			
Measure	Baseline	2-Year Target	4-Year Target
Percent Good— Interstates	75.6%	N/A	60.0%
Percent Poor— Interstates	0.14%	N/A	1.0%
Percent Good— Non-Interstate NHS	44.8%	42.0%	40.0%
Percent Poor— Non-Interstate NHS	3.24%	4.0%	4.0%

System Performance Measures (PM3)

The System Performance Final Rule established performance measures for the purposes of assessing the Interstate and Non-Interstate National Highway System, freight movement on the Interstate System, and traffic congestion and on-road mobile source emissions. As with PM2, The Federal Highway Administration (FHWA) advises state Departments of Transportation (DOTs) to set realistic targets which are based on analysis of trends

and projections of future efforts. This is the first time DOTs have been tasked with setting targets for these measures and limited historical data exists.

TDOT utilizes a multidisciplinary group of individuals within different Divisions and Bureaus to ensure that the State's PM3 Performance Targets are an important factor in determining project priorities. PM 3 is a vital part of TDOT's operational and planning procedures to ensure that performance data trends towards our projected goals and targets.

As previously noted, TDOT's budget for the Operation & Management has increased over time. Realizing that roadway widening is not always a feasible solution to congestion, TDOT created a Traffic Operations Division. The purpose of this Division is to investigate, plan for, and fund projects and technologies that help to maximize the capacity and safety of the existing infrastructure. This occurs through the use of technological innovations, standardized traffic management procedures and practices, and strategic partnerships with local and state agencies. TDOT's TSMO budget primarily includes the maintenance and operational costs for the Intelligent Transportation Systems (ITS) across the state. However, it also includes the operation of the HELP truck program in Tennessee's four largest urban areas, which was created to minimize traffic congestion, promote the safe movement of people and products, and improve the travel environment through incident management procedures.

Through the Department's Strategic Planning Office, a PM3 Working Group was established at the same time as the PM2 Working Group, consisting of staff from TDOT, FHWA, and the MPOs. The target setting process consisted of data review, trend analysis, consideration of key factors and assumptions, and review and consensus on draft targets. The Working Group provided recommendations to TDOT's Oversight Committee, which in turn presented finalized PM3 targets to the Executive Leadership for review and approval.

The PM3 Baselines and Targets are presented in the chart below. Additional information on TDOT's PM3 Targets can be found on FHWA's website at the link below:

<https://www.fhwa.dot.gov/tpm/reporting/state/state.cfm?state=Tennessee>

TDOT PM3 System Performance - Performance Measure Targets

TDOT PM3 System Performance - Performance Measure Targets			
Measure	Baseline	2-Year Target	4-Year Target
Interstate Reliability	87.7%	85.3%	83.0%
Non-Interstate NHS Reliability	90.9%	n/a	87.5%
Freight Reliability	1.35	1.35	1.33
PHED	8.54	n/a	18.81
% NON-SOV	16.6%	16.5%	16.5%
Emissions	VOC = 230.025 CO = 530.282 NOx = 363.399 PM2.5 = 2.897	VOC = 30.698 CO = 75.000 NOx = 62.840 PM2.5 = 0.120	VOC = 61.396 CO = 150.000 NOx = 125.680 PM2.5 = 0.240

Transit Asset Management Performance Measures and Targets

Introduction

Performance measurement provides transportation agencies with a better ability to identify and mitigate issues with their respective transportation networks. The introduction of The Moving Ahead for Progress in the 21st Century Act (MAP-21) and Fixing America's Surface Transportation (FAST) Act requires various transportation agencies to take a performance-based approach to planning and programming activities. TDOT is currently coordinating with regional, State, and Federal partners to maintain compliance with Federal laws and regulations related to performance management. The integration of performance management regulations outlined in MAP-21 and the FAST Act into TDOT's plans and processes is an evolving effort.

Note: This section was prepared using the most recent transportation act, the FAST Act, and was developed with the intent of including performance metrics mandated by the most recent legislation. The FAST Act did not change national performance goals related to safety, infrastructure condition, congestion reduction, system reliability, freight movement and economic viability, environmental sustainability, and reduced project delivery delays, which were established as part of MAP-21.

Federal Requirements

In July 2016, FTA published a Final Rule for Transit Asset Management in accordance with the provisions of MAP-21 and the FAST Act. The Rule adds a new part 625, "Transit Asset Management," to title 49 of the Code of Federal Regulations, which establishes the National Transit Asset Management (TAM) System in the effort to monitor and manage public transportation capital assets to enhance safety, reduce maintenance costs, increase reliability, and improve performance. This rule establishes four State of Good Repair (SGR) performance measures for which recipients of Federal transit funding must establish performance targets for their capital assets. A State of Good Repair is defined as "the condition in which a capital asset is able to operate a full level of performance." The measures are:

- Equipment (non-revenue service vehicles)
- Rolling Stock
- Facilities
- Infrastructure (rail fixed-guideway, track, signals, and systems)

In concert with this rule, the FTA and FHWA published the Final Rule and Statewide and Nonmetropolitan Transportation Planning and Metropolitan Transportation Planning (The Joint Planning Final Rule) in May 2016. This rule requires all recipients of Federal-aid highway program or Federal transit funds to link the investment priorities contained in the Statewide Transportation Improvement Program (STIP) and all local Transportation Improvement Programs (TIPs) to the achievement of the performance targets established by these agencies.

As a recipient of Federal transit funds and the sponsor of Tier II Group TAM Plan (Group Plan), TDOT has established the following performance targets for report year 2020 (October 1, 2019 – September 30, 2020):

Rolling Stock

Rolling stock performance measure targets are for revenue vehicles.

Vehicle Type	FTA Default Useful Life Benchmark (ULB – in years)	TDOT-Set Performance Measure Target
Automobile	8	Less than 25% of automobiles will exceed the 8-year ULB
Bus	14	Less than 25% of buses will exceed the 14-year ULB
Cutaway Bus	10	Less than 25% of cutaway busses will exceed the 10-year ULB
Other Rubber Tire Vehicles	14	Less than 25% of other rubber tire vehicles will exceed the 14-year ULB
Van	8	Less than 25% of vans will exceed the 8-year ULB

Equipment

Equipment performance measure targets are for non-revenue service vehicles.

Vehicle Type	FTA Default Useful Life Benchmark (ULB - in years)	TDOT-Set Performance Measure Target
Automobile	8	Less than 25% of automobiles will exceed the 8-year ULB
Other Rubber Tire Vehicles	14	Less than 25% of other rubber tire vehicles will exceed the 14-year ULB

Facilities

Facility Type	FTA TERM	TDOT-Set Performance Measure Target
Administrative/Maintenance	3	Less than 25% of Administrative/Maintenance facilities will be below a 3
Passenger/Parking	3	Less than 25% of Passenger/Parking facilities will be below a 3

*None of the participants in the TDOT-sponsored plan have capital responsibility for any transit infrastructure.

The Tennessee Department of Transportation (TDOT) acknowledges the importance of implementing performance based planning within the Statewide Transportation Improvement Program that reflects federal, state and regional priorities. The Department maintains a comprehensive inventory of the assets of each public transportation agency participating in the Group Plan, listing assets by condition by agency. TDOT uses this inventory (Appendix C of the Group Plan) to prioritize vehicles for replacement by each agency in order to achieve the established Performance Measurement targets.

Link to Transit Asset Management Plan and Targets

[Transit Asset Management Performance and Targets](#)

Chapter 6: Performance Goals & Targets

What are Performance Goals & Targets?

TDOT has historically tracked the condition of pavements and bridges throughout the state in order to evaluate the transportation system's performance. Performance measures and targets were established based on the operations, future conditions, and maintenance of the roadway system in conjunction with customer input. These performance measures have served as a good basis for TDOT to determine investment strategy, funding amounts, and project identification and provide a good foundation for the TAMP.

The national performance management measures and targets required by MAP-21 to address the condition of pavements and bridges on both the interstate system and the NHS are discussed in this chapter. TDOT has defined specific performance targets that constitute the agency's state of good repair (SOGR) for pavement and bridges on the NHS. In addition to these requirements, TDOT has established performance measures and targets for state-owned pavement and bridges not on the NHS.

Establishing performance measures and targets is fundamental to creating an asset management plan that supports the management and performance of the NHS, as well as to identify the need for preservation, maintenance, rehabilitation, or construction of new facilities. Tracking measurable conditions for pavements and bridges in relation to targets is a useful tool for TDOT to determine if the agency's goals for performance are being achieved at a network level as well as at a regional or a local level. It is also a transparent tool for TDOT to identify where funds benefit the NHS both on and off interstates.

TDOT tracks pavement and bridge conditions in a pavement management system and a bridge management system. The historic condition for each of the measurable conditions tracked is shown in Chapter 2. For pavement metrics, TDOT collects data based on ride quality (Pavement Serviceability Index) and condition (Pavement Distress Index). These two indexes are consolidated to calculate a Pavement Quality Index (PQI) which is used to gauge the overall condition of pavements. The schedule for pavement evaluation is annually on the interstate and NHS state routes and bi-annually on non-NHS state routes. For bridges, TDOT tracks the sufficiency rating of the bridge which is determined from the condition of the bridge deck, superstructure, and substructure. For large culverts (greater than 20' along the centerline of the highway), TDOT tracks the overall condition. Bridges (including large culverts) are inspected biennially.

It is important to note that TDOT historically meets or exceeds the national performance minimum standards established by MAP-21 for pavement and bridge conditions, as will be shown in the following sections of this chapter.

What are the National Performance Management Measures & Minimum Standards for Pavements & Bridges?

Through MAP-21, national performance goals have been established for pavements and bridges to maintain the condition of these assets in a state of good repair. The National Performance Management Measures for pavements identified in 23 CFR Part 490 have established four (4) measures to assess pavement condition:

PAVEMENTS

- 1) Percentage of pavements (Lane Miles) on the interstate system in Good condition,
- 2) Percentage of pavements (Lane Miles) on the interstate system in Poor condition,
- 3) Percentage of pavements (Lane Miles) on the NHS (excluding the interstate system) in Good condition, and
- 4) Percentage of pavements (Lane Miles) on the NHS (excluding the interstate system) in Poor condition.

Within the national rule, performance ratings of good, fair, and poor condition for pavements have been established by FHWA based on a combination of several metrics typically collected by every state DOT including TDOT. FHWA will use these metrics to quantify the condition of pavements in terms of roughness (IRI), cracking, rutting (asphalt) and faulting (concrete). Table 3-1 below summarizes the metrics and the performance ratings, as identified by FHWA.

METRIC	PAVEMENT TYPE	GOOD	FAIR	POOR
IRI	ALL	<95	95 to 170	>170
Cracking	Asphalt	<5%	5% to 20%	>20%
Cracking	Jointed Concrete	<5%	5% to 15%	>15%
Cracking	CRCP	<5%	5% to 10%	>10%
Rutting	Asphalt	<0.20"	0.20" to 0.40"	>0.40"
Faulting	Jointed Concrete	<0.10"	0.10" to 0.15"	>0.15"

Table 3-1: FAST Act Pavement Metrics and Performance Ratings

Using this criterion, an asphalt pavement is considered to be in good condition only if all three (3) metrics, consisting of IRI, percent cracking, and rutting, meets the criteria for good. The pavement is considered to be in poor condition if any two (2) of the three (3) metrics (IRI, percent cracking, and rutting) are determined to be in poor condition. Finally, the pavement is classified as fair if it doesn't meet the criteria of the good or poor conditions.

Similarly, a jointed concrete pavement is considered to be in good condition only if all three (3) metrics, consisting of IRI, percent cracking, and faulting, meets the criteria for good. The pavement is considered to be in poor condition if any two (2) of the three (3) metrics (IRI, percent cracking, and faulting) are determined to be in poor condition. Finally, the pavement is classified as fair if it doesn't meet the criteria of the good or poor classification.

Continuously Reinforced Concrete Pavement (CRCP) is evaluated only on two (2) metrics; IRI and cracking. CRCP is considered to be in good condition if both metrics of IRI and cracking is determined to meet the criteria for good. It is considered to be in poor condition if both IRI and cracking is determined to meet the criteria for poor. It is considered to be in fair condition if it doesn't meet the criteria of the good or poor classification. The following Table 3-2 provides a summarization of this information along with the applicable federal rule, and the minimum standard for interstate pavements.

Rule	23 CFR Part 490.313 (c)				23 CFR Part 490.315(a)
Pavement Type	Metrics	Good	Poor	Fair	Minimum Standard (Interstate)
Asphalt	IRI, Cracking, Rutting	All 3 = Good	2 of 3 = Poor	All other combinations	<5% in Poor condition
Jointed Concrete	IRI, Cracking, Rutting	All 3 = Good	2 of 3 = Poor	All other combinations	<5% in Poor condition
CRCP	IRI, Cracking	All 2 = Good	2 of 2 = Poor	All other combinations	<5% in Poor condition

Table 3-2: FAST Act Good/Fair/Poor Determination for Interstate Pavements and Minimum Standard

In order to give state and local agencies time to modify the way they collect pavement condition data to meet these collection standards, the national rule provides for a transition period. State DOTs will only be measured based on IRI rating until after the data collection cycle ending December 31, 2018 for interstate highways and December 31, 2020 for the non-interstate NHS. After these dates, state DOTs will be evaluated based on the metrics identified in Table 3-2, and will also be required to limit the portion of their inventory data that is missing, invalid, or unresolved to no more than five (5) per- cent.

BRIDGES

The process for determining the condition of bridges is similar in concept to that for pavements. The national performance management measures for bridges identified in 23 CFR Part 490 have established three (3) classifications for the purpose of assessing bridge condition (based on square foot of deck area):

- 1) Percent of NHS bridges classified as in good condition,
- 2) Percent of NHS bridges classified as in fair condition, and
- 3) Percent of NHS bridges classified as in poor condition.

Within the national rule, performance ratings of good, fair, and poor condition for bridges have been established by FHWA based on a combination of three (3) metrics that are collected by every state DOT including TDOT. FHWA will use these metrics on a 0 to 9 condition scale to quantify the condition of bridges in terms of bridge deck, superstructure, and substructure. Culverts will be evaluated based on their overall condition. The following Tables 3-3 and 3-4 summarize the metrics and the performance ratings.

Condition is determined by the lowest rating of deck, superstructure, substructure, or culvert. If the lowest rating is greater than or equal to 7, the bridge is classified as good; if the lowest rating is less than or equal to 4, the classification is poor. Bridges rated below 7 but above 4 will be classified as fair; there is no related performance measure.

Component	GOOD	FAIR	POOR (Structurally Deficient)
Deck	7 to 9	5 to 6	0 to 4
Superstructure	7 to 9	5 to 6	0 to 4
Substructure	7 to 9	5 to 6	0 to 4
Culverts	7 to 9	5 to 6	0 to 4

Table 3-3: FAST Act Components and Performance Ratings

Rule	23 CFR Part 490.409(b)				23 CFR Part 490.411(a)
Structure Type	Component	Good	Poor (Structurally Deficient)	Fair	Minimum Standard (NHS bridges)
Bridge	Deck, Super-structure, Sub-structure	All Components = Good	1 or more Components = Poor	All other combinations	< = 10% of total deck area rated as POOR (Structurally Deficient)
Culvert	Overall Condition Rating	Rating = Good	Rating = Poor	Rating = Fair	

Table 3-4: FAST Act Good/Fair/Poor Determination for NHS Bridges and Minimum Standard

What are TDOT’s Targets for the National Performance Management Measures for Pavements and Bridges?

TDOT has established performance targets for the National Performance Management Measures identified in 23 CFR Part 490 as indicated in Table 3-5. An Oversight Committee consisting of key TDOT managers was established to provide oversight and coordination for implementation of all MAP-21 and FAST Act final rules, including development of performance targets.

Asset	System	% Good			% Poor		
		2-year	4-year	Baseline	2--year	4-year	Baseline
Pavements	Interstate	N/A	>60%	N/A	N/A	<1%	N/A
	Non-Interstate NHS	>40%	>40%	44.8%	<4%	<4%	3.2%
Bridges*	NHS (Interstate and non-Interstate)	>36%	>36%	39.5%	>36%	>36%	39.5%

Table 3-5: TDOT National Performance Management Targets

Basis for Non-Interstate National Highway System (NHS) Pavement in Good Condition:

Non-interstate targets are based on the "full measure" as defined in 23 CFR 490.313(c), not IRI alone as defined in paragraph e of the same section. Targets were established per the full measure on time in accordance with the law as defined in 23 CFR 490.105(e). Using the "full measure", TDOT estimated a baseline value of 44.8%. Performance projections of the full measure at current funding levels extended below what TDOT considers an acceptable state of good repair, so a minimum target was selected within range of TDOT's historical state of good repair.

Performance projections using IRI alone, in which projected decline is much less severe, indicate non-interstate NHS % Good will be 72.8% at the mid-performance period and 72.2% by the end of the performance period.

Basis for Non-Interstate National Highway System (NHS) Pavement in Poor Condition:

Non-interstate targets are based on the "full measure" as defined in 23 CFR 490.313(c), not IRI alone as defined in paragraph e of the same section. Targets were established per the full measure on time in accordance with the law as defined in 23 CFR 490.105(e) and may be adjusted during the mid-performance period. Using the "full measure", TDOT estimated a baseline value of 3.2%. Based on network analysis of the full measure using pavement management data, it is expected that values for % Poor will stay within reasonable range of recent historical observations. Thus, targets were set within a similar range.

Historical calculations of using IRI alone indicate a gradual increase in %Poor. Projects for 2019 and 2021 % Poor using IRI alone are 7.0 and 7.3%, respectively.

How has TDOT Defined State of Good Repair for Pavement and Bridges?

TDOT has a long-standing history of maintaining the state's pavement and bridges in good condition, which are serviceable to Tennesseans based on the traffic they serve. The agency's long-term goals are to maintain pavement and bridges in a state of good repair throughout the asset's life time at the lowest possible cost.

TDOT has established long-term performance targets for pavements and bridges based on their importance and functional need. For example, interstate highways are the most important facilities since they provide the backbone for the movement of people, freight, and commerce within the state as well as across the nation. Historically, TDOT has not differentiated between state routes that are on the NHS and those that are not part of the NHS; however, this will change in the near future, and TDOT will ensure that NHS routes are meeting the established targets for that system. The following Tables, 3-6 and 3-7, provide the state of good repair performance measures and targets for the agency's pavements and bridges based on highway system. It should

be noted that for bridges, TDOT has established the same performance measures and targets for the state’s SOGR as for the national performance management measures.

Asset	System	Performance Measure	Good	Poor
Pavements	Interstate	PQI	PQI >4.0	PQI <2.0
	Non-Interstate NHS	PQI	PQI >3.5	PQI <2.0
	Non-NHS State	PQI	PQI >3.5	PQI <2.0
Bridges*	Interstate	Condition ratings for Deck, Superstructure, Substructure	All three ≥7	One or more ≤4
	Non-Interstate NHS	Condition ratings for Deck, Superstructure, Substructure	All three ≥7	One or more ≤4
	Non-NHS State	Condition ratings for Deck, Superstructure, Substructure	All three ≥7	One or more ≤4

*Table 3-6: State of Good Repair Performance Measures *(Based on square feet of bridge deck)*

Asset	System	Good	Poor
Pavements	Interstate	>50%	<0.5%
	Non-Interstate NHS	>45%	<0.5%
	Non-NHS State	>45%	<0.5%
Bridges*	Interstate	>36%	<6%
	Non-Interstate NHS	>36%	<6%
	Non-NHS State	>36%	<6%

*Table 3-7: TDOT State of Good Repair Targets *(Based on square feet of bridge deck)*

What Is The Gap Between Pavement Performance And Targets?

As described previously, TDOT tracks Pavement Quality Index (PQI) for asphalt and concrete to determine the condition of the roadways. The PQI is a composite index number based primarily on the ride quality of the

pavement, (Pavement Smoothness Index), and the condition of the pavement, (Pavement Distress Index), and is measured on a 0 to 5 scale. A pavement on the interstate system with a PQI greater than 4.0 is in good condition, while a pavement with a PQI of less than 2.0 is in poor condition. Similarly, pavements on non-interstate NHS and non-NHS state routes with a PQI greater than 3.5 are considered to be in good condition and pavements with a PQI less than 2.0 is in poor condition.

Using these characteristics, pavement performance is calculated and reported per number of lane miles. These results are used to assist the department in determining funding amounts, allocations to the four TDOT regions, and choosing the appropriate work types to minimize whole-life cost, i.e. a combination of maintenance, preservation, rehabilitation, or reconstruction needed for the roadways.

Figures 3-1 through 3-4 below show the PQI rating for each system of roadway from 2012 to 2018. As shown, in 2018, 52% of lane miles on the interstates had a PQI >4.0, which is above the SOGR target of 50%. In 2018, 54.7% of NHS state routes and 47.3% of non-NHS state routes had a PQI >3.5, which are both above the SOGR target of 45% for non-interstate state routes. Local MPO’s who own routes on the NHS have agreed to accept TDOT’s targets for State of Good Repair, thus the same target has been shown in Figure 3-3 for locally owned NHS routes.

Figure 3-1: Historical Pavement Performance Rating and Target on Interstates

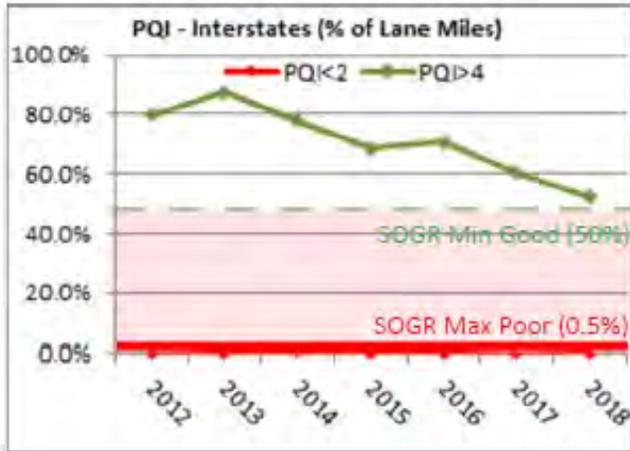


Figure 3-2: Historical Pavement Performance Rating and Target on NHS State Routes

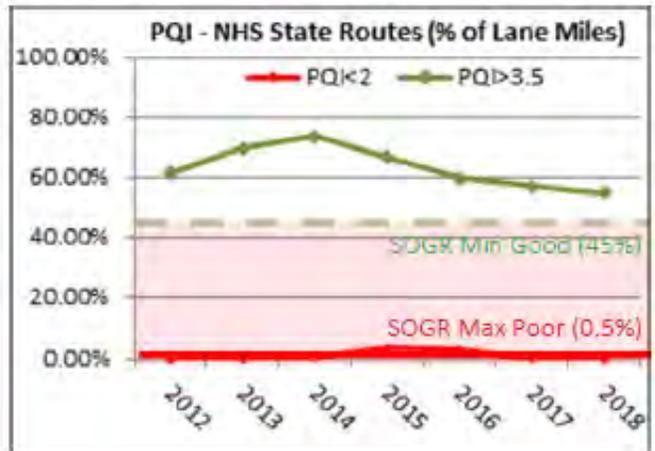


Figure 3-3: Historical Pavement Performance Rating and Target on NHS Local Routes

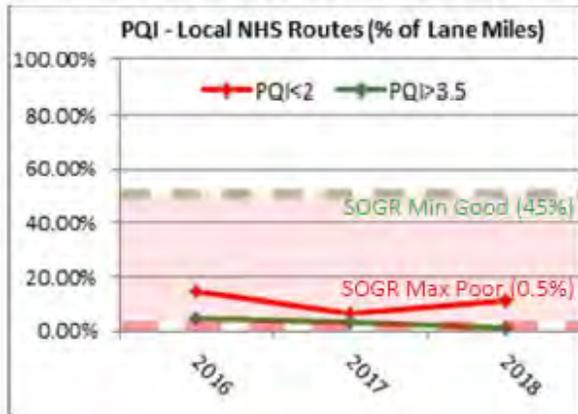
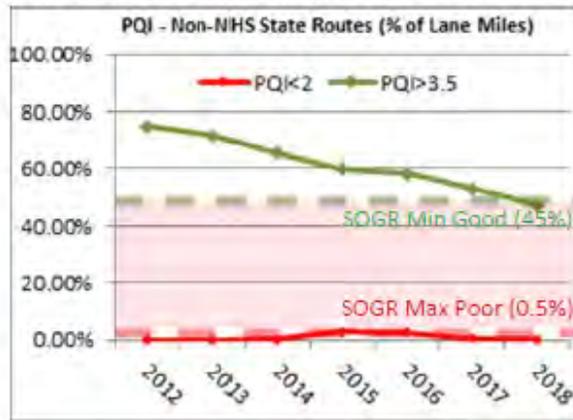


Figure 3-4: Historical Pavement Performance Rating and Target on Non-NHS State Routes



What Is The Gap Between Bridge Performance And Targets?

Since TDOT has established a dependable bridge management process using the NBIS inspection reports to determine program and project needs, the department will be able to make a smooth transition to the TAMP requirements. The inspection program requires an in-depth evaluation of the deck, substructure, and superstructure for bridges, and key features of large culverts based on the national bridge inspection standards. The results from the inspections are used to determine the type of work activity required for the bridge or large culvert, i.e. maintenance, preservation, rehabilitation, or replacement.

Although the overall condition of the bridge is reported in the sufficiency rating, the condition of the deck, substructure, and superstructure may trigger work needed to preserve the bridge and extend the life cycle of the structure. The target value for each of the metrics is six (6) or higher. If it is lower than a six (6), the bridge becomes a candidate for a preservation treatment or some type of maintenance plan to address the deficiency. If the condition is less than a five (5), other major work type is considered such as rehabilitation or even replacement of the entire structure.

The sufficiency rating which reflects the compilation of the deck, substructure, and superstructure evaluation or the large culvert evaluation is used to determine the maintenance or preservation method needed for the bridge or large culvert. The following Figure 3-5 shows the sufficiency rating for the bridge inspections conducted in 2016-2018 on each system. As shown, in 2018 there were 2.9% of interstate bridges rated as poor, 3.8% on the non-interstate NHS state routes, 8.4% on the NHS local routes, 0% on federal routes, and 2.4% on non-NHS state routes. TDOT’s bridges are within the agency’s SOGR targets of 36% in good condition with no more than 6% of all state-owned bridges in poor condition. TDOT has not previously set any targets for federal or locally-owned bridges, thus no target is shown for those systems. In terms of how Tennessee’s bridges compare with the national performance minimum standard (<= 10% of deck area rated poor), it is noted that only 4.9% of all bridges on the NHS are rated poor and well within the agency’s targets of 36% in good condition and no more than 6% in poor condition.

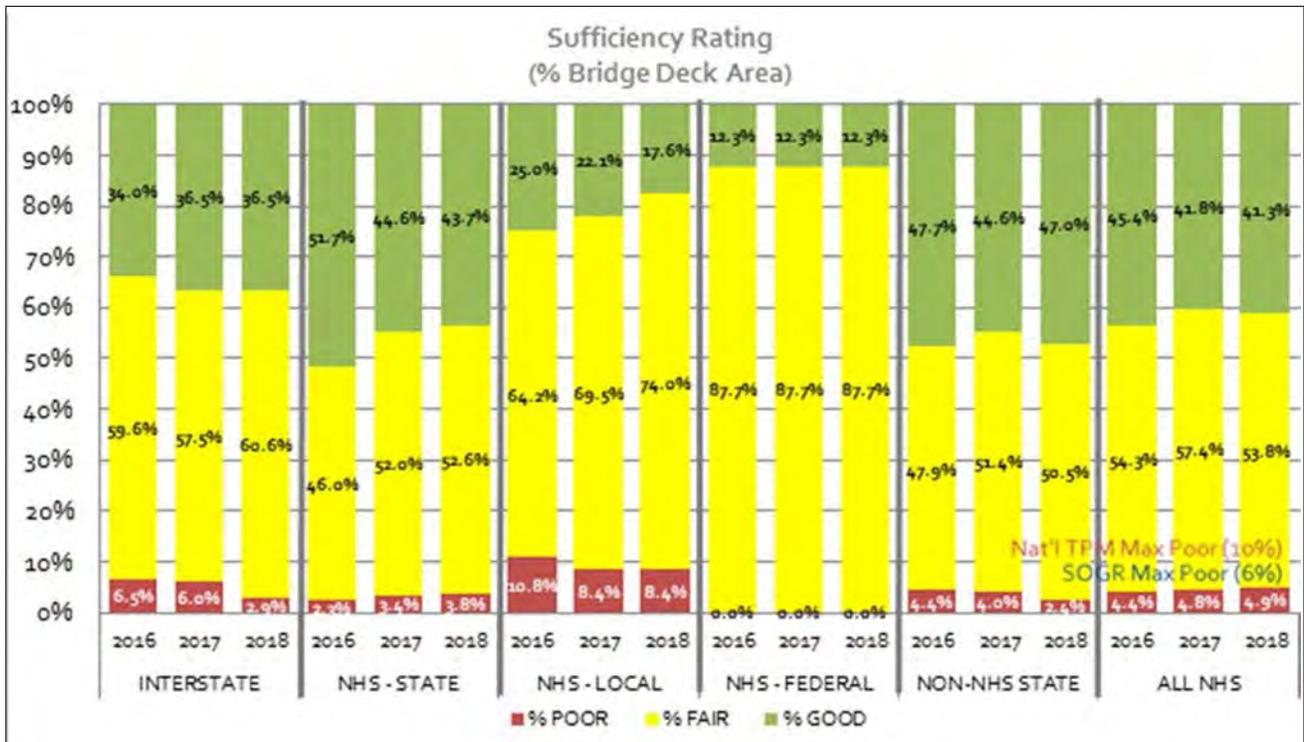


Figure 3-5: Bridge Condition Rating

How Does TDOT Stay Ahead of the Performance Targets?

As described by the performance measures and targets, TDOT is currently meeting or exceeding the federal minimum performance standards for NHS pavements and bridges. To enhance TDOT's ability to maintain this high standard of bridge conditions that have been historically established the agency has recently implemented a new bridge management system (BMS). The new BMS will assist the agency in predicting the future needs to preserve the system and maximize the use of their assets at minimum cost. The BMS is used to track the metrics of the bridges and large culverts as described in Chapter 2. This same system can be used to evaluate future needs through life cycle analysis. Similarly, the Pavement Management System (PMS) is the engine that stores the results of the pavement condition survey and provides the analysis to assist TDOT managers with the information and data to develop pavement management programs to meet TDOT's goals and objectives using life cycle cost processes discussed more detail in Chapter 4.

It is difficult to predict what will happen over the course of the next ten (10) years and even more difficult to predict future traffic growth on a statewide level. While there is no perfect method for prediction of the future growth, traffic models are used to provide the best possible information for growth scenarios. The industry standard for a small study area is to review the historical growth in an area and assume the same amount of growth continues for the foreseeable future. However, to predict traffic growth for a ten-year horizon statewide, the statewide model was reviewed to predict growth for specific metropolitan areas in the state and for the remaining rural areas of Tennessee.

The percentage of growth expected to be seen in the next ten (10) years is shown in the table below:

Area	10 Year Growth Rate (Statewide Model)
Greater Chattanooga	0.9%
Greater Knoxville	1.1%
Jackson	1.0%
Memphis	0.9%
Middle TN	1.5%
Tri-Cities	0.8%
Areas Outside MPO's	1.1%

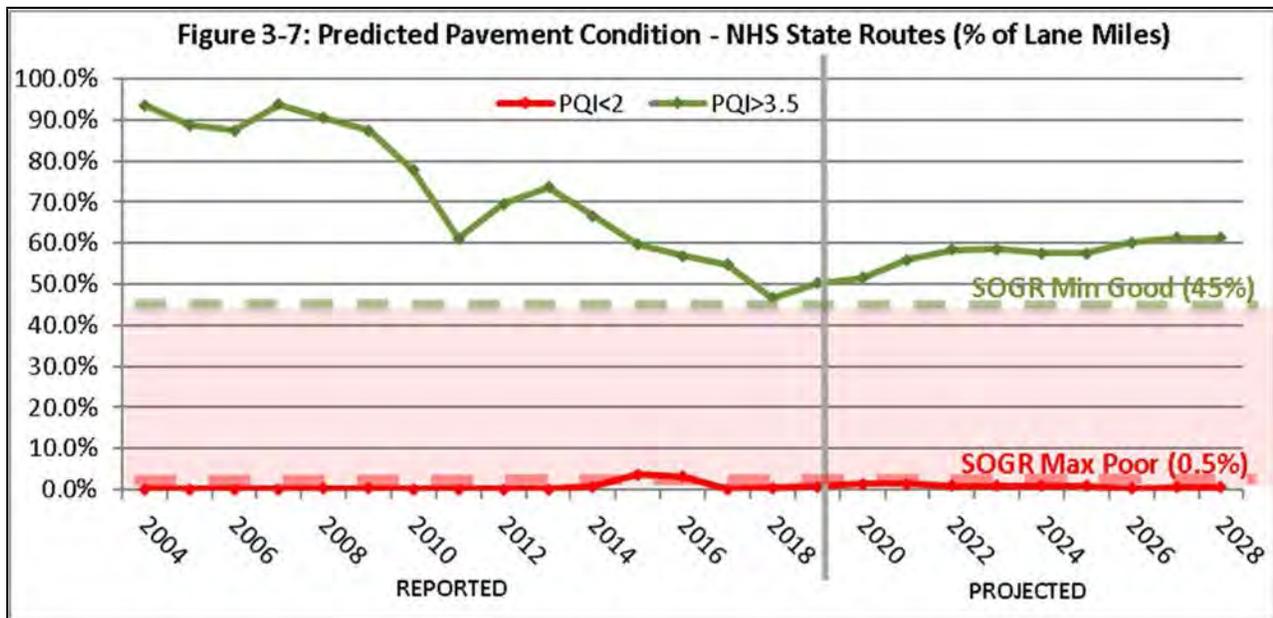
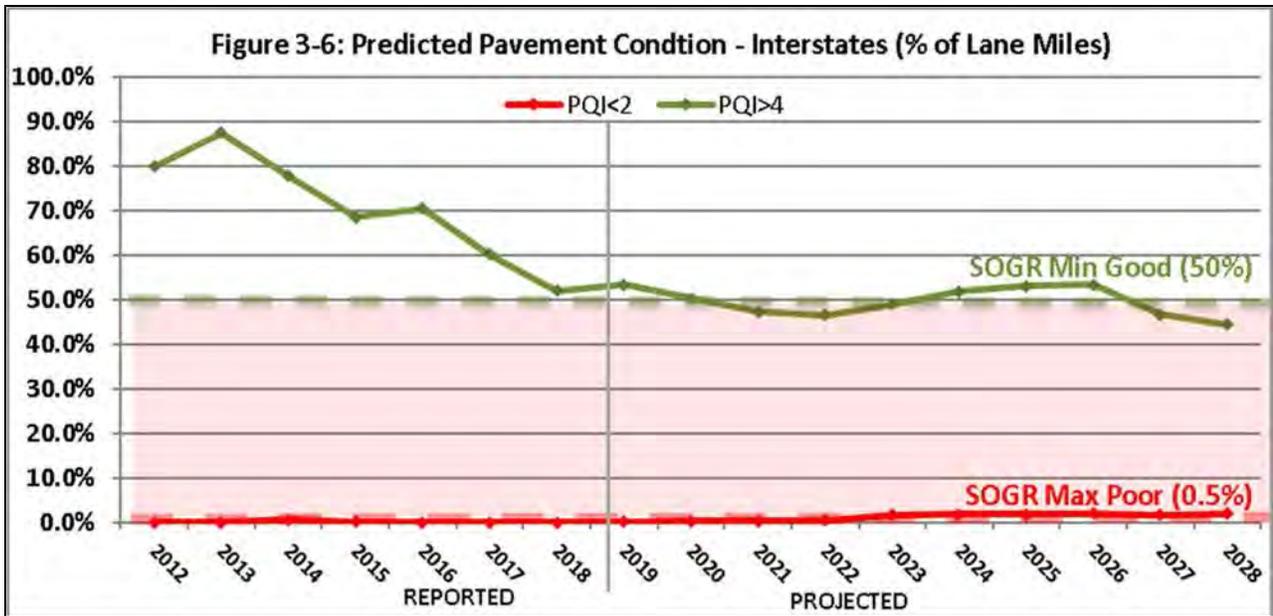
Table 3-8: Growth Rate Table

These growth rate factors can be applied to each area of Tennessee using the PMS and BMS to help with the future analysis of the pavement and bridge conditions. The department can use this analysis to plan for maintenance and repair of the pavement and bridges over the next ten (10) years.

What is TDOT's Predicted Pavement Condition (10 years)?

Using the PMS, TDOT has projected the percentage of lane miles in good and poor condition for the years 2018 – 2028 on each of the route systems shown in Figures 3-6 through 3-9 below.

Figure 3-6 shows that, with current available funding levels (\$66 million), the pavement conditions for the interstate system are expected to dip slightly below TDOT's target of at least 50% of lane miles with a PQI > 4.0 over the next 10 years. Figure 3-7 shows how the pavement condition is predicted to remain above the minimum of 45% of lane miles with a PQI > 4.0 over the next 10 years.



PROJECTED

Figure 3-8 shows historical pavement condition on the local NHS routes, but does not include any prediction for future years. This is because TDOT does not have access to construction history on local NHS routes, which is required for predicting performance. TDOT is working with MPOs to gather this information and will project future performance in later versions of the TAMP.

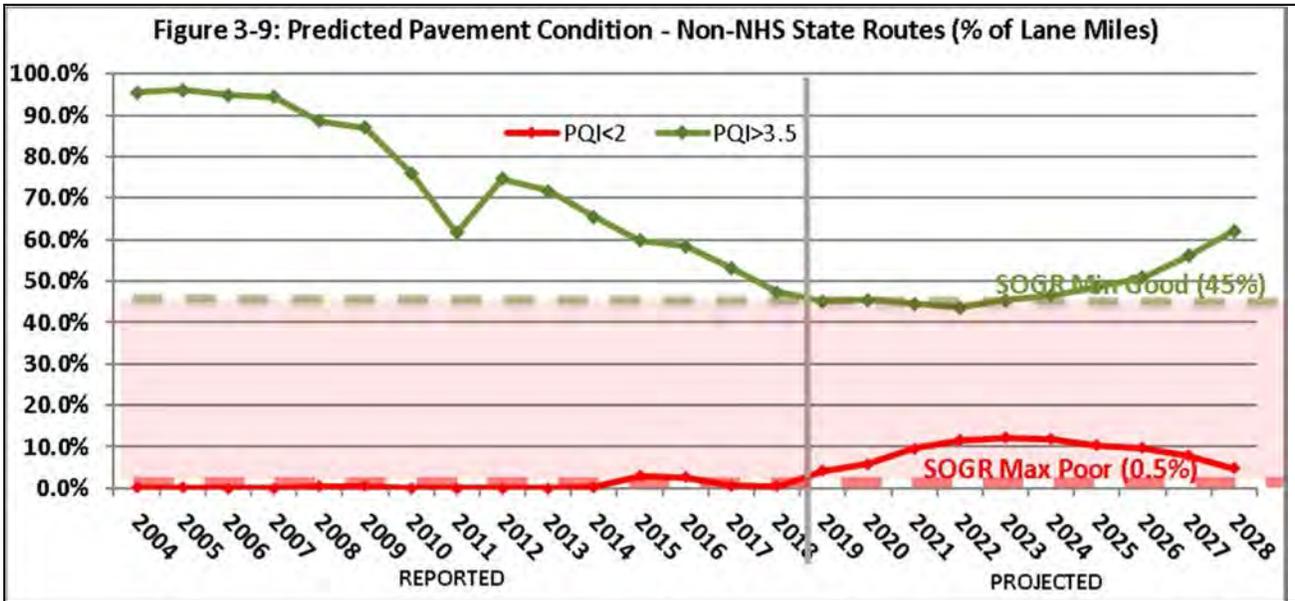
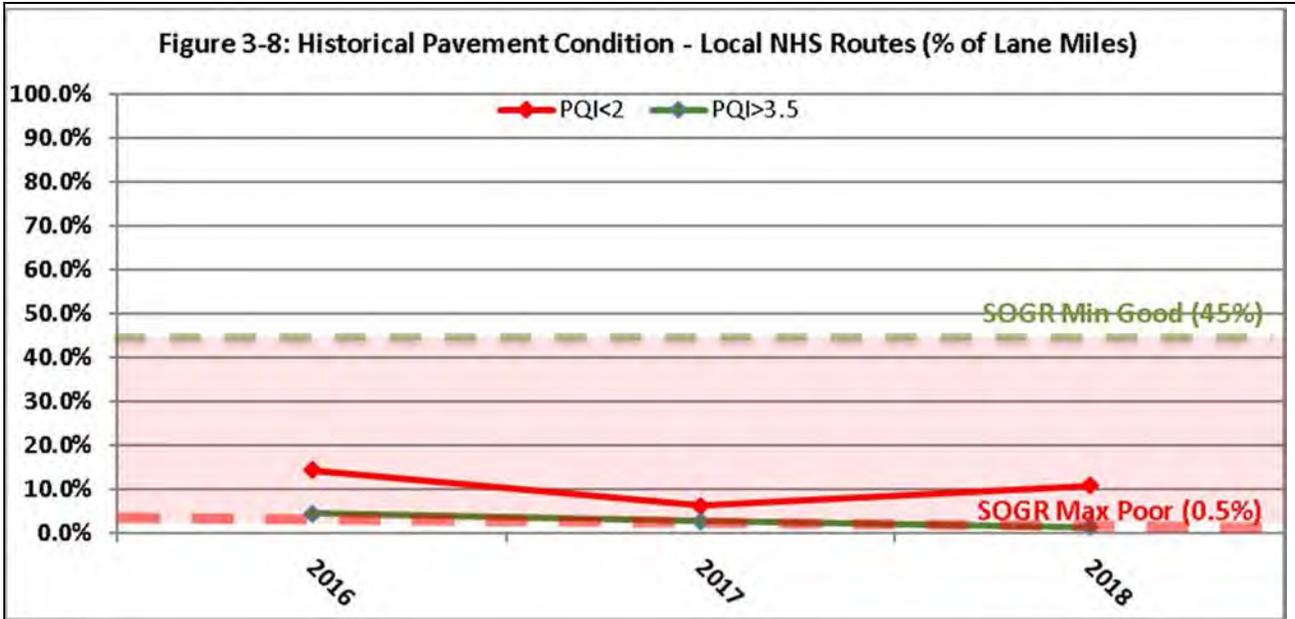


Figure 3-9 contains a projection of the performance of pavements on state routes that are not part of the NHS. Although this group makes up the majority of the lane miles in TDOT’s inventory (64%), this data indicates that the condition has historically been well above the target and is expected to remain at or above the target for the majority of years within the analysis period. The percentage of non-NHS state routes with a PQL<2.0 is projected to get as high as 12% at current funding, which is far above TDOT’s target of 0.5%.

Pavement management analyses can be conducted many different ways, each with their own particular benefits and inaccuracies. The figures shown in this chapter are designed to maximize cost-effectiveness of treatment selections. By the analysis shown, TDOT's interstate and NHS routes are projected to remain within the SOGR targets for % Good with current funding, but projections indicate targets may likely not be met for % Poor on non-NHS routes. An alternative analysis adjusted to meet % Poor targets, known as a "worst first" approach, produces projections where TDOT does not meet its targets for % Good. A more likely reality is that a combination of the two selection approaches – maximizing cost-effectiveness while minimizing the percentage of "Poor" roads – will result, but a combined analysis such as this is not currently possible. The Department is currently working to improve analyses in an effort to generate realistic output that confidently assesses whether increased funding is required. Concurrently, processes are being implemented to ensure pavement management predictions are properly being utilized as a resource for project selection. An assessment will be made annually on the Department's confidence with performance prediction. Once enough confidence is gained in performance predictions and those analyses properly verify the Department is efficiently selecting projects, a funding needs assessment will be made.

While those state routes that are not part of the NHS may not carry as much traffic as those designated as NHS routes, they still carry a fairly substantial portion of vehicular traffic in the state, and are an important part of our transportation network. While this decision may not have implications regarding TDOT's ability to comply with MAP-21 requirements, it is still an important investment decision for the agency going forward, which could impact how state dollars are invested in other areas of concern (safety, bridges, capacity, transit, etc.).

What is TDOT's Predicted Bridge Condition (10 years)?

Since TDOT's BMS does not yet have the capability of forecasting bridge condition, the department has chosen to predict the condition of the bridges, from 2019 to 2028, using a straight-line projection. TDOT continues to refine the condition forecasting capabilities of its BMS to improve their ability to predict the condition of the bridges over time, based on various funding scenarios. The results of the current straight-line average forecast are broken down for each facility type in Figures 3-10 through 3-15.

Figure 3-10 shows that the percentage of interstate bridges in poor condition is projected to remain below 4% over the next ten (10) years, which is well below the national performance minimum standard of no more than 10% in poor condition and also meeting TDOT's SOGR target of less than 6% poor and at least 36% good. Figure 3-11 shows that all NHS bridges are also expected to meet these targets, coming in at under 6% poor each year.

Figure 3-10: Predicted Bridge Condition - Interstates (% of Deck Area)

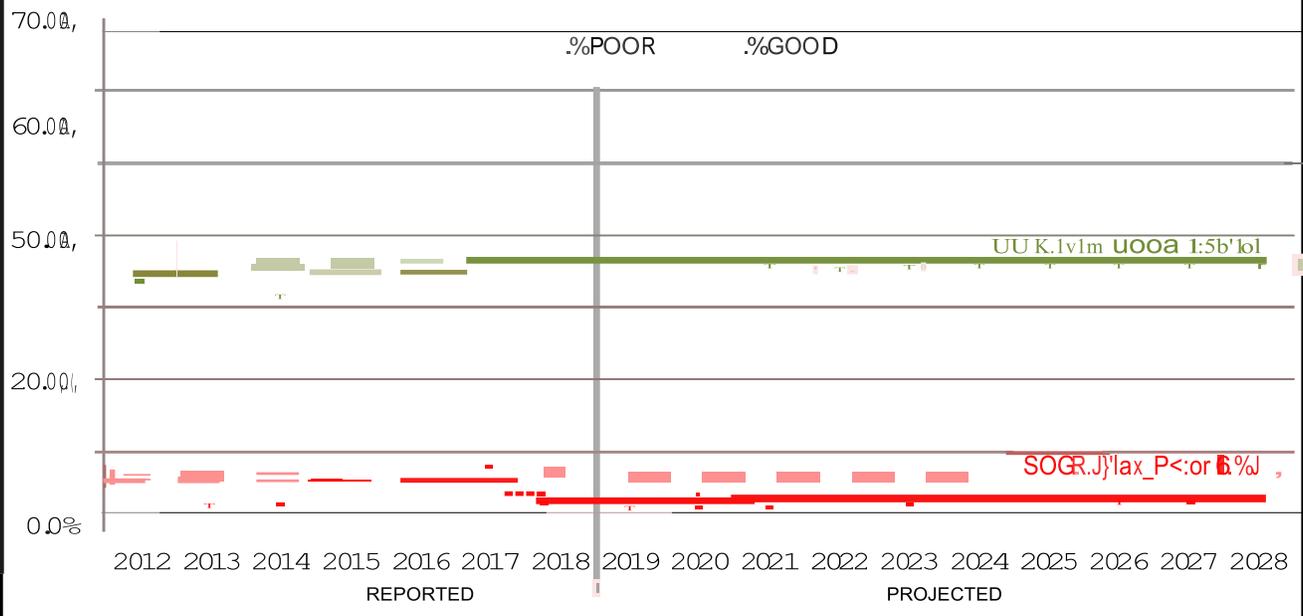
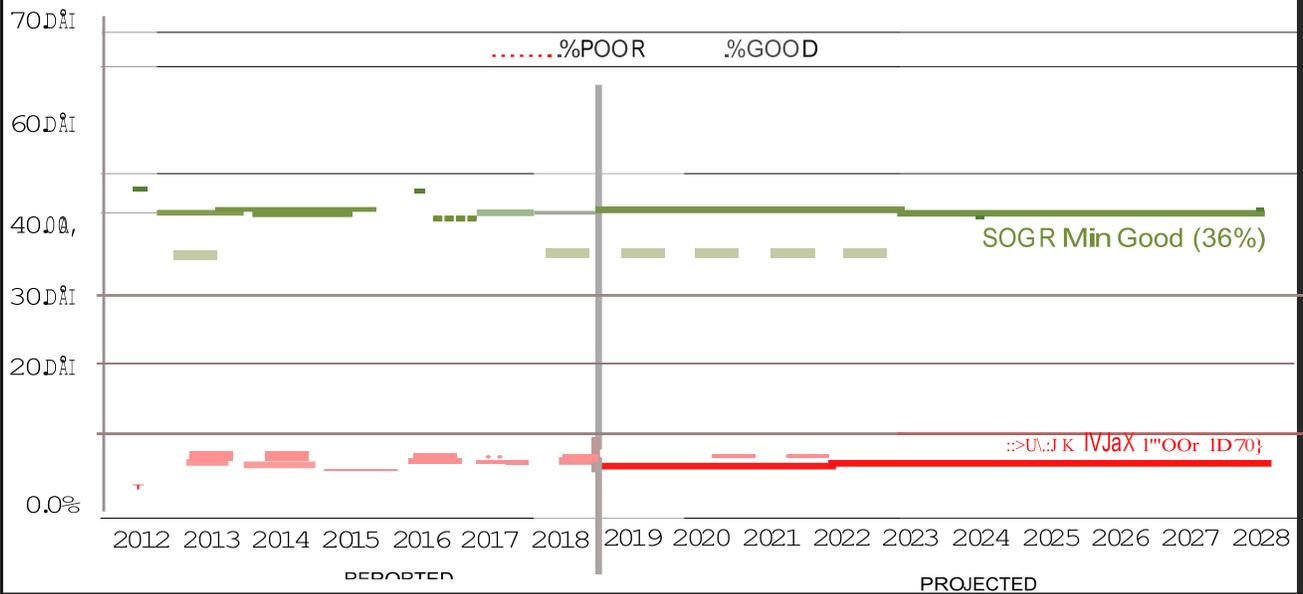
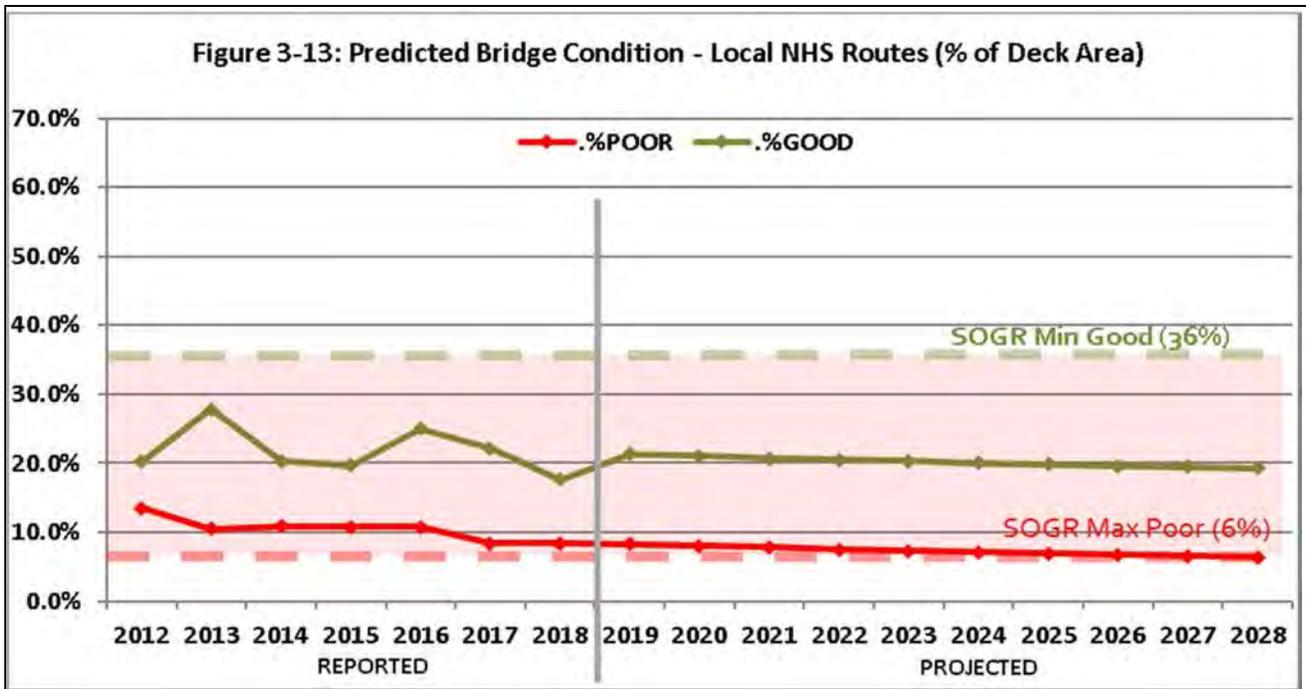
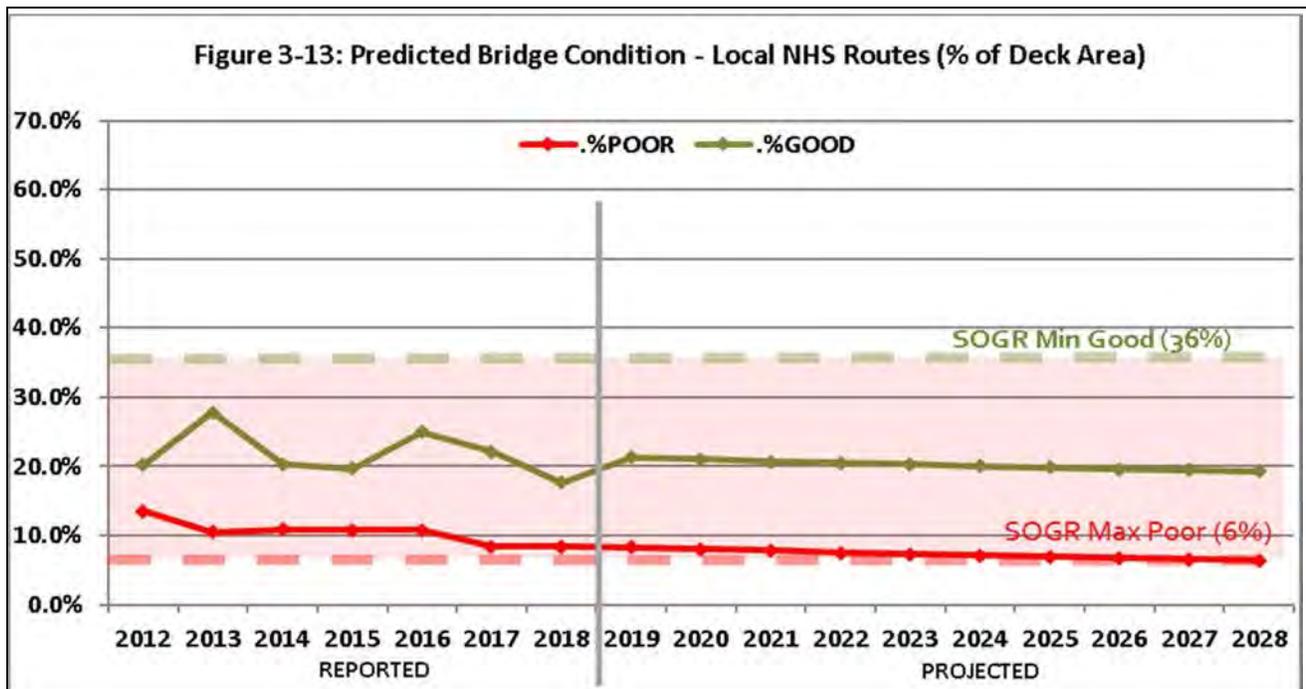


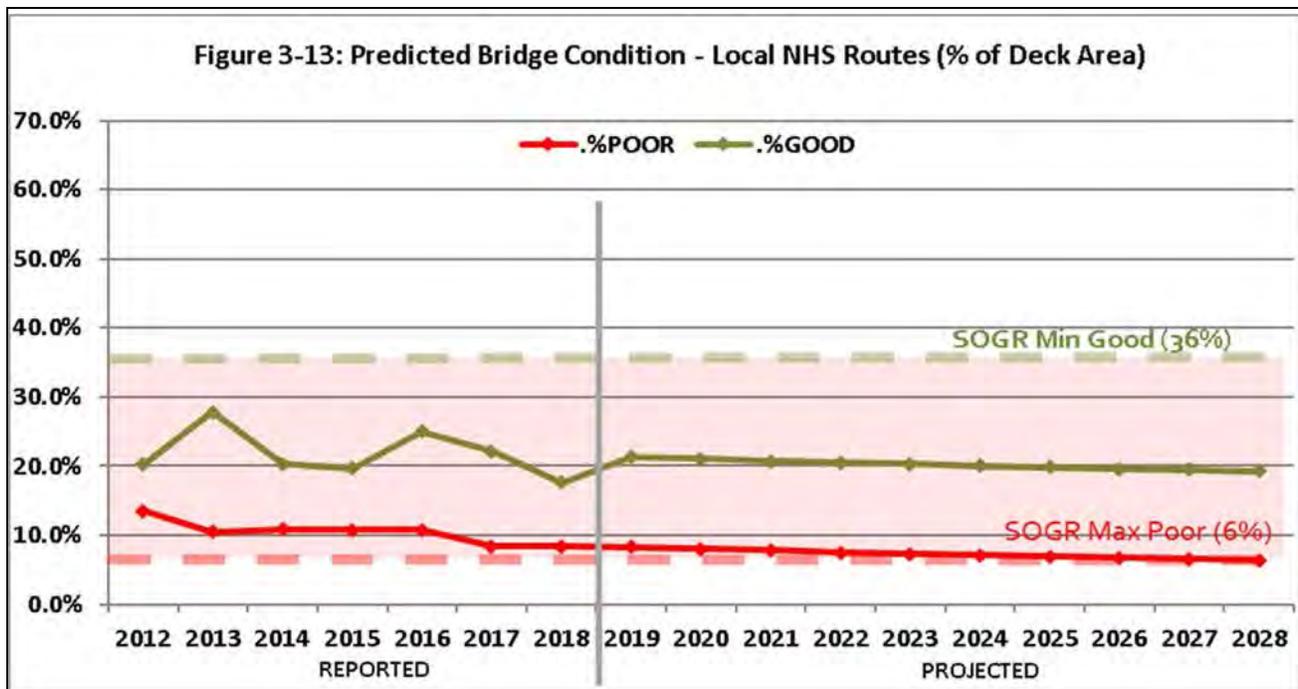
Figure 3-11: Predicted Bridge Condition - All NHS Routes (% of Deck)





In Figure 3-12, state-owned Non-NHS bridges are predicted to remain below 4% poor, while in Figure 3-13, local NHS bridges are anticipated to decrease from 8.4% to 6.4% over the same ten (10) year period. Local agencies have elected to accept the state DOT's state of good repair targets of 36% good and 6% poor bridge deck area.





The estimated funding to achieve these SOGR targets for bridges is approximately \$136 million per year. Around \$85 million per year is expected to fund construction of replacement of deficient bridges, while approximately \$40 million will be used for design and construction costs for rehabilitation and repair projects and \$4.4 million will be reserved for bridge preservation treatments. The remaining \$5 million will fund the bridge maintenance program.

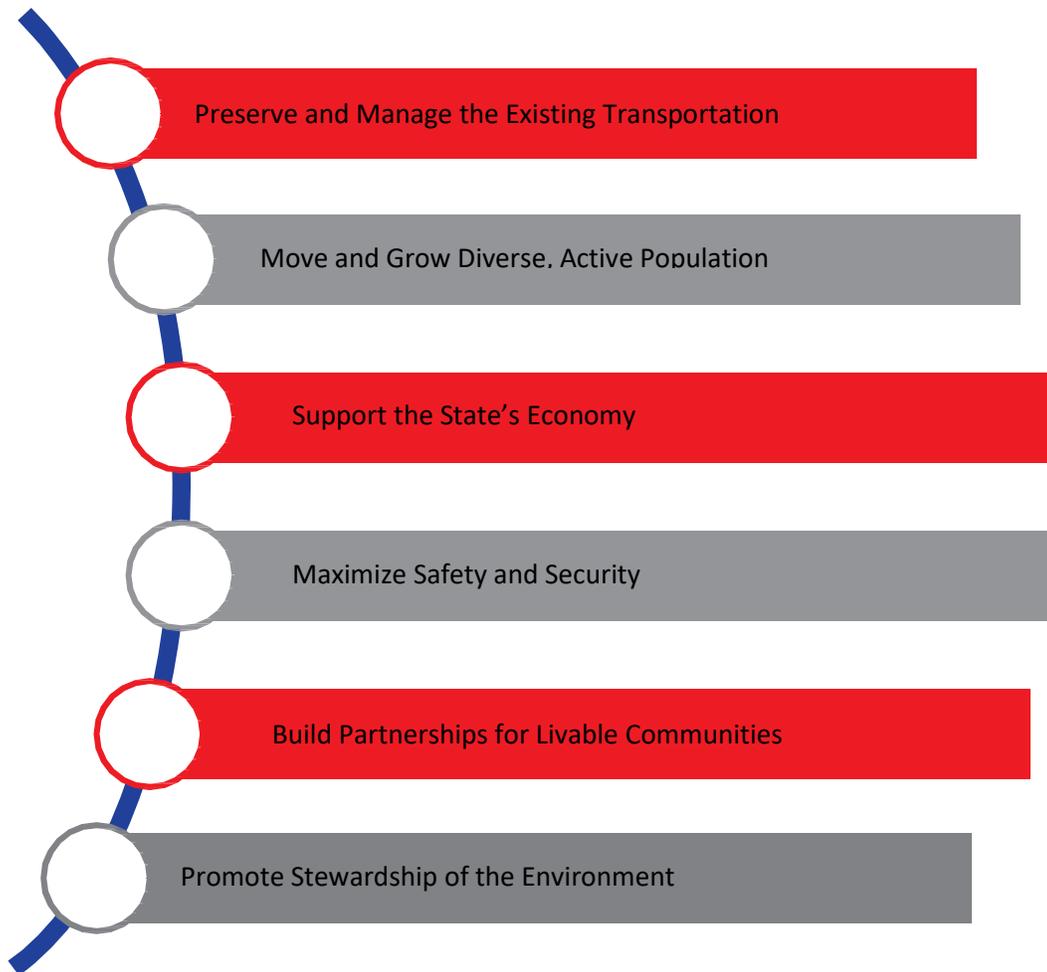
The bridge management budget has been relatively flat over the past several years; however, TDOT expects to increase funding by about \$1 million annually to keep up with inflation and to allow for additional projects to be completed each year. Although TDOT does not assign funding for bridges by system, certain factors are considered during the project selection process, which impacts where those bridges carrying higher volumes of traffic will end up on the priority list. Since the interstate and NHS routes tend to carry the most traffic, they tend to be prioritized for repair/rehabilitation/replacement before the lower volume bridges. This ensures that the NHS and interstate bridges continue to remain in a state of good repair and keeps Tennessee’s bridges among the best in the nation.

What Factors Outside of Physical Condition Affect TDOT’s Gap Analysis?

TDOT plans for the operations of the transportation system in multiple ways; many factors affecting the operations are part of the project selection process for the State Transportation Improvement Plan (STIP). Locations that commonly experience bottleneck or congestion problems, that see heavy truck traffic, or that experience traffic growth due to new developments are all issues that receive priority as part of the selection process. The items included in the project selection process are categorized to align with the Guiding Principles

(see Figure 3-14) established as part of TDOT's Long Range Policy Plan. Each of the categories has several time frames that determine the scoring for that goal. Under the goal to Preserve and Manage the Existing Transportation System, the evaluated items include level of service (LOS), average annual daily traffic (AADT), and freight movement (see Figure 3-15 below). The LOS is weighted the most and will see the impacts of the traffic growth discussed earlier. It is important to note that the goals of the matrix are established in order to address the operations of the transportation system and currently don't include a score for the asset condition.

Figure 3-14: TDOT's Guiding Principles for Developing the STIP



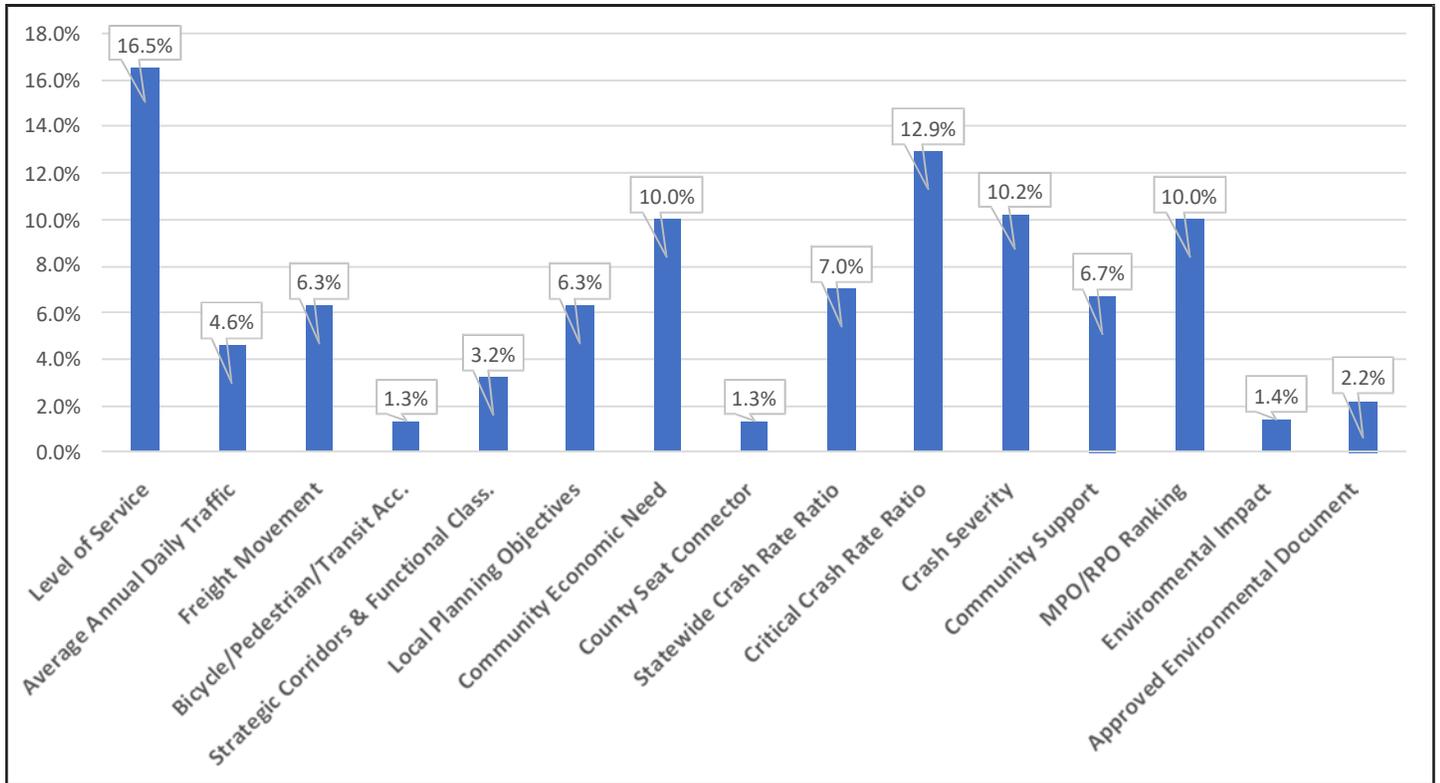


Figure 3-15: STIP Project Selection Prioritization Matrix Weighting

Several of the goals include weighted scores for roadways that are determined by evaluating an aspect used to measure the effectiveness of the NHS operations for providing safe and efficient movement of people and goods. The list below includes the goal and the specific characteristic evaluated that relate to the effectiveness of the NHS system.

- Move a Growing, Diverse, Active Population
 - Strategic Corridors and Functional Classification – the score is based on the roadway classification and also if it is part of the NHS
- Support the State’s Economy
 - Community Economic Need – highways that are identified as a route for industrial or office park locations receive high scores in this category
- Maximize Safety and Security
 - Statewide Crash Rate Ratio
 - Critical Crash Rate Ratio
 - Crash Severity

In order to account for the condition of the pavement and bridges and to ensure that TDOT is able to continue to meet the state of good repair targets, TDOT is considering revising the project selection matrix. Options under consideration are to revise the matrix to establish appropriate criteria and weighting of the PMS and BMS results. Additional options are to give roadways that are part of the NHS an appropriate weighted score to

reflect the routes' importance. This addition to the matrix would help address the pavement and bridge condition deficiencies by creating a weighted score which addresses roadways in poor condition.

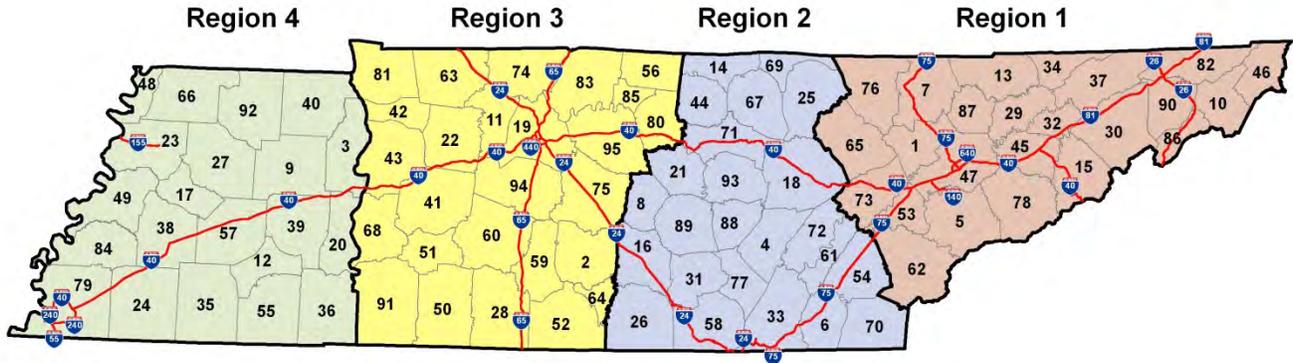
How Will TDOT Monitor the Performance of Pavement and Bridges?

As explained in earlier portions of this section, TDOT has a number of processes in place to monitor the condition of pavements and bridges and determine if the investment strategy and program of projects are in line with the objectives of the agency and the long-term state of good repair targets. Below is a summary of TDOT processes to identify potential problems, gaps, and development of strategies to head-off issues.

- On an annual basis, pavement condition results will be extracted from the pavement condition survey and reported to TDOT senior management. Additionally, pavement condition performance will be estimated based on current condition and budgetary amounts. Results will be compared to TDOT's long-term state of good repair targets and the targets TDOT will establish as a part of 23 USC 150(d) for the NHS. As described in Chapter 7, the results of the annual pavement performance report will be used to identify issues in TDOT's pavement management program, determination of funding amounts, or other gaps. Adjustments in program strategy and funding will be considered by senior management within the context of the overall vision and funding needs of the department.
- On an annual basis, bridge condition results will be extracted from the bridge management system and reported to TDOT senior management. Additionally, bridge performance will be estimated based on current conditions and budgetary amounts. Results will be compared to TDOT's long-term state of good repair targets and the targets TDOT will establish as a part of 23 USC 150(d) for the NHS. As described in Chapter 7, the results of the annual bridge performance report will be used to identify issues in TDOT's bridge management program, determination of funding amounts, or other gaps. Adjustments in program strategy and funding will be considered by senior management within the context of the overall vision and funding needs of the department.
- TDOT will also evaluate funding needs and effectiveness of the programming of projects, services, and efforts to meet the performance requirements of other sections of MAP-21 on safety, system performance/congestion, freight movement, and congestion mitigation and air quality. All of these various performance expectations will be considered by TDOT's senior management as annual budgets are developed in conjunction with the STIP and 3-Year construction program. With well-defined pavement and bridge programs and systems in place to evaluate the condition and future performance based on life-cycle cost planning, TDOT will be able to make informed decisions based on reliable data and state-of-the-practice analysis.

Chapter 7: Rural STIP Projects

Region Map with County Numbers List



Alphabetical List of Counties in Tennessee

01 Anderson	17 Crockett	33 Hamilton	49 Lauderdale	65 Morgan	81 Stewart
02 Bedford	18 Cumberland	34 Hancock	50 Lawrence	66 Obion	82 Sullivan
03 Benton	19 Davidson	35 Hardeman	51 Lewis	67 Overton	83 Sumner
04 Bledsoe	20 Decatur	36 Hardin	52 Lincoln	68 Perry	84 Tipton
05 Blount	21 DeKalb	37 Hawkins	53 Loudon	69 Pickett	85 Trousdale
06 Bradley	22 Dickson	38 Haywood	54 McMinn	70 Polk	86 Unicoi
07 Campbell	23 Dyer	39 Henderson	55 McNairy	71 Putnam	87 Union
08 Cannon	24 Fayette	40 Henry	56 Macon	72 Rhea	88 Van Buren
09 Carroll	25 Fentress	41 Hickman	57 Madison	73 Roane	89 Warren
10 Carter	26 Franklin	42 Houston	58 Marion	74 Robertson	90 Washington
11 Cheatham	27 Gibson	43 Humphreys	59 Marshall	75 Rutherford	91 Wayne
12 Chester	28 Giles	44 Jackson	60 Maury	76 Scott	92 Weakley
13 Claiborne	29 Grainger	45 Jefferson	61 Meigs	77 Sequatchie	93 White
14 Clay	30 Greene	46 Johnson	62 Monroe	78 Sevier	94 Williamson
15 Cocke	31 Grundy	47 Knox	63 Montgomery	79 Shelby	95 Wilson
16 Coffee	32 Hamblen	48 Lake	64 Moore	80 Smith	

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17022437446	124118.00	3.0	SR-437			TDOT

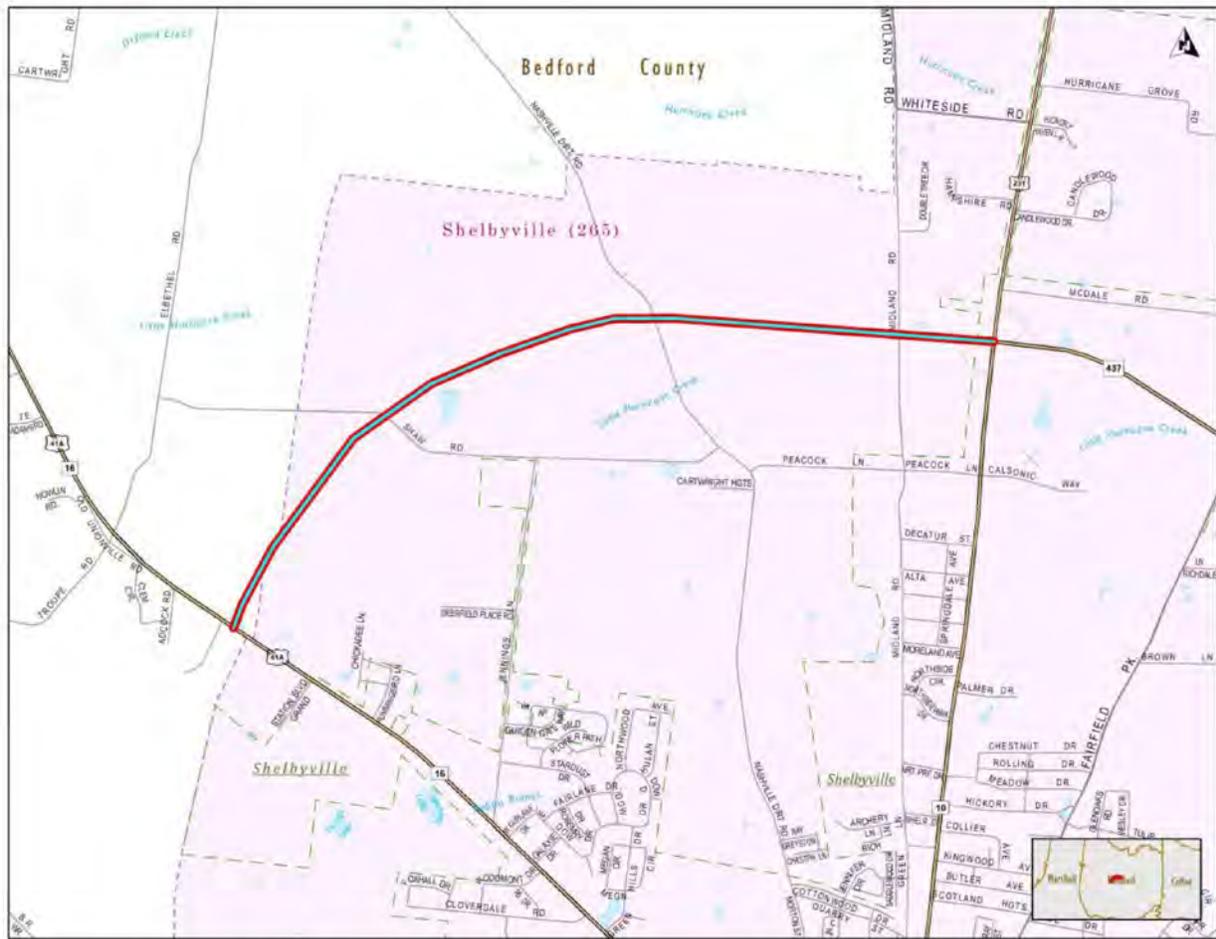
County:
BEDFORD

Total Cost	TIP No.	Conformity Status	LRP No.
\$26,280,000			

Termini:
SHELBYVILLE BYPASS FROM US-41A (SR-16) TO US-231 (SR-10) (IA)

Description:
CONSTRUCT NEW 2-LANE FACILITY

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	NHPP	\$1,000,000	\$800,000	\$200,000	

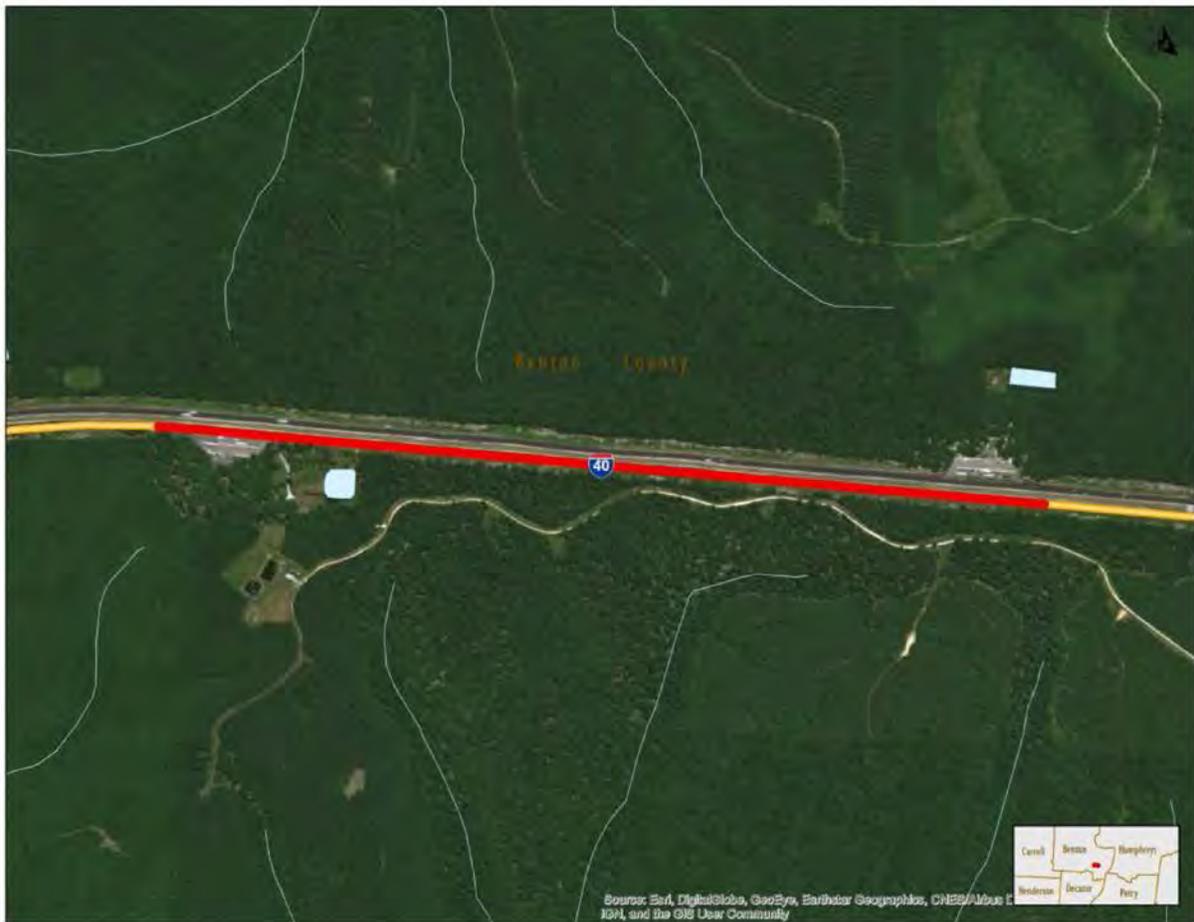


ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20031040802	124123.00	1.1	I-40			TDOT
County:						
BENTON						
Total Cost	TIP No.	Conformity Status		LRP No.		
\$4,800,000						
Termini:						
BENTON COUNTY REST AREA RENOVATION (IA)						
Description:						
REST AREA IMPROVEMENTS						

<u>Fiscal Year</u>	<u>Type of Work</u>	<u>Funding Type</u>	<u>Total Funds</u>	<u>Fed Funds</u>	<u>State Funds</u>	<u>Local Funds</u>
2022	PE-N	NHPP	\$500,000	\$400,000	\$100,000	
2022	PE-D	NHPP	\$600,000	\$480,000	\$120,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17032001448	105768.00	4.1	SR-1 (US-70)			TDOT

County:

BENTON

Total Cost	TIP No.	Conformity Status	LRP No.
\$52,780,500			

Termini:

CAMDEN BYPASS TO TENNESSEE RIVER (IA)

Description:

WIDEN FROM TWO LANE TO FIVE LANE TYPICAL SECTION

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	CONST	STBG	\$47,000,000	\$37,600,000	\$9,400,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20071075779	128535.00	0.33	I-75			TDOT

County:

CAMPBELL

Total Cost	TIP No.	Conformity Status	LRP No.
\$1,900,000			

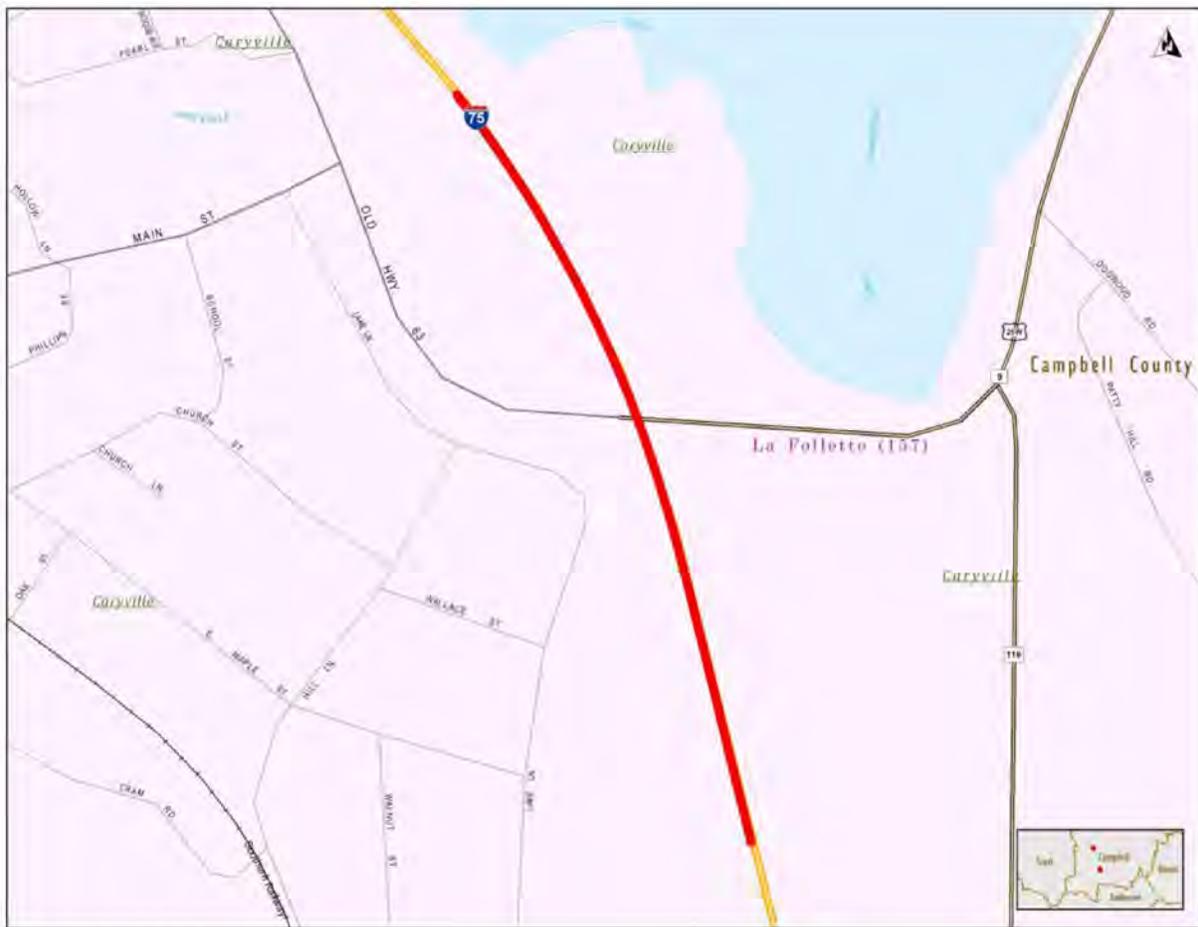
Termini:

ITS EXPANSION AT SR-63 (HOWARD BAKER RD., EXIT 141) INTERCHANGE AND SR-9/SR-116 (US-25W, EXIT 134) INTERCHANGE (IA)

Description:

INTELLIGENT TRANSPORTATION SYSTEM

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	PE-D	NHPP	\$200,000	\$160,000	\$40,000	
2022	CONST	NHPP	\$1,700,000	\$1,360,000	\$340,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17072063466	101406.04	2.01	SR-63			TDOT

County:

CAMPBELL

Total Cost	TIP No.	Conformity Status	LRP No.
\$3,900,000			

Termini:

(HOWARD BAKER HWY.) FROM NEAR THE INTERSECTION OF SR-297 AND WEST OF OLD SR-63 TO WEST OF STINKING CREEK ROAD (TRUCK CLIMBING LANE) (***) (IA)

Description:

CONSTRUCT TRUCK CLIMBING LANE AND INTERSECTION IMPROVEMENTS AT THE INTERSECTION OF SR-297 AND SR-63

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	STBG	\$600,000	\$480,000	\$120,000	
2022	CONST	STBG	\$3,300,000	\$2,640,000	\$660,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17072063467	101407.01	4.92	SR-63			TDOT

County:

CAMPBELL

Total Cost	TIP No.	Conformity Status	LRP No.
\$27,981,000			

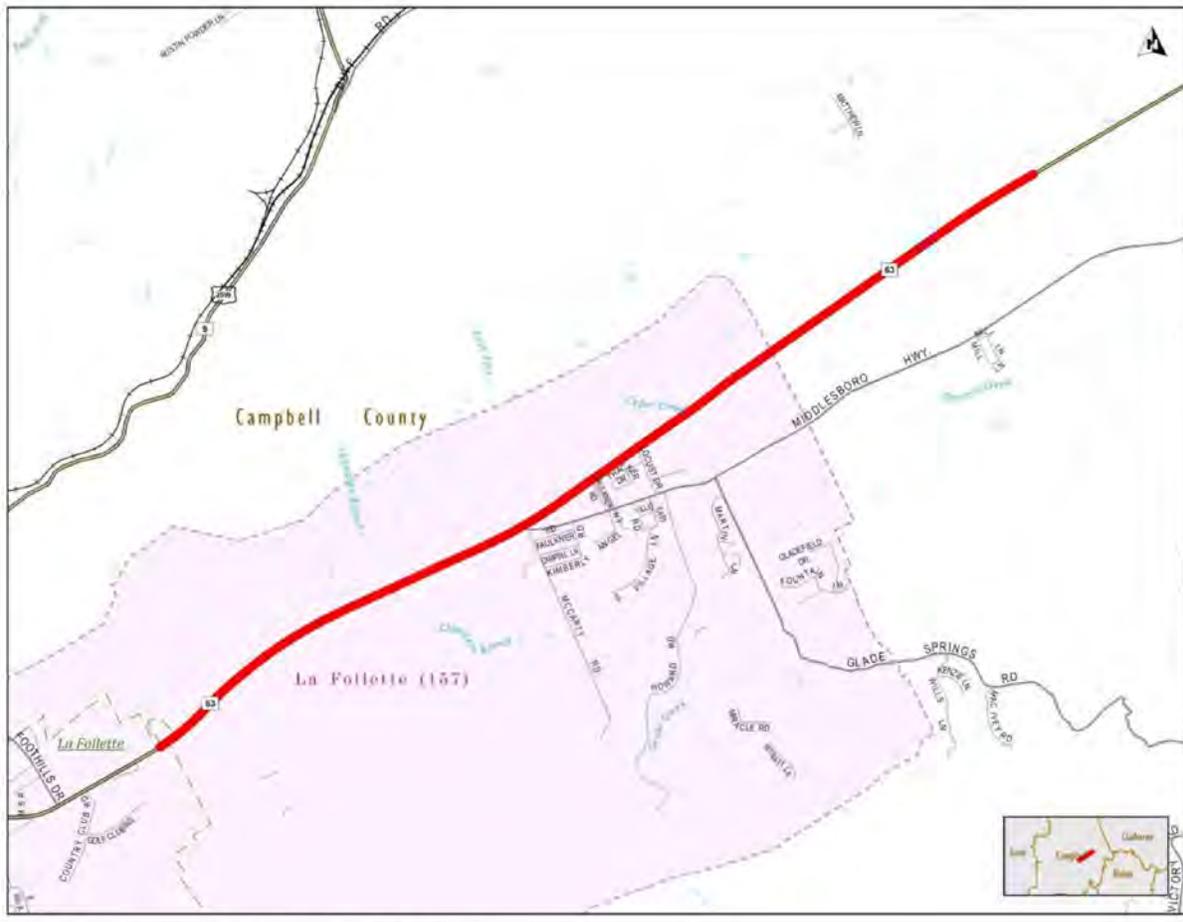
Termini:

(GENERAL CARL W. STINER HWY.), FROM NEAR MYERS LANE (LAFOLLETTE CITY LIMITS) TO NEAR FRONTIER ROAD/WOODSON LANE (IA)

Description:

WIDEN 2-LN TO 5-LN

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	APD	\$23,200,000	\$23,200,000		



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17072063468	101407.02	5.26	SR-63			TDOT

County:
 CAMPBELL

Total Cost	TIP No.	Conformity Status	LRP No.
\$46,600,000			

Termini:
 (GENERAL CARL W. STINER HWY.), FROM NEAR FRONTIER ROAD/WOODSON LANE TO NEAR CLAIBORNE COUNTY LINE (IA)

Description:
 WIDEN FROM 2-LN TO 5-LN

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	NHPP	\$3,682,000	\$3,682,000	\$0	
2021	CONST	APD	\$43,800,000	\$43,800,000		



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17071075469	124153.00	24.98	I-75			TDOT

County:

CAMPBELL

Total Cost	TIP No.	Conformity Status	LRP No.
\$11,400,000			

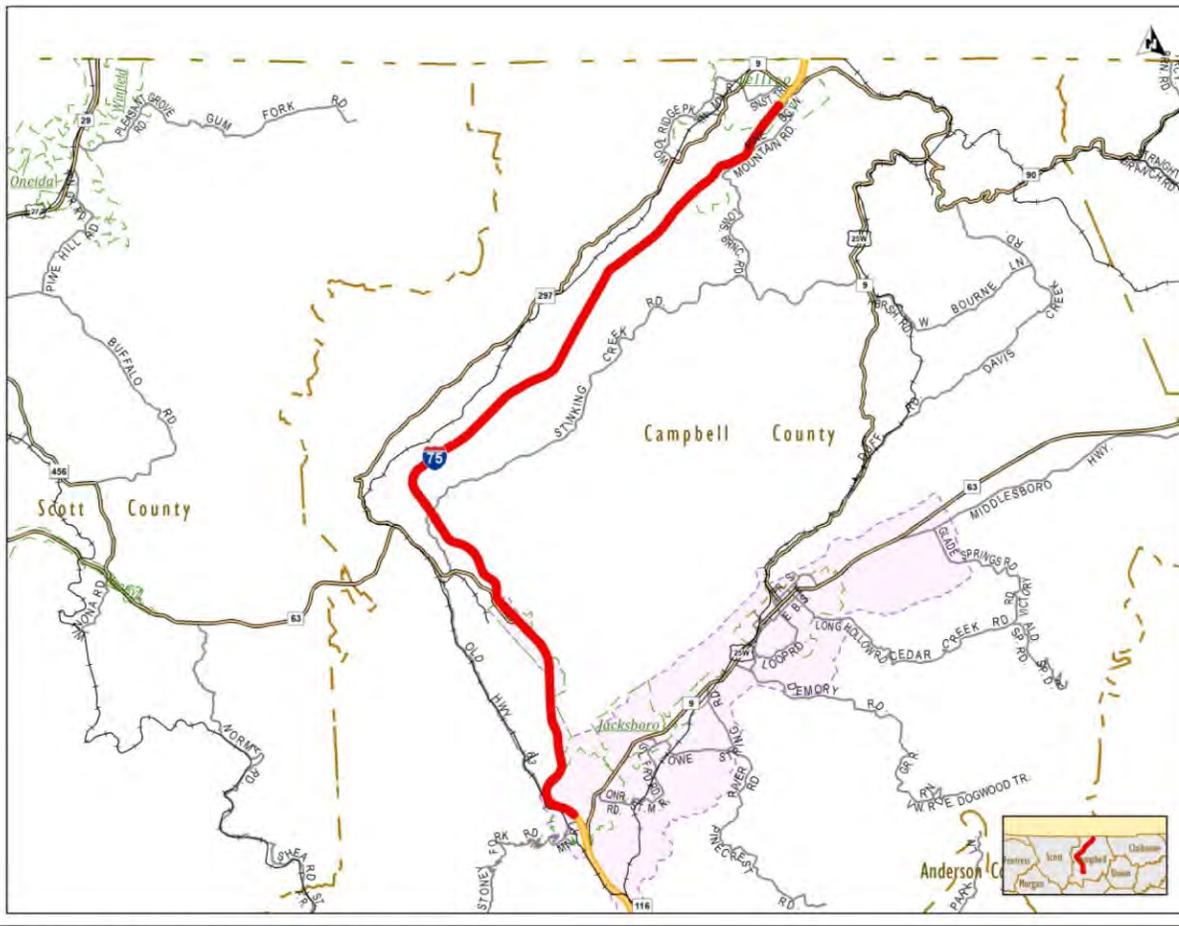
Termini:

FROM NEAR MM 135 TO NEAR MM 160 (SR-9/US-25W, JELICO (IA))

Description:

ITS EXPANSION TO INCLUDE THE INSTALLATION OF A POWER AND COMMUNICATION NETWORK AND ITS DEVICES SUCH AS CCTV CAMERAS, DMS, AND RDS. ALSO, PROJECT TO INCLUDE THE INSTALLATION OF A FOG AND SEVERE WEATHER DETECTION SYSTEM OVER JELICO MOUNTAIN.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	NHPP	\$750,000	\$675,000	\$75,000	
2021	CONST	NHPP	\$10,400,000	\$9,360,000	\$1,040,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17082001470	124038.00	5.44	SR-1			TDOT

County:

CANNON

Total Cost	TIP No.	Conformity Status	LRP No.
\$13,200,000			

Termini:

FROM WEST OF WOODBURY TO NEW SR-1 (US-70S) EAST OF WOODBURY (IA)

Description:

MISCELLANEOUS SAFETY IMPROVEMENTS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	PE-N	NHPP	\$50,000	\$40,000	\$10,000	
2022	PE-D	NHPP	\$50,000	\$40,000	\$10,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17272076471	100326.01	3.7	SR-76 (US-79)			TDOT

County:

CARROLL-GIBSON

Total Cost	TIP No.	Conformity Status	LRP No.
\$42,548,500			

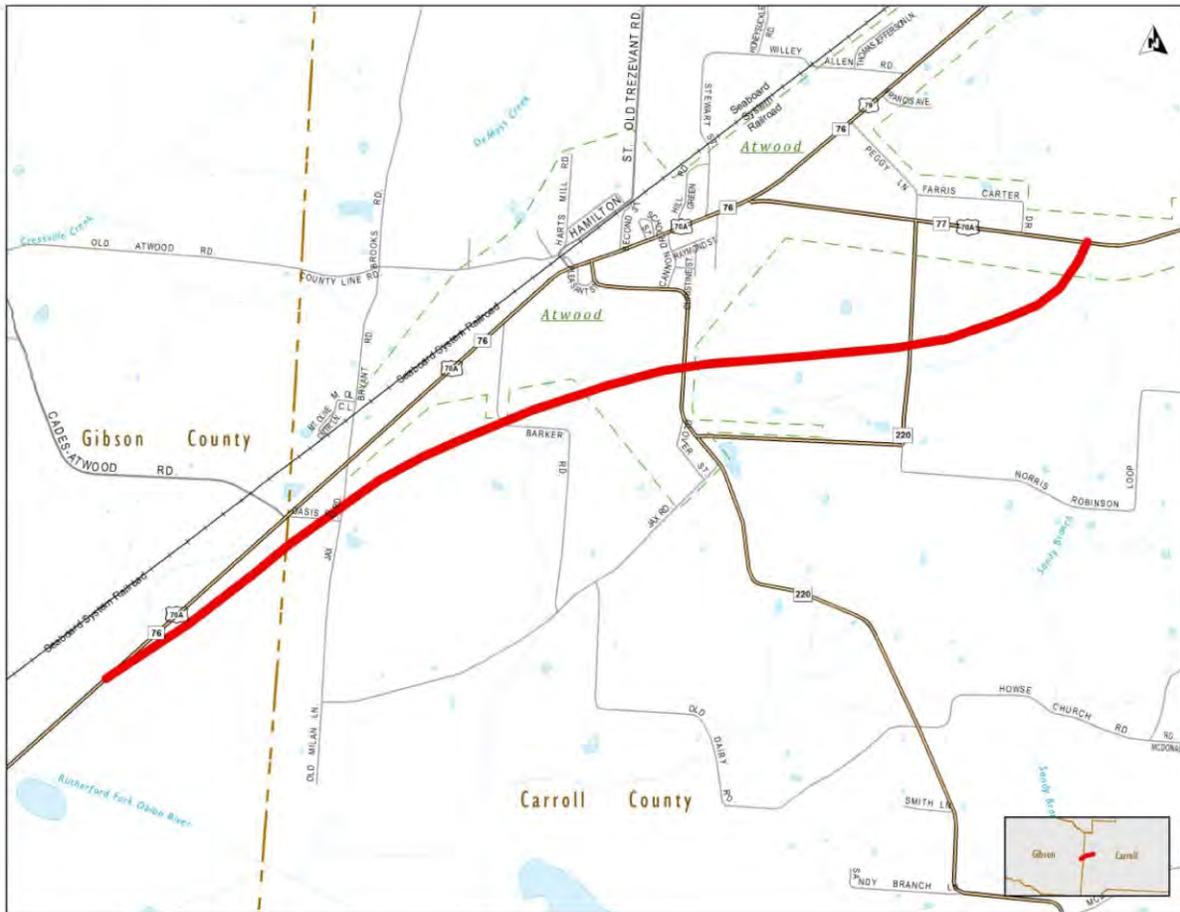
Termini:

FROM WEST OF SADES-ATWOOD ROAD TO EAST OF SR-77 (IA)

Description:

WIDEN 2-LN TO 4-LN WITH PORTIONS ON NEW ALIGNMENT

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	CONST	NHPP	\$34,300,000	\$27,440,000	\$6,860,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17092076472	100326.03	4.8	SR-76 (US-79)			TDOT

County:
CARROLL

Total Cost	TIP No.	Conformity Status	LRP No.
\$16,256,000			

Termini:
FROM WEST OF CUTLIP LANE TO WEST OF SYDNOR RD/WINSTON RD (IA)

Description:
Widen 2-ln to 4-ln with portions on new alignment

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	ROW	NHPP	\$1,100,000	\$880,000	\$220,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17122022474	124164.00	8.55	SR-22			TDOT

County:

MCNAIRY, CHESTER, HENDERSON

Total Cost	TIP No.	Conformity Status	LRP No.
\$2,200,000			

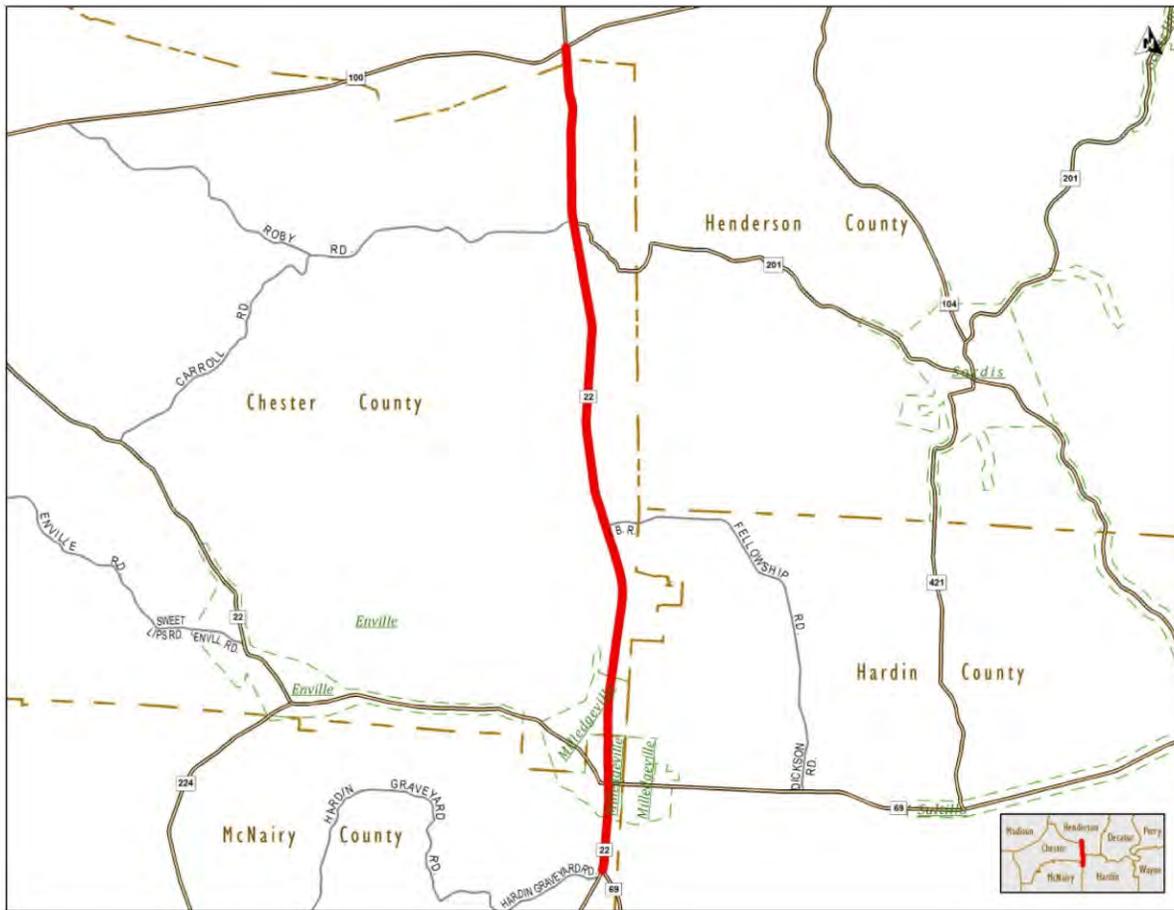
Termini:

FROM SR-69 IN MILLEDGEVILLE TO SR-100 IN HENDERSON COUNTY (IA)

Description:

SPOT SAFETY IMPROVEMENTS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	PE-N	STBG	\$140,000	\$112,000	\$28,000	
2022	PE-D	STBG	\$60,000	\$48,000	\$12,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17132032475	118734.00	0.01	SR-32			TDOT

County:
 CLAIBORNE

Total Cost	TIP No.	Conformity Status	LRP No.
\$6,800,000			

Termini:
 INTERCHANGE AT SR-345 (IA)

Description:
 CONSTRUCT NEW INTERCHANGE

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	APD	\$200,000	\$200,000	\$0	
2020	ROW	NHPP	\$500,000	\$400,000	\$100,000	
2020	CONST	APD	\$5,800,000	\$5,800,000		



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17132063476	101409.00	4.8	SR-63			TDOT

County:
 CLAIBORNE

Total Cost	TIP No.	Conformity Status	LRP No.
\$54,300,000			

Termini:
 FROM CAMPBELL COUNTY LINE TO HALL LANE (IA)

Description:
 WIDEN FROM 2-LN TO 5-LN

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	CONST	APD	\$43,700,000	\$43,700,000		



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17132063477	105763.00	6.2	SR-63			TDOT

County:

CLAIBORNE

Total Cost	TIP No.	Conformity Status	LRP No.
\$14,300,000			

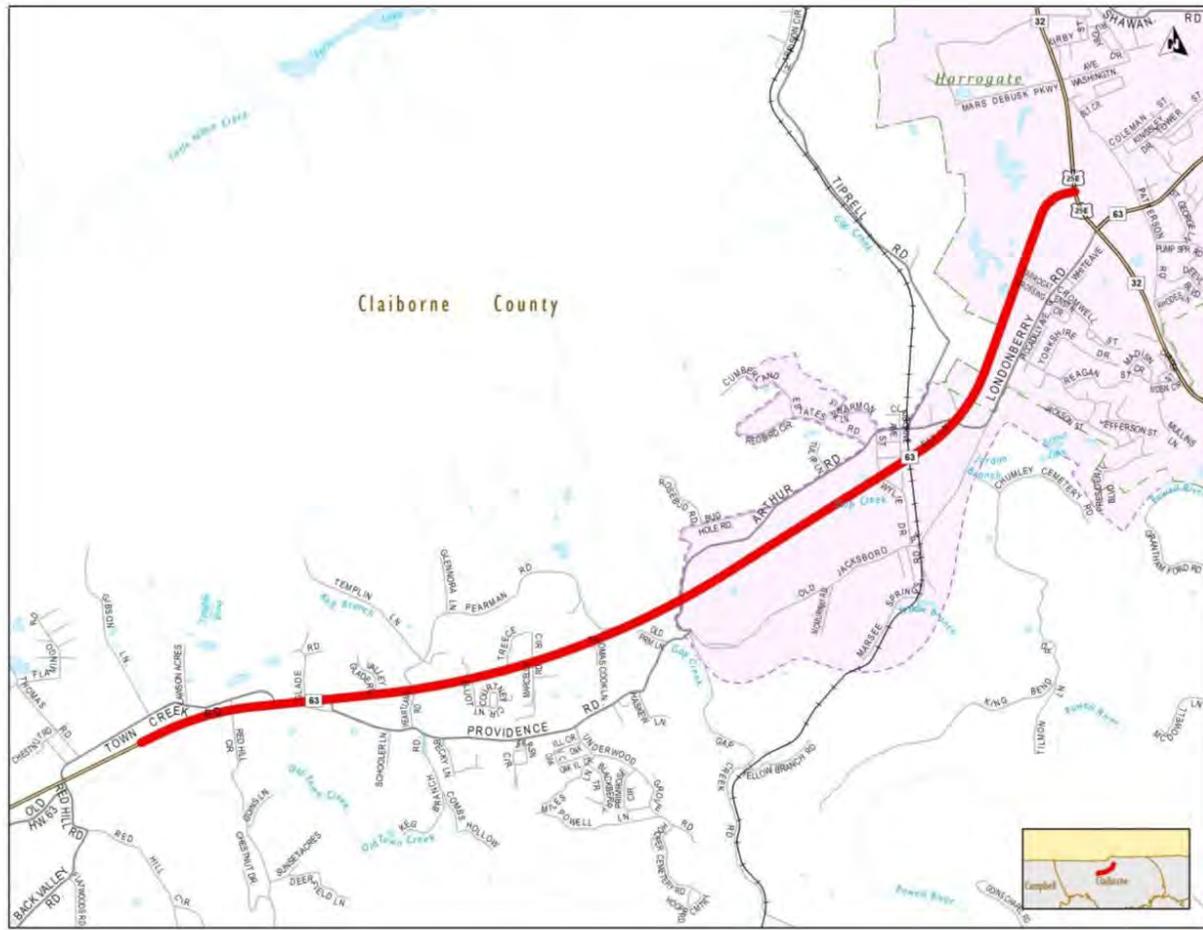
Termini:

FROM WEST OF OLD TOWN CREEK TO SR-32 (US-25E) (EPD) (IA)

Description:

WIDEN 2-LN TO 3-LN AND CONSTRUCT SHARED USE PATH

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	NHPP	\$100,000	\$80,000	\$20,000	
2020	CONST	NHPP	\$12,700,000	\$10,160,000	\$2,540,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17142052478	127491.00	0.86	SR-52			TDOT

County:

CLAY

Total Cost	TIP No.	Conformity Status	LRP No.
\$14,200,000			

Termini:

(CELINA BYPASS), FROM NEAR KYLE STREET TO EXISTING SR-52 NEAR WASHINGTON STREET

Description:

NEW CONSTRUCTION (NEW START)

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-N	STBG	\$350,000	\$280,000	\$70,000	
2020	PE-D	STBG	\$150,000	\$120,000	\$30,000	
2022	ROW	STBG	\$1,000,000	\$800,000	\$200,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17152035479	101399.00	4.72	SR-35 (US-321)			TDOT

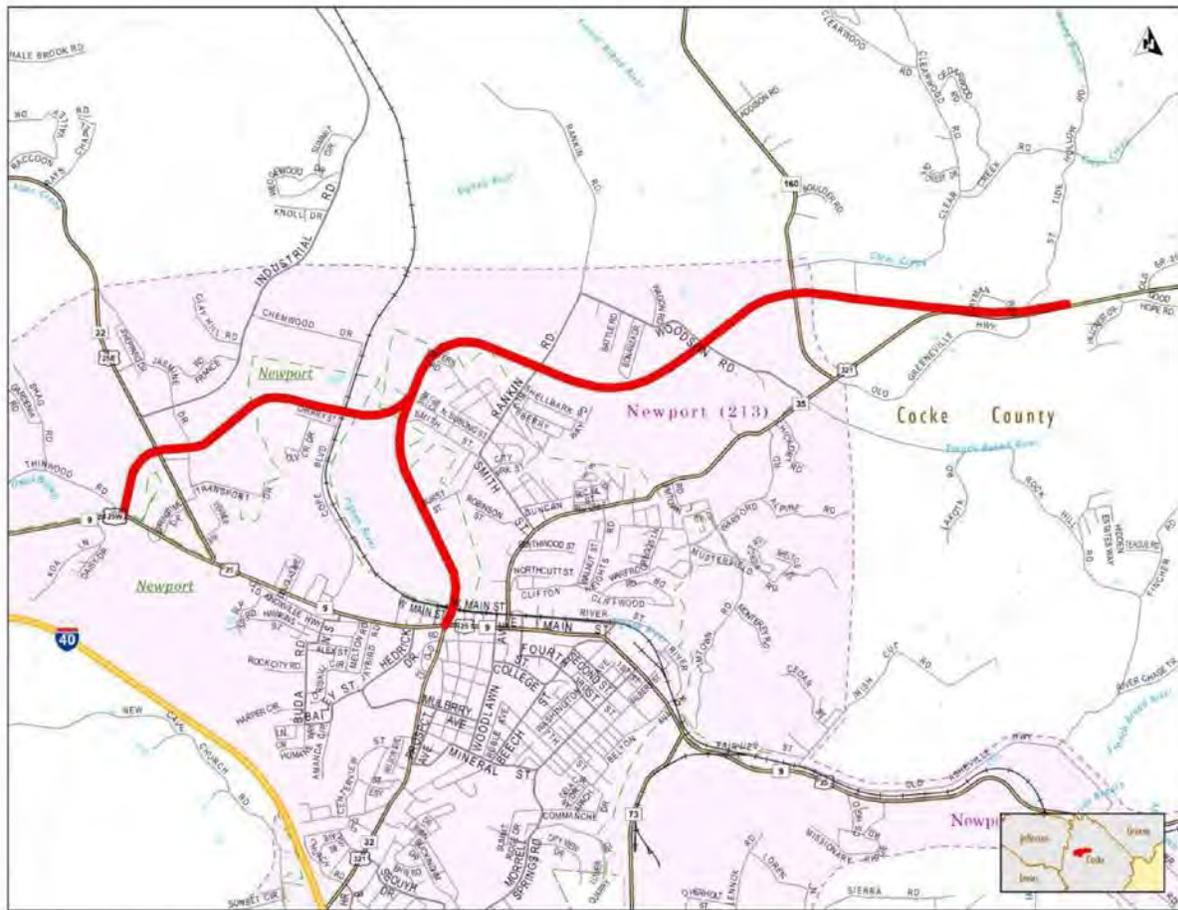
County:
COCKE

Total Cost	TIP No.	Conformity Status	LRP No.
\$82,463,000			

Termini:
(NEWPORT BYPASS), FROM SR-9 TO SAINT TIDE HOLLOW ROAD (IA)

Description:
CONSTRUCTION NEW: 5-LN RURAL AND SUPER 2-LN ON 4-LN ROW BYPASS AROUND NEWPORT

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	NHPP	\$72,000,000	\$57,600,000	\$14,400,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17151040480	124292.00	21.9	I-40			TDOT

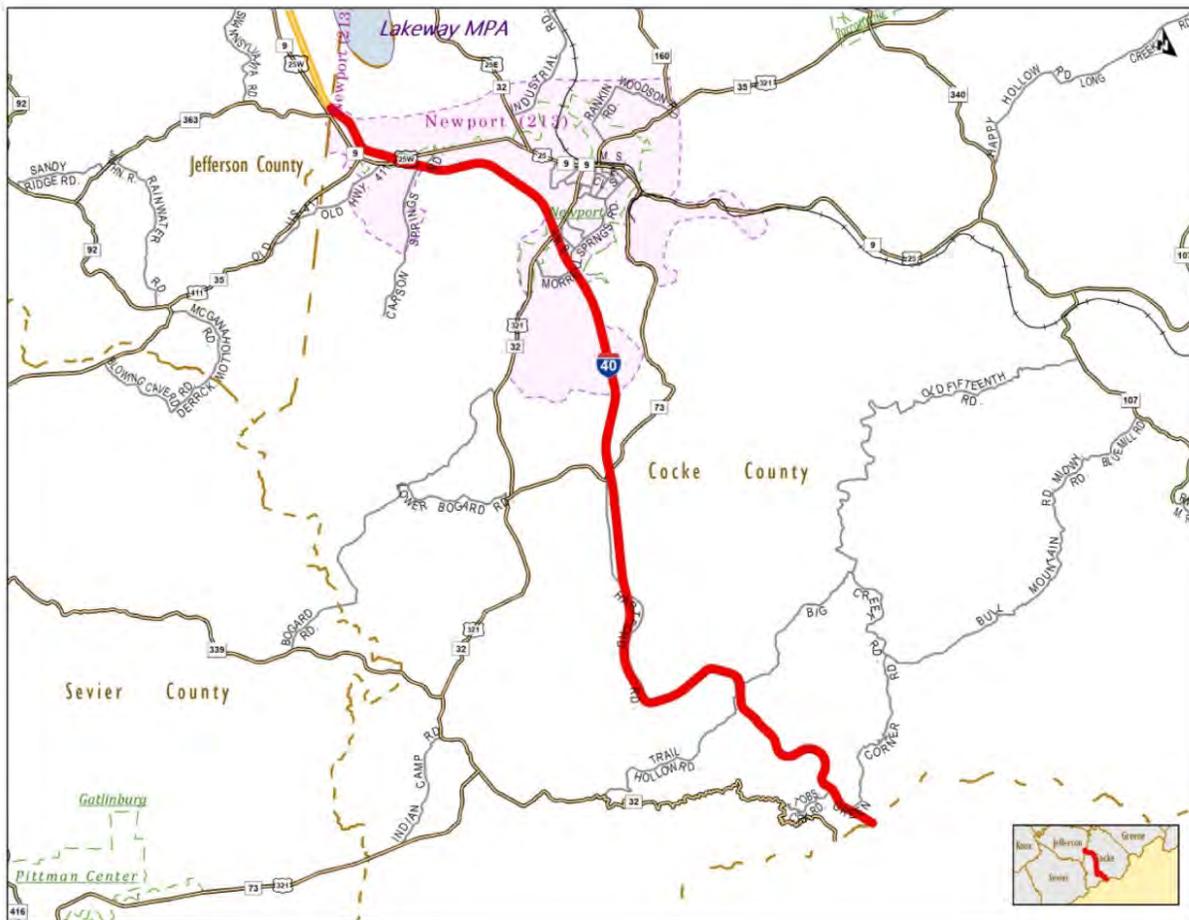
County:
COCKE

Total Cost	TIP No.	Conformity Status	LRP No.
\$2,400,000			

Termini:
FROM JEFFERSON COUNTY LINE TO NORTH CAROLINA STATE LINE (IA)

Description:
INTELLIGENT TRANSPORTATION SYSTEM DEPLOYMENT

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	PE-N	NHPP	\$140,000	\$112,000	\$28,000	
2021	PE-D	NHPP	\$60,000	\$48,000	\$12,000	
2023	CONST	NHPP	\$2,200,000	\$1,760,000	\$440,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17151040481	124301.00	0.39	I-40			TDOT

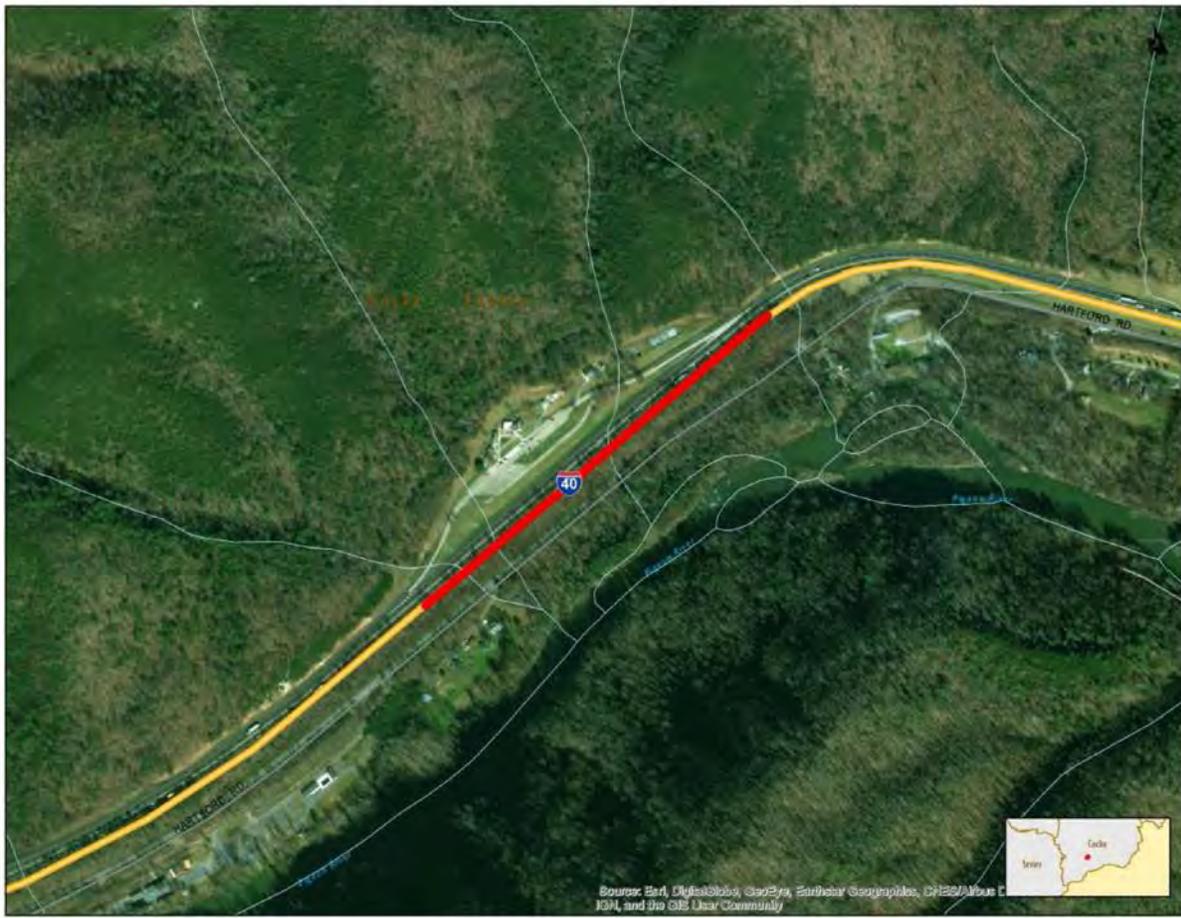
County:
 COCKE

Total Cost	TIP No.	Conformity Status	LRP No.
\$3,100,000			

Termini:
 "HARTFORD" WELCOME CENTER RENOVATION (IA)

Description:
 REST AREA IMPROVEMENTS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	PE-N	NHPP	\$70,000	\$56,000	\$14,000	
2022	PE-D	NHPP	\$30,000	\$24,000	\$6,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20162002780	123817.00	2.1	SR-2 (US-41)			TDOT

County:

COFFEE

Total Cost	TIP No.	Conformity Status	LRP No.
\$30,300,000			

Termini:

(US-41, HILLSBORO HWY), FROM JOE HICKERSON ROAD TO AEDC ROAD (IA)

Description:

WIDENING FROM 2 TO 5 LANES

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-N	NHPP	\$700,000	\$560,000	\$140,000	
2020	PE-D	NHPP	\$300,000	\$240,000	\$60,000	
2022	ROW	NHPP	\$5,000,000	\$4,000,000	\$1,000,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17181040484	124054.00	53.2	I-40			TDOT

County:

CUMBERLAND, PUTNAM

Total Cost	TIP No.	Conformity Status	LRP No.
\$30,370,000			

Termini:

(CUMBERLAND PLATEAU) FROM NEAR MM 285 TO NEAR SR-299 (EXIT 338) (IA)

Description:

INTELLIGENT TRANSPORTATION SYSTEM EXPANSION

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	NHPP	\$110,000	\$99,000	\$11,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17181040485	124051.00	0.1	I-40			TDOT

County:

CUMBERLAND

Total Cost	TIP No.	Conformity Status	LRP No.
\$3,800,000			

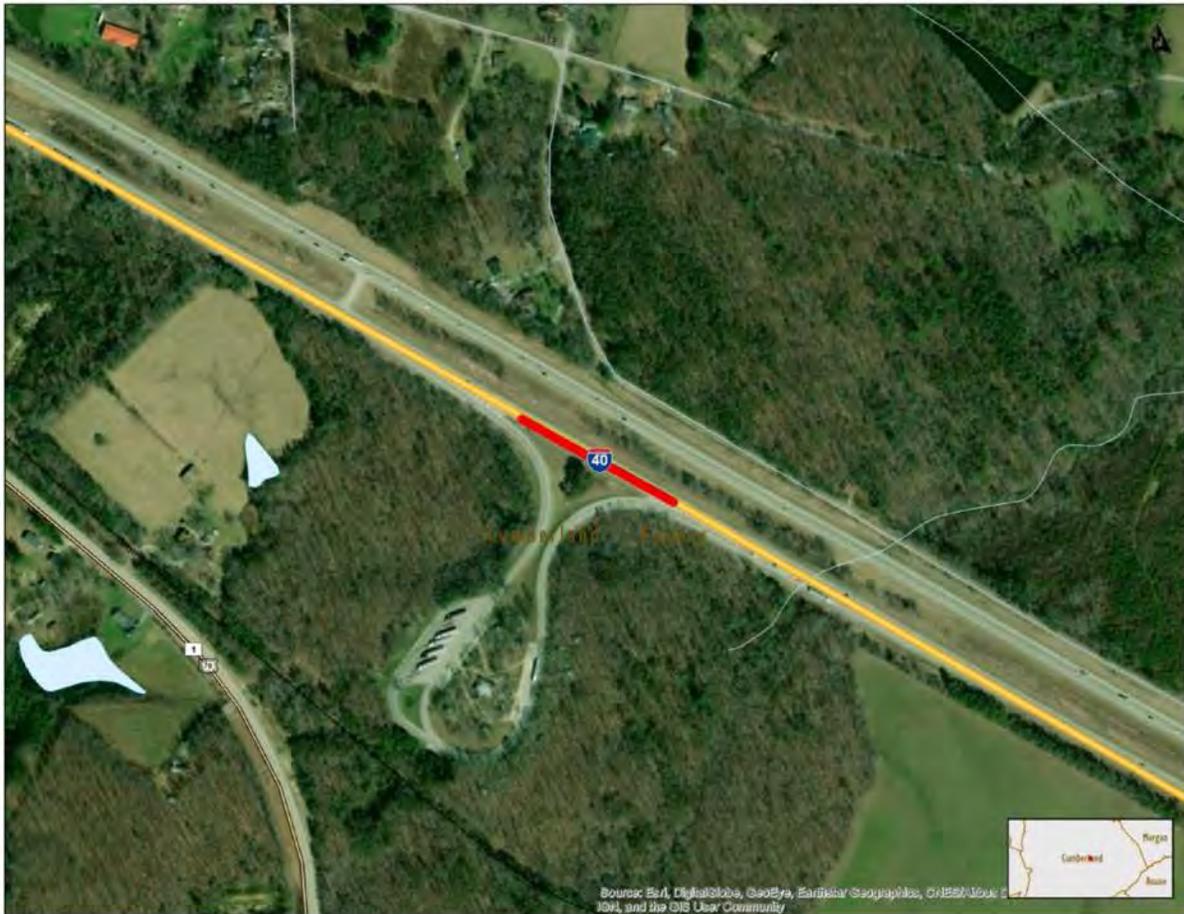
Termini:

CUMBERLAND COUNTY REST AREA RENOVATION (IA)

Description:

REST AREA IMPROVEMENTS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	PE-N	NHPP	\$70,000	\$56,000	\$14,000	
2022	PE-D	NHPP	\$30,000	\$24,000	\$6,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17182028486	100260.01	4.88	SR-28 (US-127)			TDOT

County:

CUMBERLAND

Total Cost	TIP No.	Conformity Status	LRP No.
\$77,050,000			

Termini:

NORTH OF I-40 TO NEAR POTATO FARM ROAD (IA)

Description:

WIDEN FROM 2-LN TO A MULTI-LANE FACILITY

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	NHPP	\$54,100,000	\$43,280,000	\$10,820,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17182028487	100260.03	3.45	SR-28 (US-127)			TDOT

County:

CUMBERLAND

Total Cost	TIP No.	Conformity Status	LRP No.
\$45,700,000			

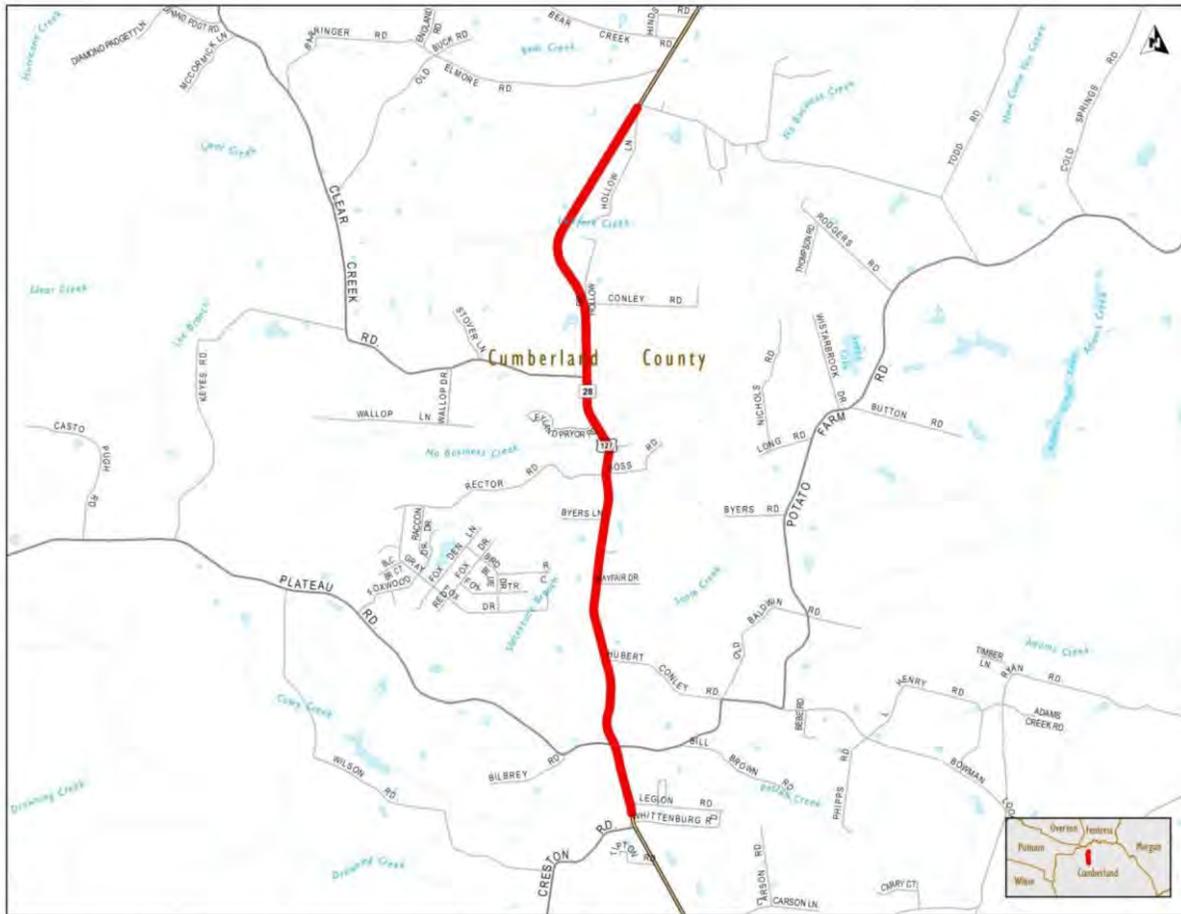
Termini:

NEAR POTATO FARM ROAD TO NEAR HOLLOW LANE (EPD)

Description:

WIDEN FROM 2-LN TO A MULTI-LANE FACILITY

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	NHPP	\$9,500,000	\$7,600,000	\$1,900,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17182028488	100260.04	3.05	SR-28 (US-127)			TDOT

County:

CUMBERLAND

Total Cost	TIP No.	Conformity Status	LRP No.
\$30,400,000			

Termini:

FROM NEAR HOLLOW LANE TO NEAR LOWE ROAD (EPD) (IA)

Description:

WIDEN FROM 2-LN TO A MULTI-LANE FACILITY

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	NHPP	\$7,400,000	\$5,920,000	\$1,480,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17231155506	124207.00	0.6	I-155			TDOT

County:

DYER

Total Cost	TIP No.	Conformity Status	LRP No.
\$3,400,000			

Termini:

"DYERSBURG" WELCOME CENTER RENOVATION (IA)

Description:

REST AREA IMPROVEMENTS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	PE-N	NHPP	\$100,000	\$80,000	\$20,000	
2021	PE-D	NHPP	\$100,000	\$80,000	\$20,000	
2022	ROW	NHPP	\$1,000,000	\$800,000	\$200,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17232104507	104123.07	3.81	SR-104			TDOT

County:

DYER

Total Cost	TIP No.	Conformity Status	LRP No.
\$54,000,000			

Termini:

FROM SR-20 (US-412) IN DYERSBURG TO EAST OF DON HURLEY ROAD (1A)

Description:

WIDEN EXISTING TO 12 FT TRAVEL LANES AND 4-10 FT SHOULDERS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	ROW	STBG	\$7,000,000	\$5,600,000	\$1,400,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17242460508	101607.01	2.8	SR-460			TDOT

County:

FAYETTE

Total Cost	TIP No.	Conformity Status	LRP No.
\$16,000,000			

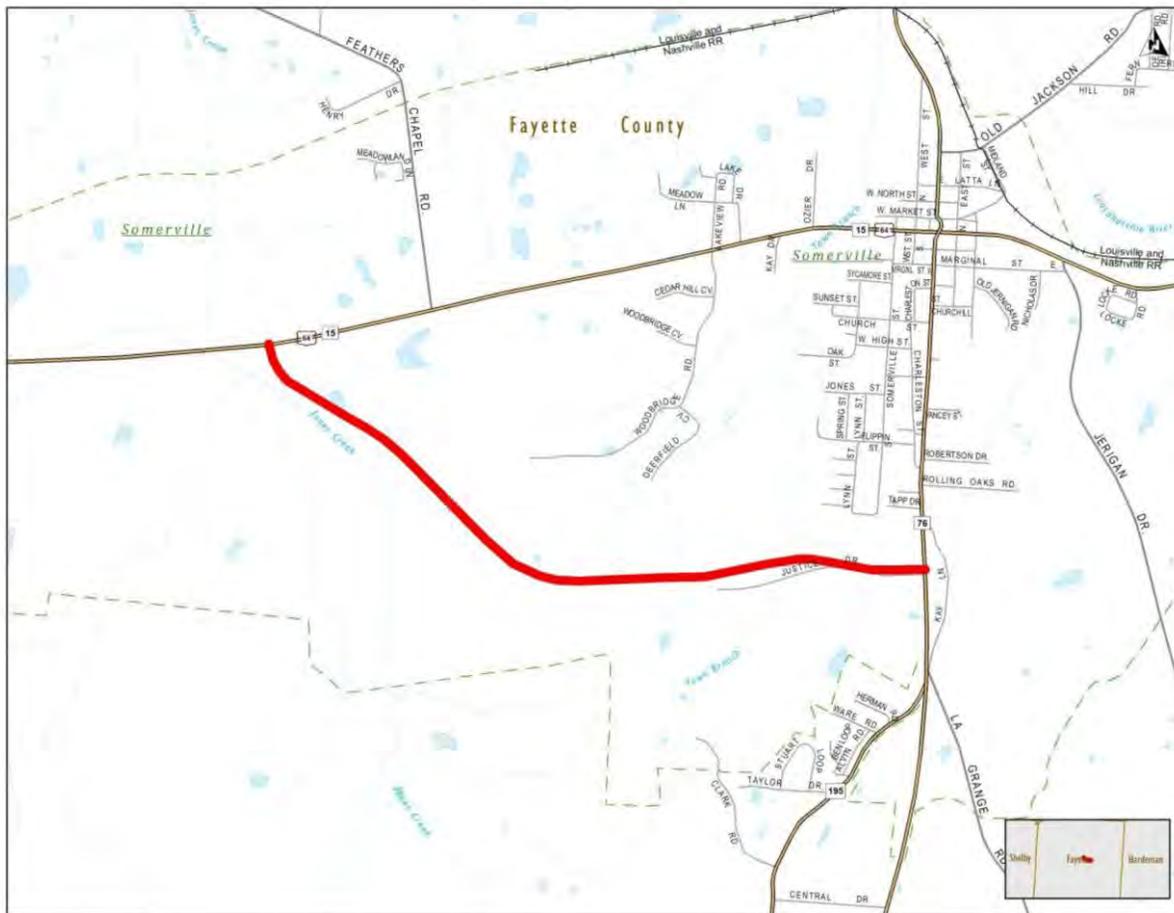
Termini:

(SOMERVILLE BELTWAY), FROM NEAR SR-15 (US-64) WEST OF SOMERVILLE TO NEAR SR-76 SOUTH OF SOMERVILLE (IA)

Description:

2-LANE BYPASS ON NEW ALIGNMENT W/ AT GRADE INTERSECTIONS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	NHPP	\$16,000,000	\$12,800,000	\$3,200,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17252028509	107774.01	3.23	SR-28 (US-127)			TDOT

County:

FENTRESS

Total Cost	TIP No.	Conformity Status	LRP No.
\$24,500,000			

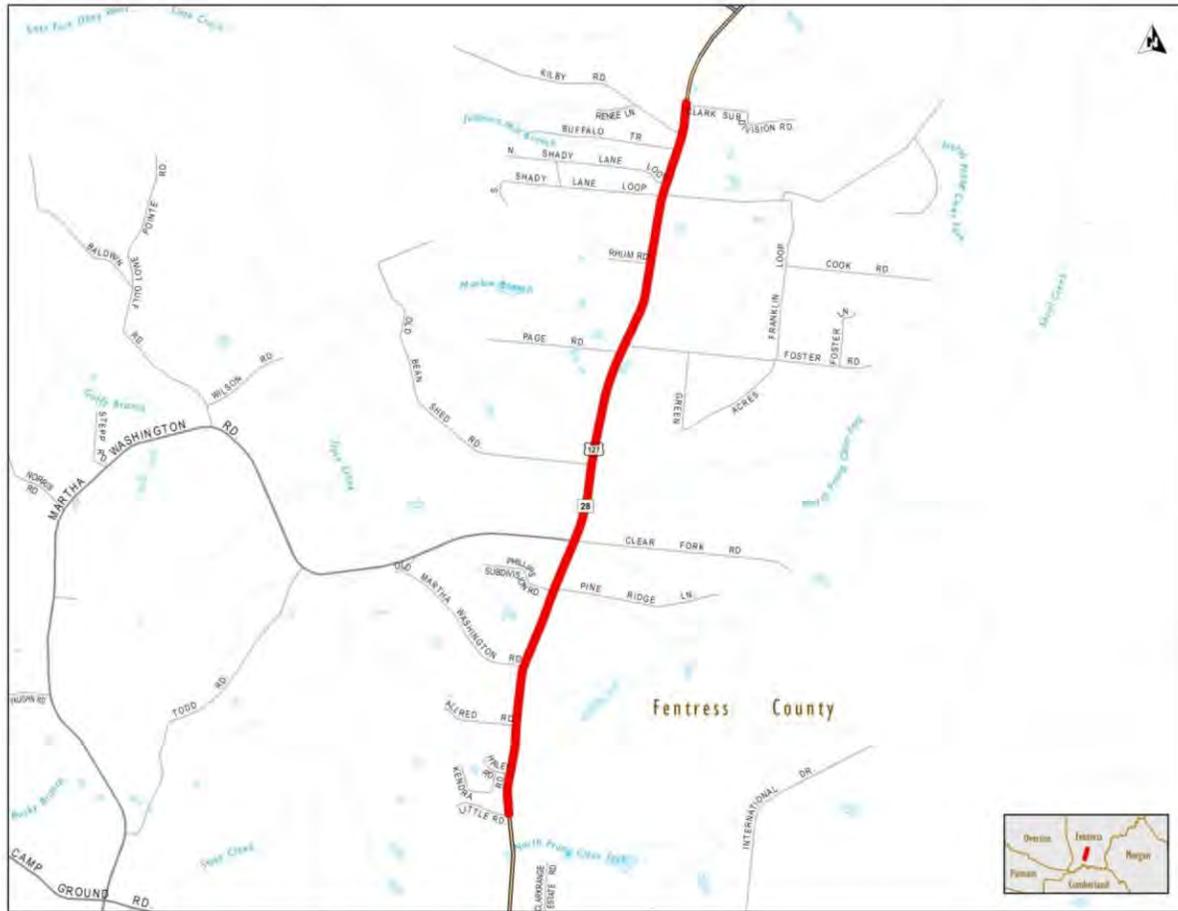
Termini:

(SOUTH YORK HIGHWAY), FROM NEAR LITTLE ROAD TO NORTH OF KILBY ROAD (IA)

Description:

WIDEN FROM 2-LN TO A MULTI-LANE FACILITY

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	NHPP	\$1,000,000	\$800,000	\$200,000	
2020	ROW	NHPP	\$3,700,000	\$2,960,000	\$740,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17252028510	123081.00	19.2	SR-28 (US-127)			TDOT

County:

FENTRESS, PICKETT

Total Cost	TIP No.	Conformity Status	LRP No.
\$10,000,000			

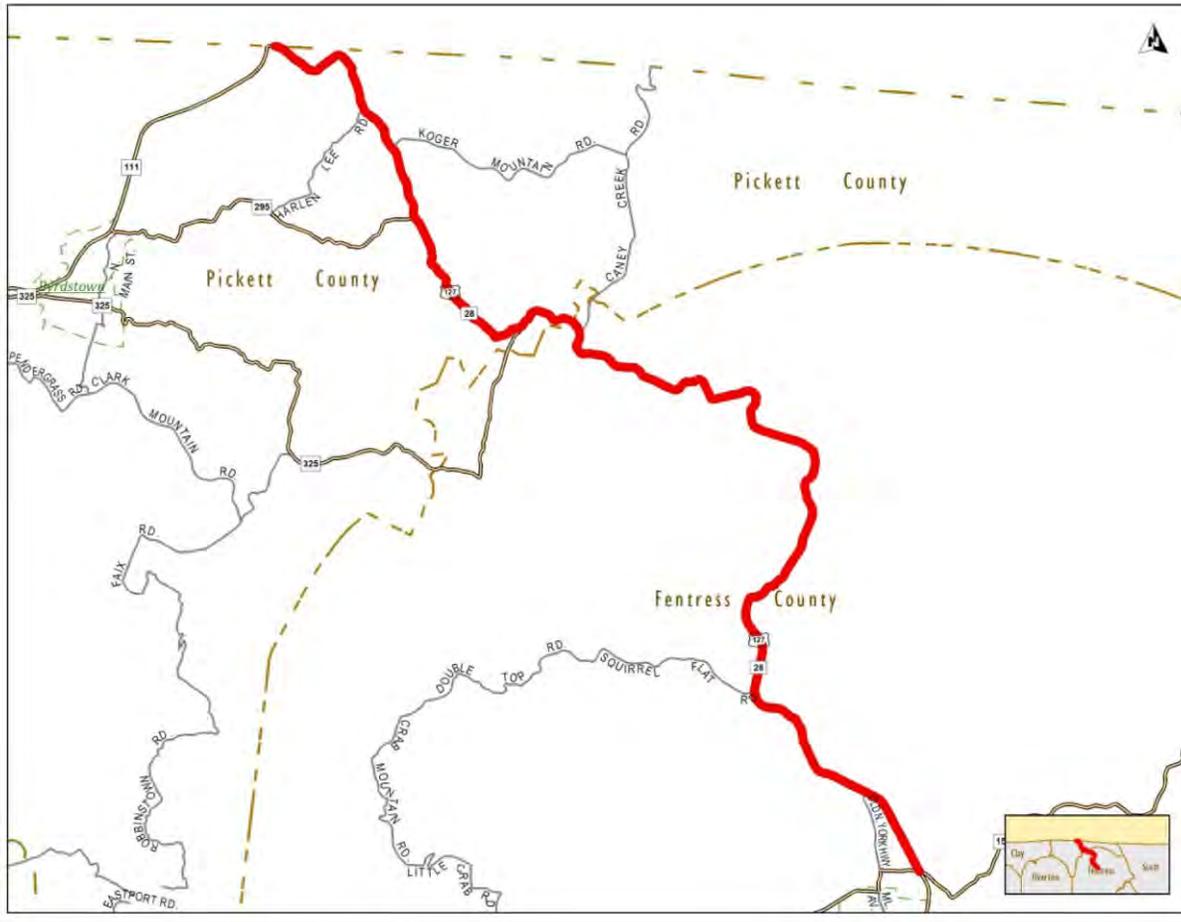
Termini:

FROM NORTH OF JAMESTOWN TO NEAR SR-111 (IA)

Description:

SPOT IMPROVEMENTS INCLUDING PASSING; TRUCK CLIMBING LANES, TURN LANES AND INTERSECTION REALIGNMENTS AND IMPROVEMENTS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	NHPP	\$500,000	\$400,000	\$100,000	
2022	ROW	NHPP	\$2,000,000	\$1,600,000	\$400,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17182028511	100260.05	3.27	SR-28 (US-127)			TDOT

County:

FENTRESS-CUMBERLAND

Total Cost	TIP No.	Conformity Status	LRP No.
\$78,200,000			

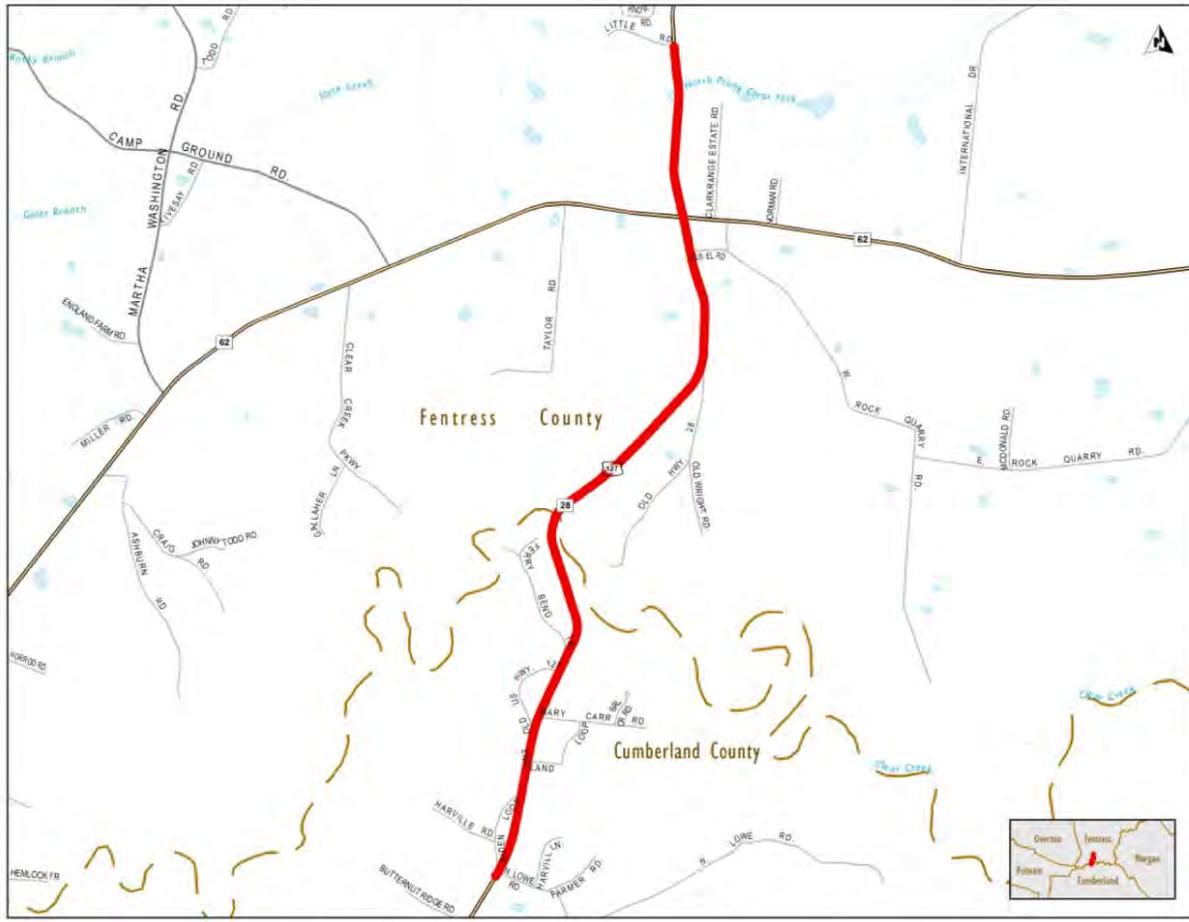
Termini:

FROM NEAR NORTH LOWE ROAD TO NEAR LITTLE ROAD, NORTH OF SR-62 (IA)

Description:

WIDEN FROM EXISTING 2 LANE TO A 4 TO 5 LANE TYPICAL SECTIONS.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	CONST	NHPP	\$71,200,000	\$56,960,000	\$14,240,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17292001512	101410.02	5.7	SR-1 (US-11W)			TDOT

County:

GRAINGER

Total Cost	TIP No.	Conformity Status	LRP No.
\$40,716,900			

Termini:

FROM RUTLEDGE TO WEST OF HELTON ROAD (IA)

Description:

WIDEN 2-LN TO 5-LN

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	STBG	\$8,647,500	\$6,918,000	\$1,729,500	
2020	ROW	HPP	\$4,559,219	\$3,647,375	\$911,844	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17292001513	101410.01	4.62	SR-1 (US-11W)			TDOT

County:

GRAINGER

Total Cost	TIP No.	Conformity Status	LRP No.
\$36,600,000			

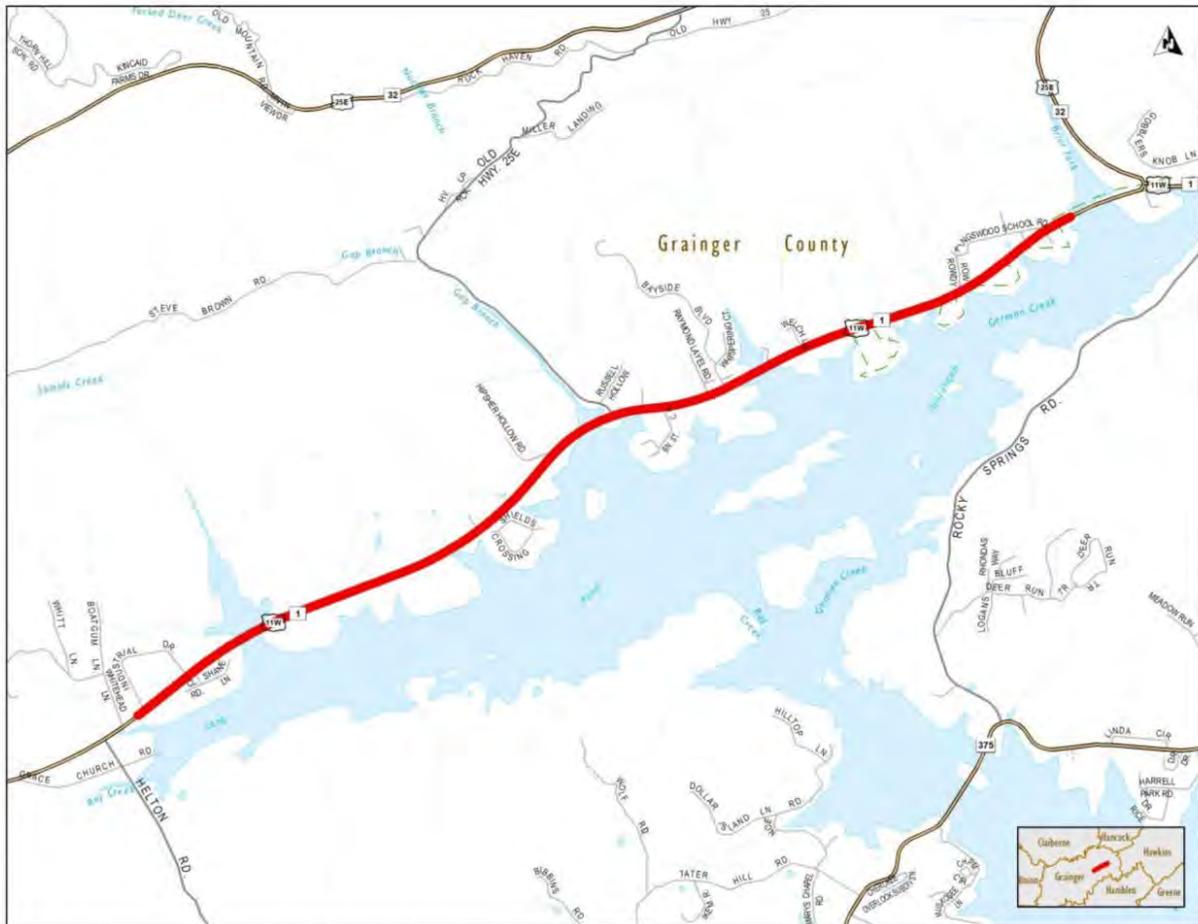
Termini:

FROM WEST OF HELTON ROAD TO BEAN STATION (IA)

Description:

WIDEN 2-LN TO 5-LN

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	STBG	\$7,800,000	\$6,240,000	\$1,560,000	
2022	CONST	STBG	\$27,300,000	\$21,840,000	\$5,460,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20301081781	124345.00	0.45	I-81			TDOT

County:

GREENE

Total Cost	TIP No.	Conformity Status	LRP No.
\$3,900,000			

Termini:

GREENE COUNTY REST AREA RENOVATION (IA)

Description:

GREENE COUNTY REST AREA RENOVATION (IA)

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	PE-N	NHPP	\$50,000	\$40,000	\$10,000	
2022	PE-D	NHPP	\$50,000	\$40,000	\$10,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17302035515	100229.00	6.16	SR-35 (US-321)			TDOT

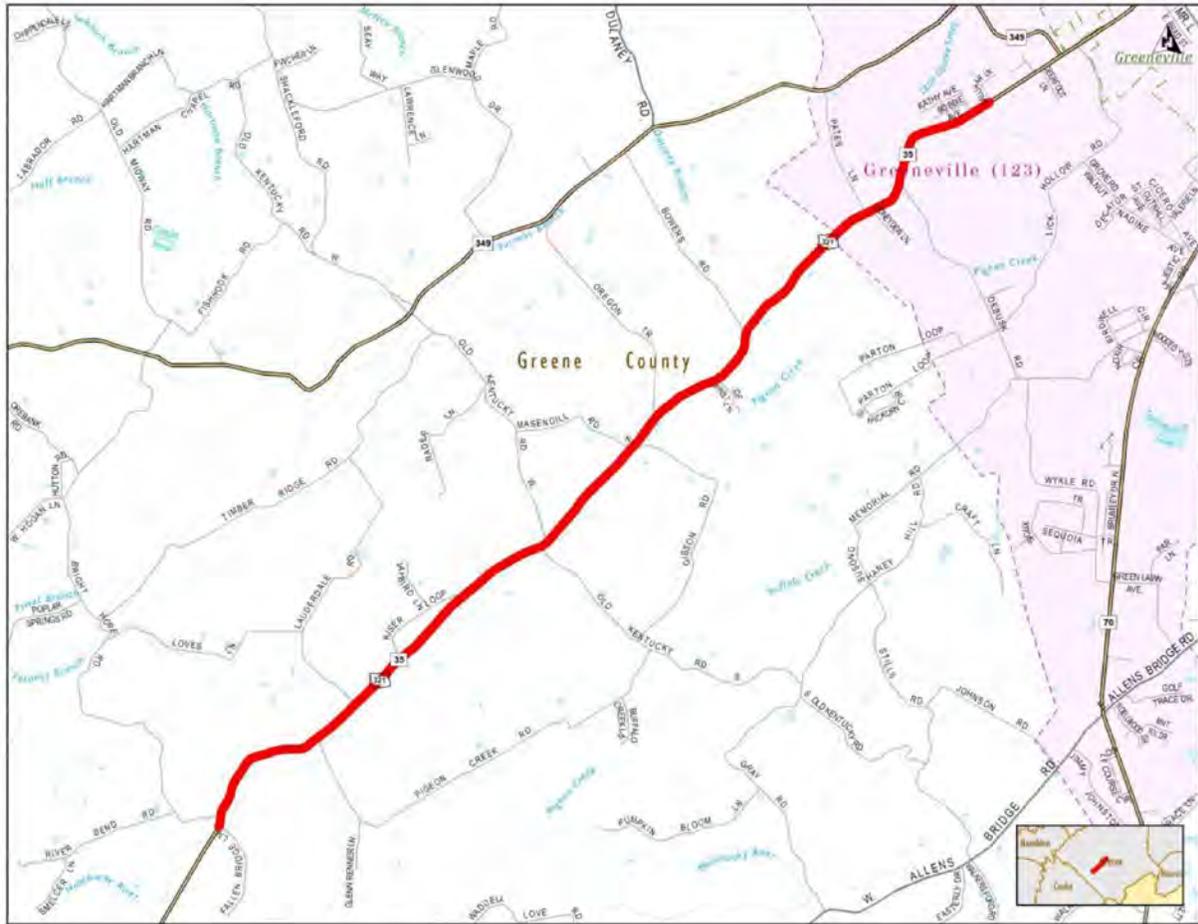
County:
GREENE

Total Cost	TIP No.	Conformity Status	LRP No.
\$36,210,000			

Termini:
FROM NORTH OF NOLICHUCKY RIVER NEAR BRIGHT HOPE ROAD TO 0.918 MILES SOUTH OF SR-349 (WARRENSBURG ROAD) NEAR PATE LANE (EPD) (IA)

Description:
CONSTRUCT SUPER 2-LANE ON 5-LANE ROW ON NEW LOCATION

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	ROW	STBG	\$6,100,000	\$4,880,000	\$1,220,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17352458539	101593.01	2.9	SR-458			TDOT

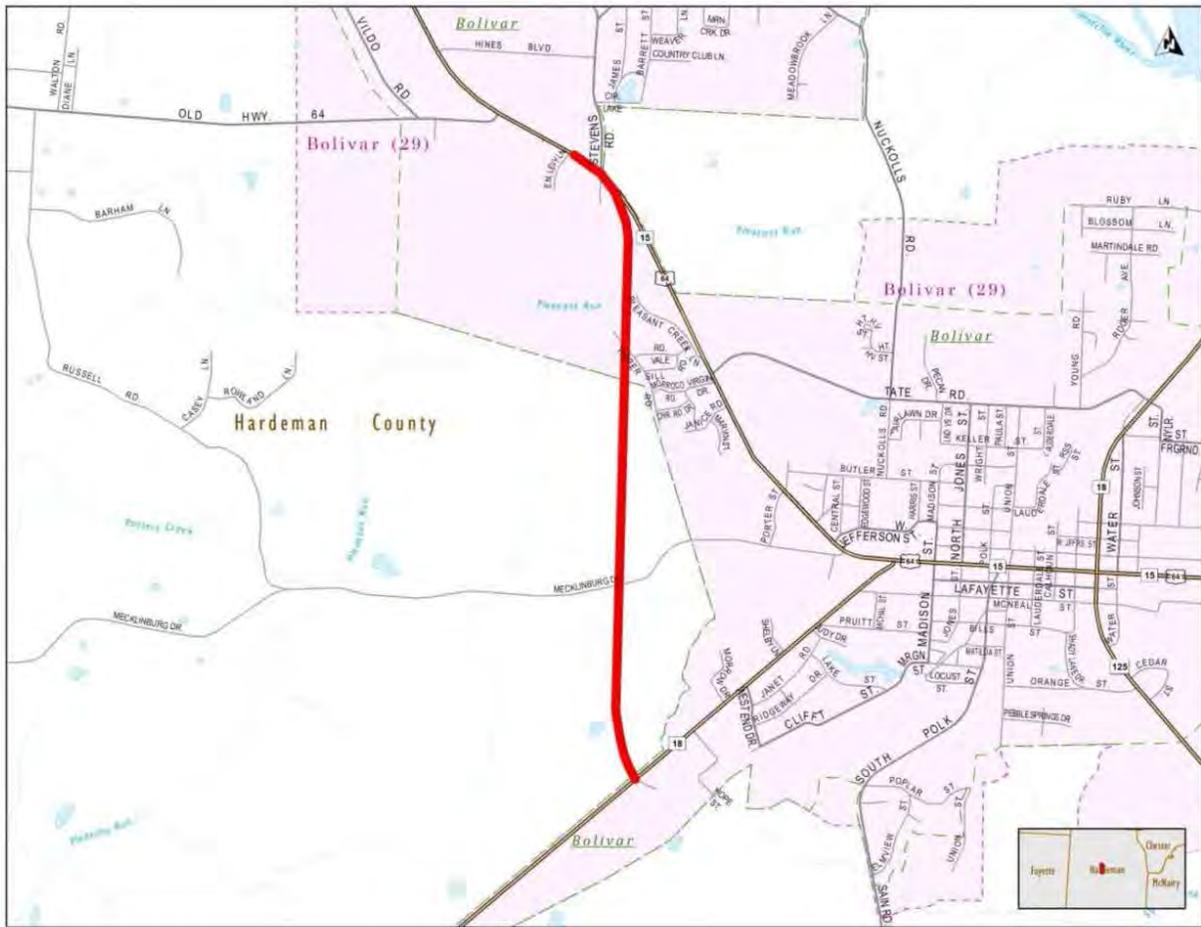
County:
HARDEMAN

Total Cost	TIP No.	Conformity Status	LRP No.
\$24,890,000			

Termini:
FROM SR-15 (US-64) WEST OF BOLIVAR TO 0.2 MILE EAST OF SR-18 (EPD) (IA)

Description:
CONSTRUCT 2-LANES ON A 4-LANE RIGHT-OF-WAY ON NEW LOCATION

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	NHPP	\$1,700,000	\$1,360,000	\$340,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17352458540	101593.02	2.797	SR-458 (US-64)			TDOT

County:

HARDEMAN

Total Cost	TIP No.	Conformity Status	LRP No.
\$29,676,000			

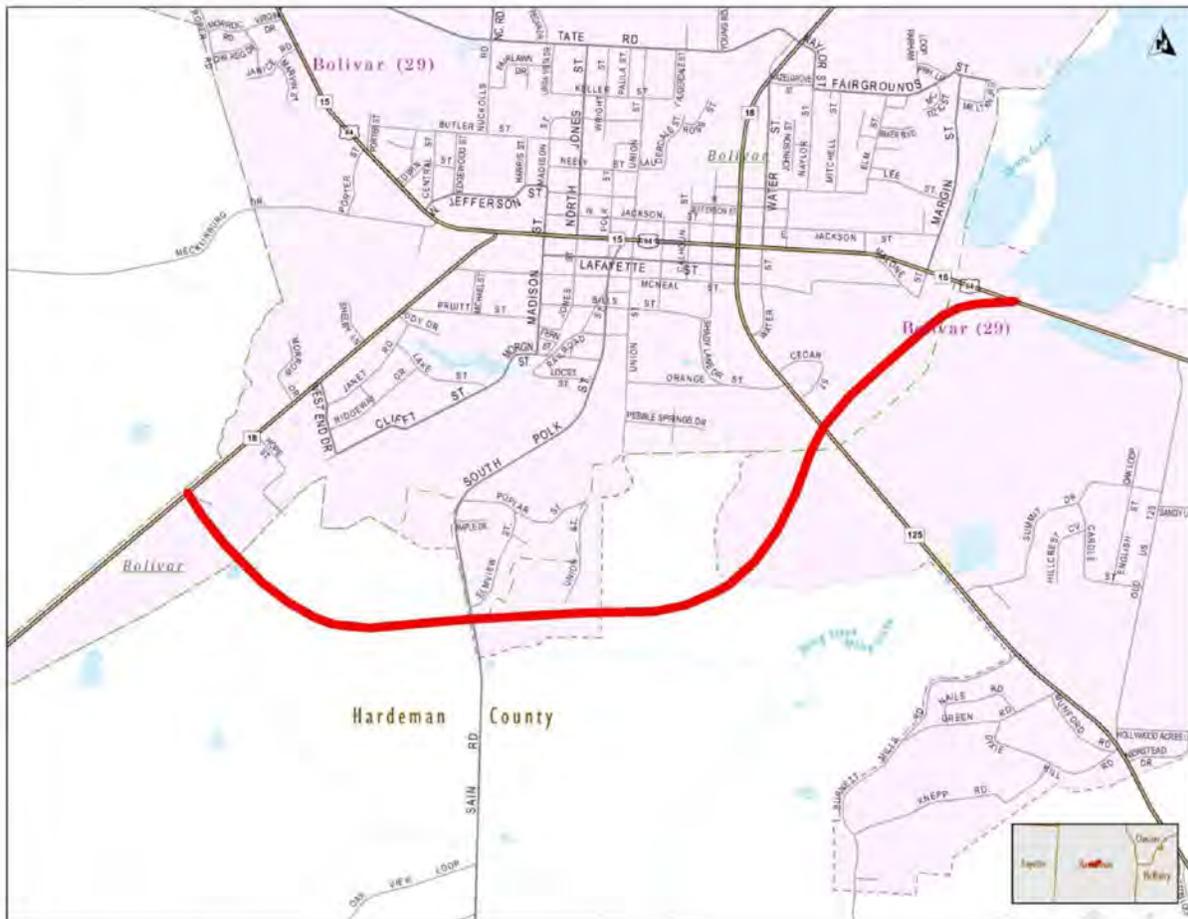
Termini:

(BOLIVAR BYPASS & SR-15(US-64)), FROM EAST OF SR-18 TO WEST OF OLD MIDDLETON ROAD (IA)

Description:

CONSTRUCT 2 LANES ON A 4-LANE RIGHT-OF-WAY ON NEW LOCATION FROM NEAR SR 18 TO NEAR SR 15 (US-64), THEN WIDEN SR 15 (US-64) FROM 2 LANES TO 5 LANES TO NEAR MIDDLETON ROAD EAST OF BOLIVAR.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	NHPP	\$6,276,000	\$5,020,800	\$1,255,200	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17362128542	101885.01	3.4	SR-128			TDOT

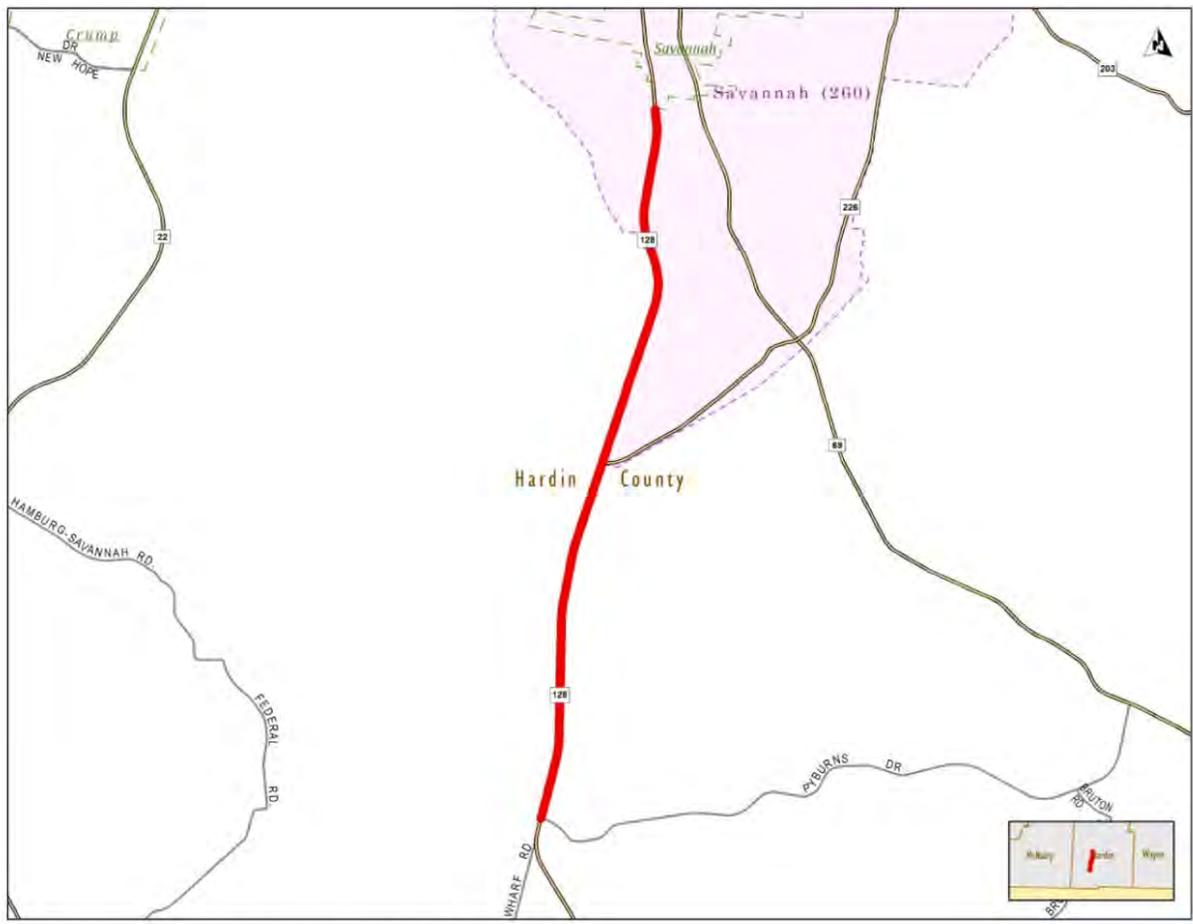
County:
HARDIN

Total Cost	TIP No.	Conformity Status	LRP No.
\$9,330,000			

Termini:
FROM PYBURNS DRIVE TO SR-226 (AIRPORT ROAD) (IA)

Description:
LEFT-TURN LANE AT PYBURNS DRIVE AND SR-226, TRUCK CLIMBING LANE FOR STEEP VERTICAL CURVE JUST NORTH OF HATLEY CREEK. RE-ALIGN TWO(2) INTERSECTIONS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	STBG	\$4,000,000	\$3,200,000	\$800,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17372031543	101394.00	4.3	SR-31			TDOT

County:

HAWKINS

Total Cost	TIP No.	Conformity Status	LRP No.
\$7,300,000			

Termini:

FROM MOORESBURG TO ADAMS LANE (EPD) (IA)

Description:

WIDEN 2-LN TO 3-LN

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	STBG	\$4,400,000	\$3,618,000	\$782,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17372066544	107579.00	5.62	SR-66			TDOT

County:
HAWKINS

Total Cost	TIP No.	Conformity Status	LRP No.
\$33,700,000			

Termini:
FROM SR-34 IN BULLS GAP TO SOUTH OF SPEEDWELL RD/OLD HWY 66 (IA)

Description:
WIDEN EXISTING 2 (10 FT) LANES TO 2 (12 FT) LANES

<u>Fiscal Year</u>	<u>Type of Work</u>	<u>Funding Type</u>	<u>Total Funds</u>	<u>Fed Funds</u>	<u>State Funds</u>	<u>Local Funds</u>
2022	ROW	STBG	\$6,800,000	\$5,440,000	\$1,360,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17382019545	102251.02	4.04	SR-19			TDOT

County:

HAYWOOD

Total Cost	TIP No.	Conformity Status	LRP No.
\$8,700,000			

Termini:

FROM NEAR LAUDERDALE COUNTY LINE TO EAST OF BINFORD LANE (1A)

Description:

WIDEN EXISTING TO A SUPER 2-LANE (12' TRAFFIC LANES WITH 10 FOOT SHOULDERS AND IMPROVE SIGHT DISTANCES)

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	ROW	STBG	\$2,500,000	\$2,000,000	\$500,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17382019546	102251.03	3.88	SR-19			TDOT

County:

HAYWOOD

Total Cost	TIP No.	Conformity Status	LRP No.
\$8,900,000			

Termini:

FROM EAST OF BINFORD LANE TO EAST OF BOBBY MANN ROAD (IA)

Description:

WIDEN EXISTING TO A SUPER 2-LANE (12' TRAFFIC LANES WITH 10 FOOT SHOULDERS AND IMPROVE SIGHT DISTANCES)

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	STBG	\$500,000	\$400,000	\$100,000	\$0



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17382019547	102251.04	3.33	SR-19			TDOT

County:

HAYWOOD

Total Cost	TIP No.	Conformity Status	LRP No.
\$8,900,000			

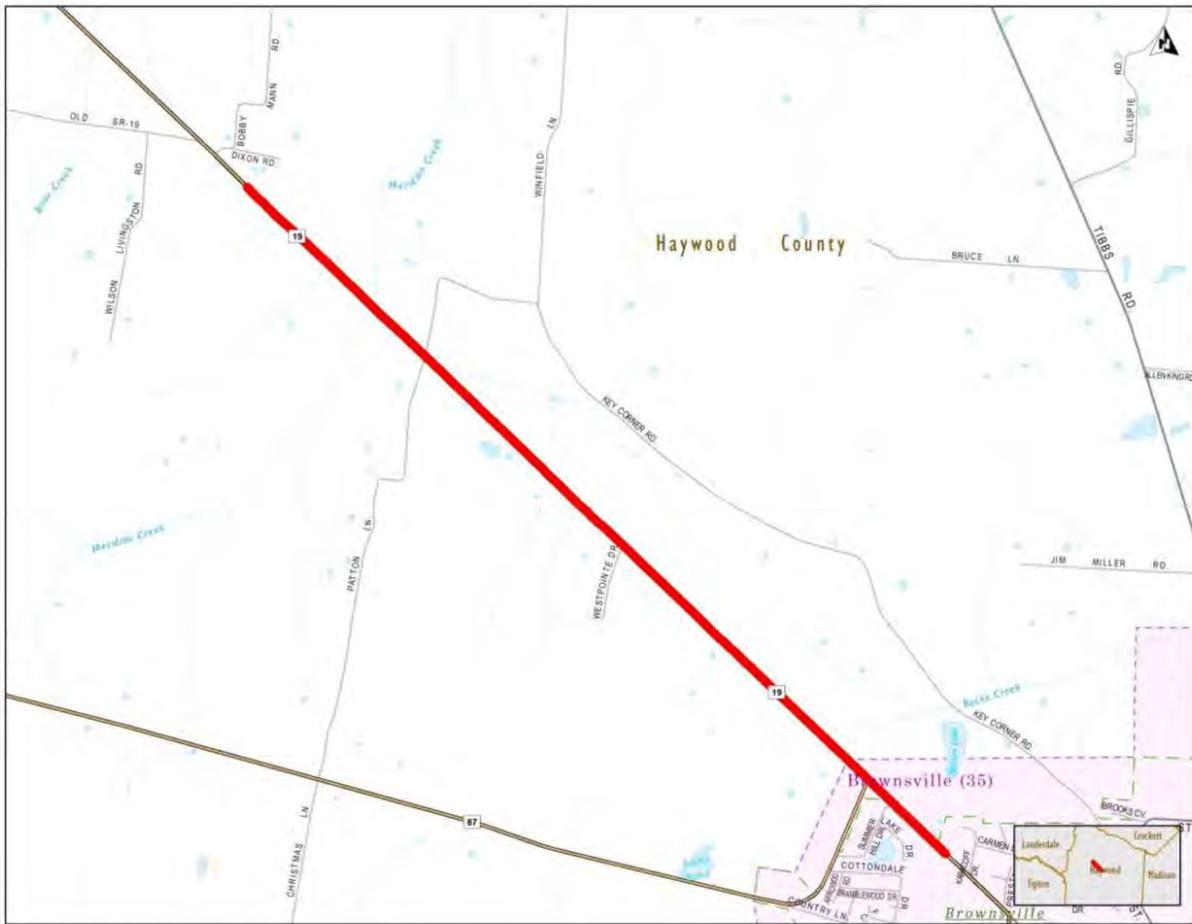
Termini:

FROM EAST OF BOBBY MANN ROAD TO EAST OF SR-87 (IA)

Description:

WIDEN EXISTING TO A SUPER 2-LANE

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	STBG	\$500,000	\$400,000	\$100,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17392459548	100321.01	4.38	SR-459			TDOT

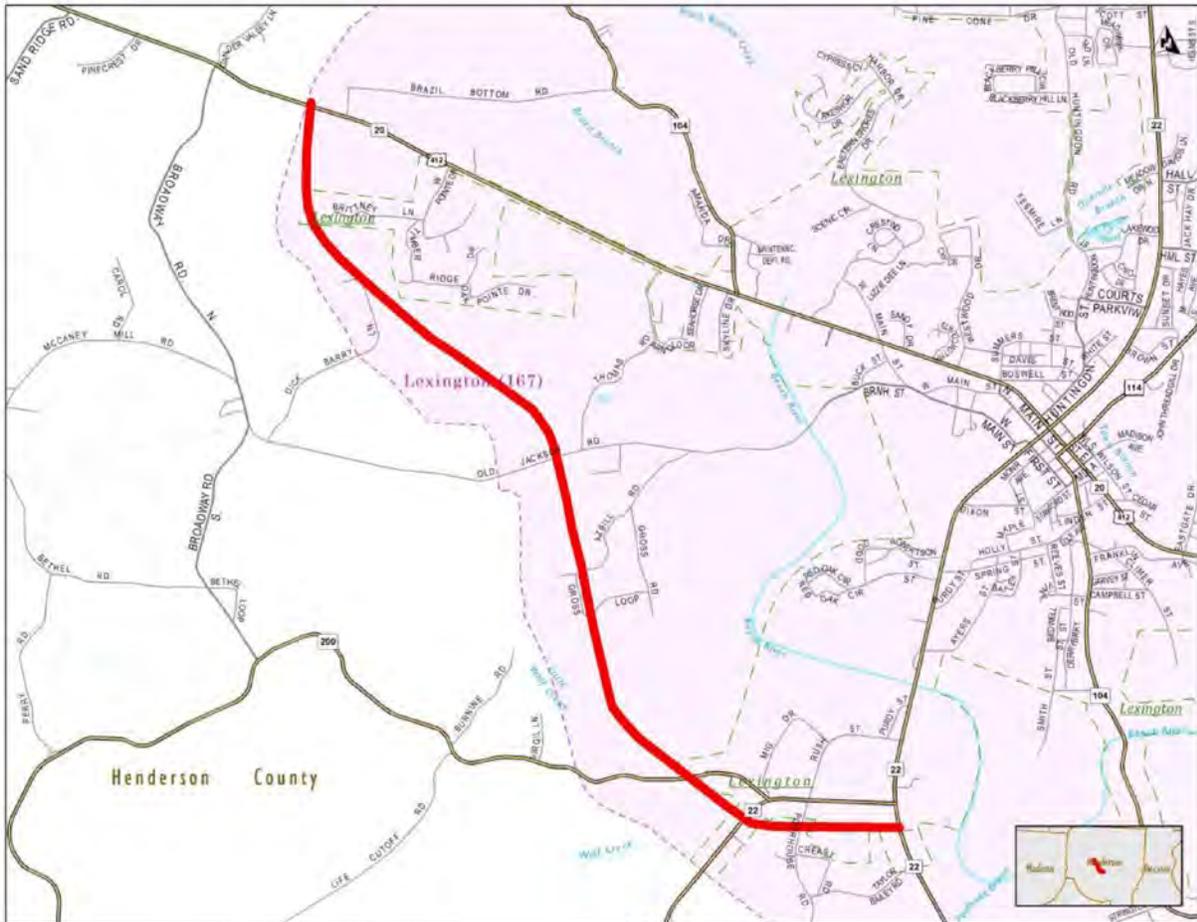
County:
 HENDERSON

Total Cost	TIP No.	Conformity Status	LRP No.
\$60,000,000			

Termini:
 ((LEXINGTON BYPASS), FROM SR-20 (US-412), WEST OF LEXINGTON TO NEAR SR-22, SOUTH OF LEXINGTON (***) (IA) #1 PRIORITY

Description:
 SUPER 2-LANE BYPASS ON NEW ALIGNMENT

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	STBG	\$29,700,000	\$23,760,000	\$5,940,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20402054784	101886.02	9.46	SR-54			TDOT

County:

HENRY

Total Cost	TIP No.	Conformity Status	LRP No.
\$36,600,000			

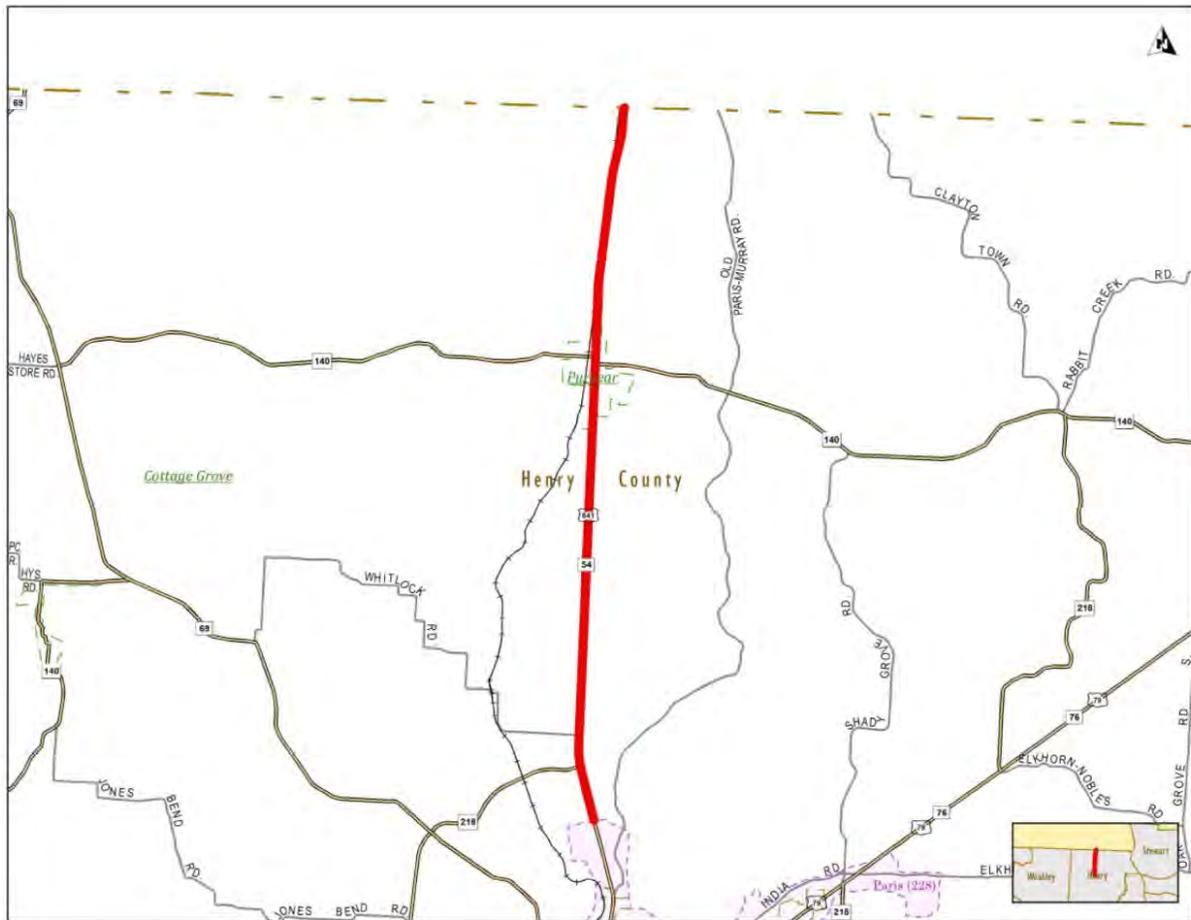
Termini:

FROM NEAR SMITH ROAD IN PARIS TO CROSSLAND ROAD/BRANNON LANE (NORTH OF PURYEAR) (IA)

Description:

CONSTRUCT 3-LANE ON 5-LANE ROW

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	PE-N	NHPP	\$200,000	\$160,000	\$40,000	
2022	PE-D	NHPP	\$2,400,000	\$1,920,000	\$480,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17412046550	124395.00	6.5	SR-46			TDOT

County:

HICKMAN, DICKSON

Total Cost	TIP No.	Conformity Status	LRP No.
\$83,800,000			

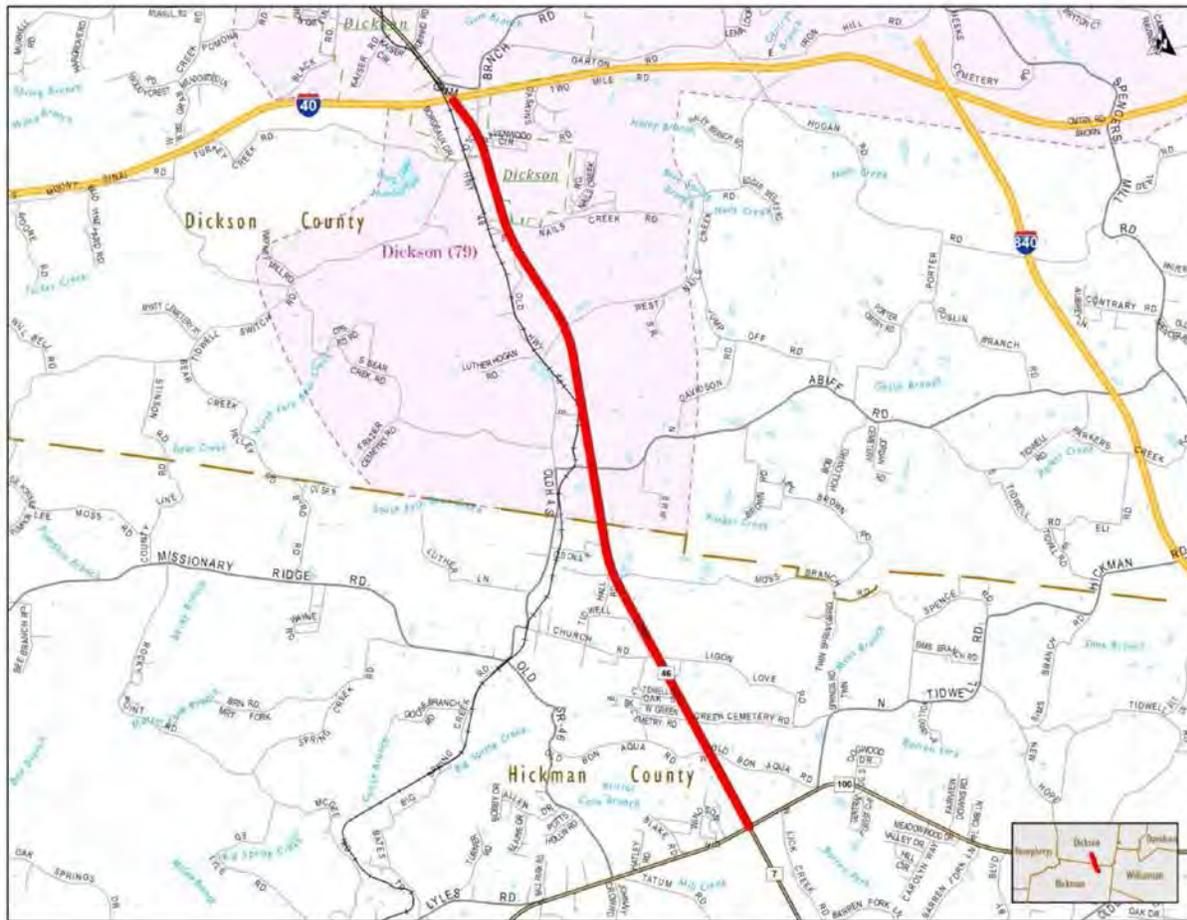
Termini:

FROM NEAR SR-100 IN HICKMAN COUNTY TO NEAR I-40 IN DICKSON COUNTY (IA)

Description:

PRELIMINARY ENGINEERING TO WIDEN FROM 3 LANES TO 5 LANES

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	STBG	\$3,400,000	\$2,720,000	\$680,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17432013551	111109.02	3.41	SR-13			TDOT

County:

HUMPHREYS

Total Cost	TIP No.	Conformity Status	LRP No.
\$8,900,000			

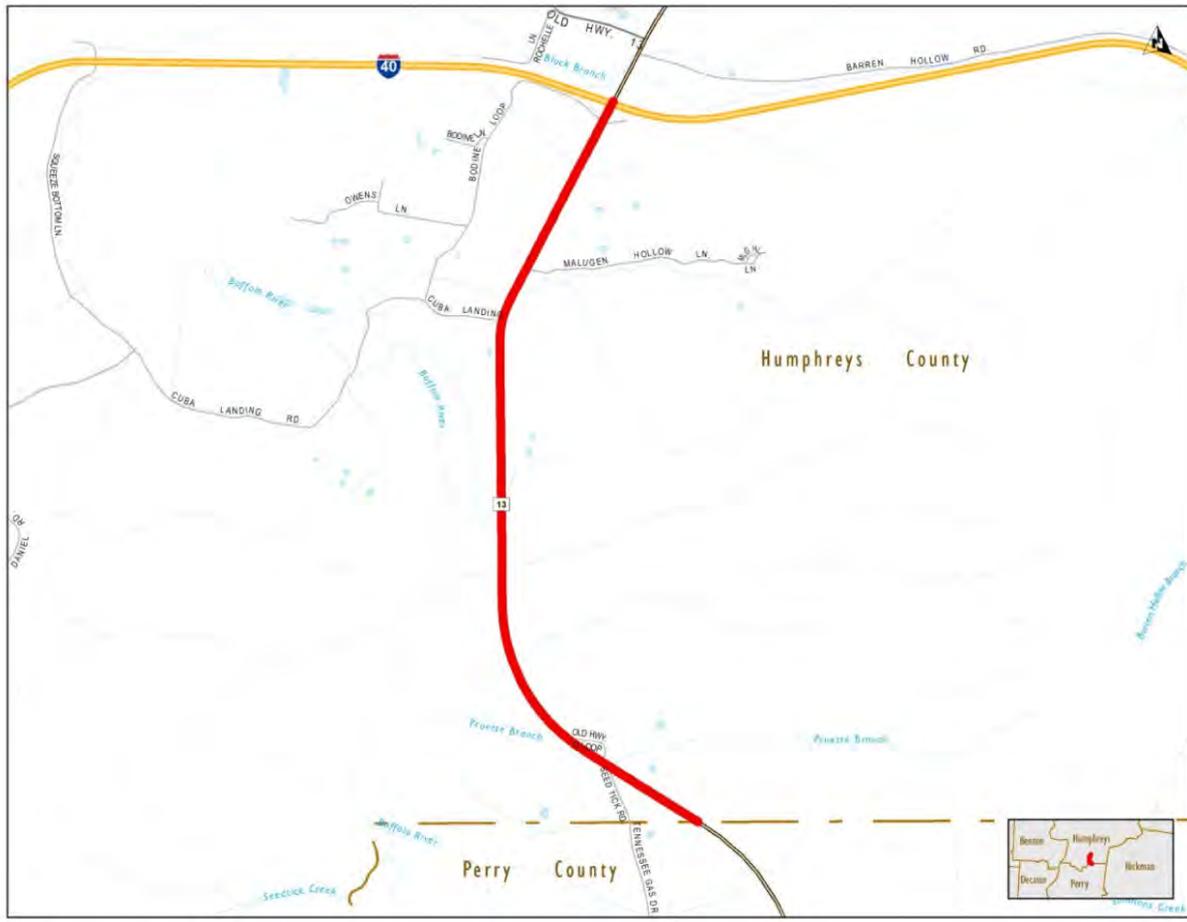
Termini:

FROM NEAR THE PERRY COUNTY LINE TO NEAR I-40 (IA)

Description:

SR-13 SPOT SAFETY IMPROVEMENTS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	ROW	STBG	\$1,200,000	\$960,000	\$240,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17442096552	124083.00	2.23	SR-96			TDOT

County:

JACKSON

Total Cost	TIP No.	Conformity Status	LRP No.
\$9,700,000			

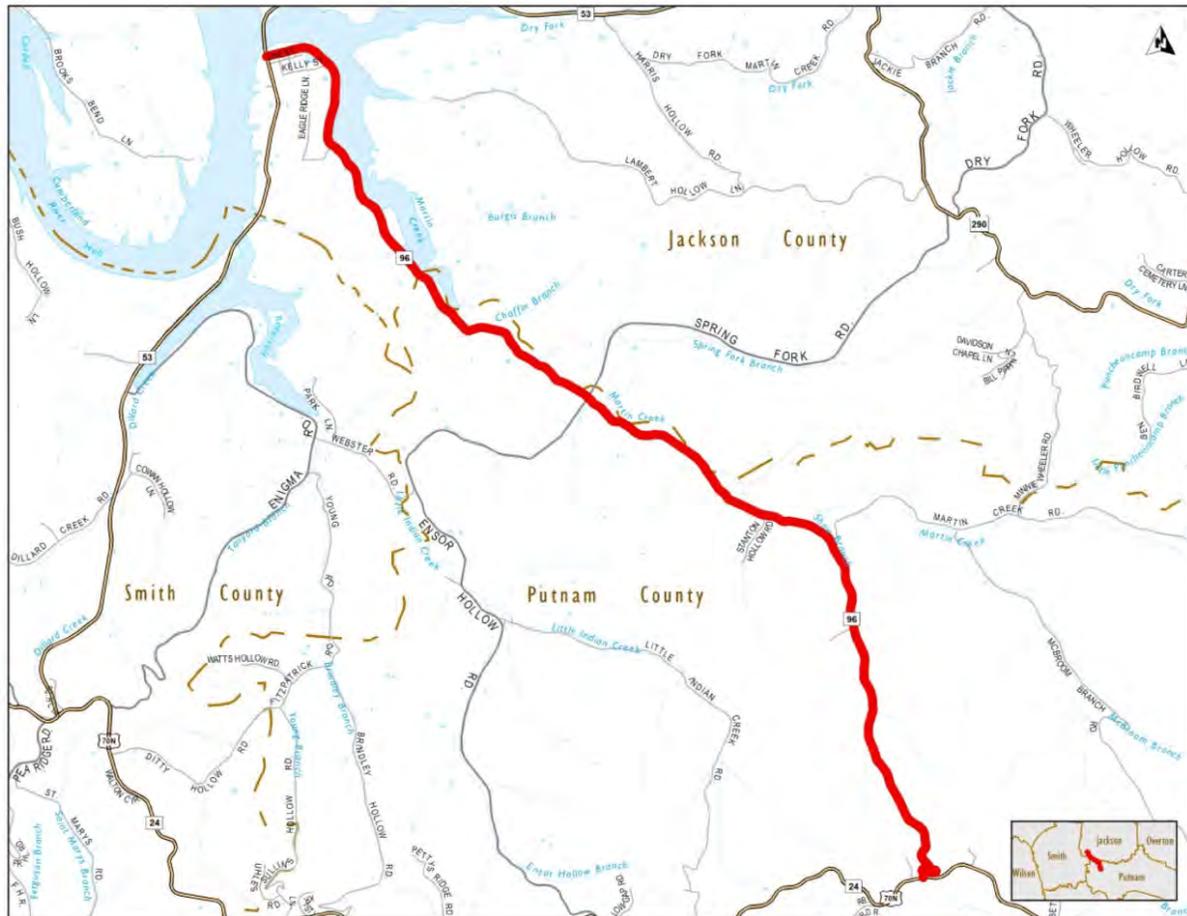
Termini:

FROM SR-53 TO US-70N (SR-24), (SPOT IMPROVEMENTS) (IA)

Description:

SR-96 SPOT SAFETY IMPROVEMENTS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	PE-N	STBG	\$300,000	\$240,000	\$60,000	
2022	PE-D	STBG	\$400,000	\$320,000	\$80,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17482021568	124583.00	5.6	SR-21			TDOT

County:

LAKE, OBION

Total Cost	TIP No.	Conformity Status	LRP No.
\$14,500,000			

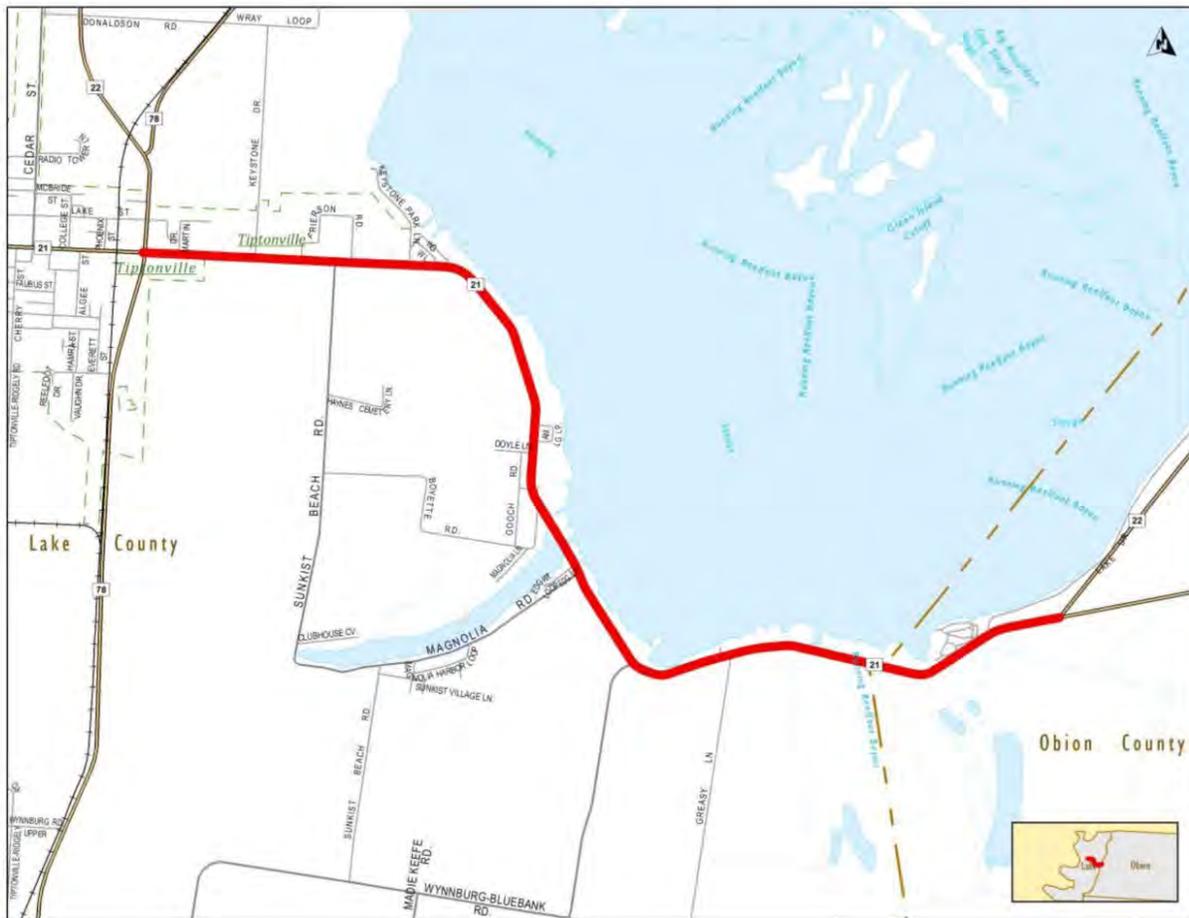
Termini:

FROM SR-78 TO SR-22 (IA)

Description:

WIDEN EXISTING 2-LANE TO 2 12-FOOT TRAVEL LANES, 8-FOOT SHOULDERS COMPATIBLE WITH PEDESTRIAN AND BICYCLE USE, AND SPOT IMPROVEMENT AT INTERSECTIONS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-N	STBG	\$200,000	\$160,000	\$40,000	
2020	PE-D	STBG	\$500,000	\$400,000	\$100,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17492019569	102251.01	3.9	SR-19			TDOT

County:

LAUDERDALE

Total Cost	TIP No.	Conformity Status	LRP No.
\$12,600,000			

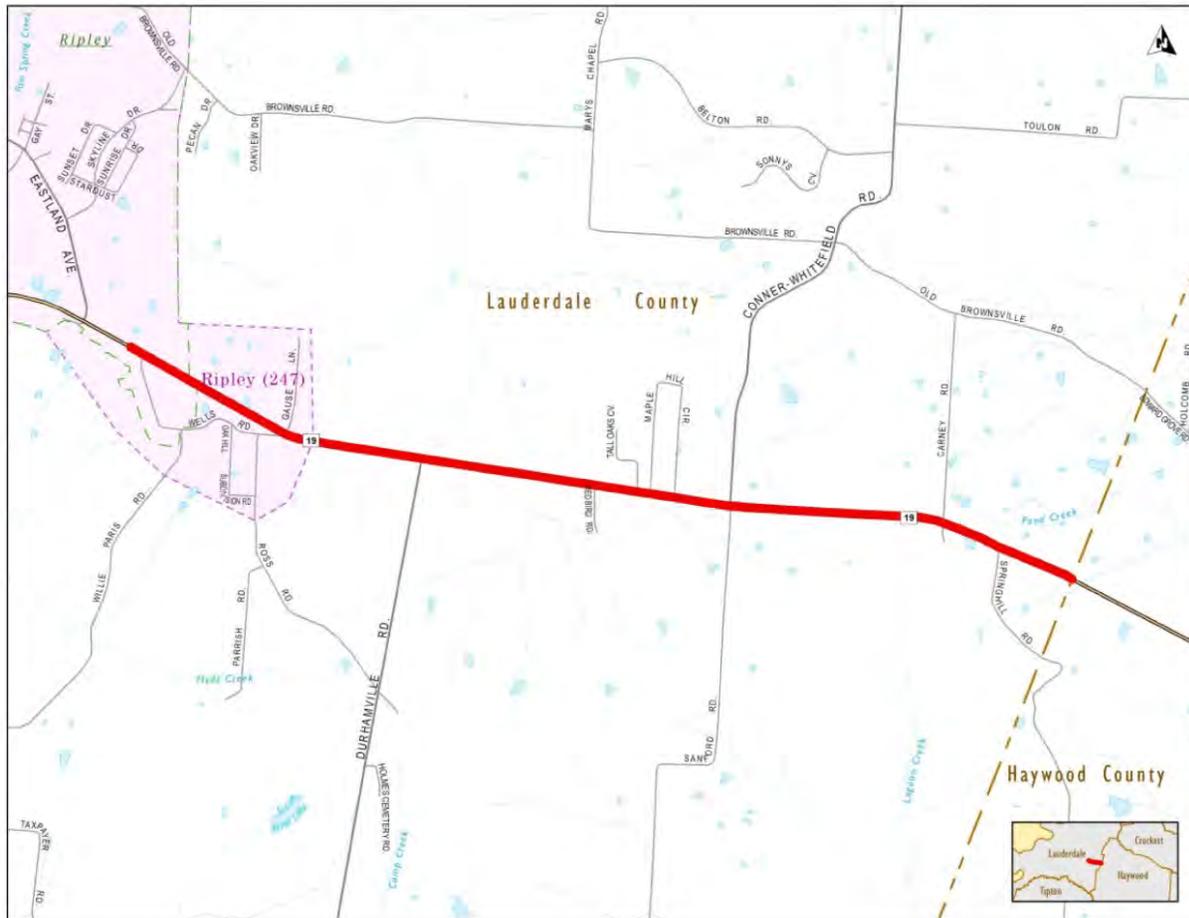
Termini:

FROM EAST OF EASTLAND AVENUE TO HAYWOOD COUNTY LINE (***) (IA)

Description:

RECONSTRUCT EXISTING 2-LN TO A SUPER 2-LN (12 FOOT LANES WITH 10 FOOT SHOULDERS AND IMPROVE SIGHT DISTANCES)

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	STBG	\$300,000	\$240,000	\$60,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17522010570	124496.00	1.37	SR-10 (US-231)			TDOT

County:

LINCOLN

Total Cost	TIP No.	Conformity Status	LRP No.
\$14,100,000			

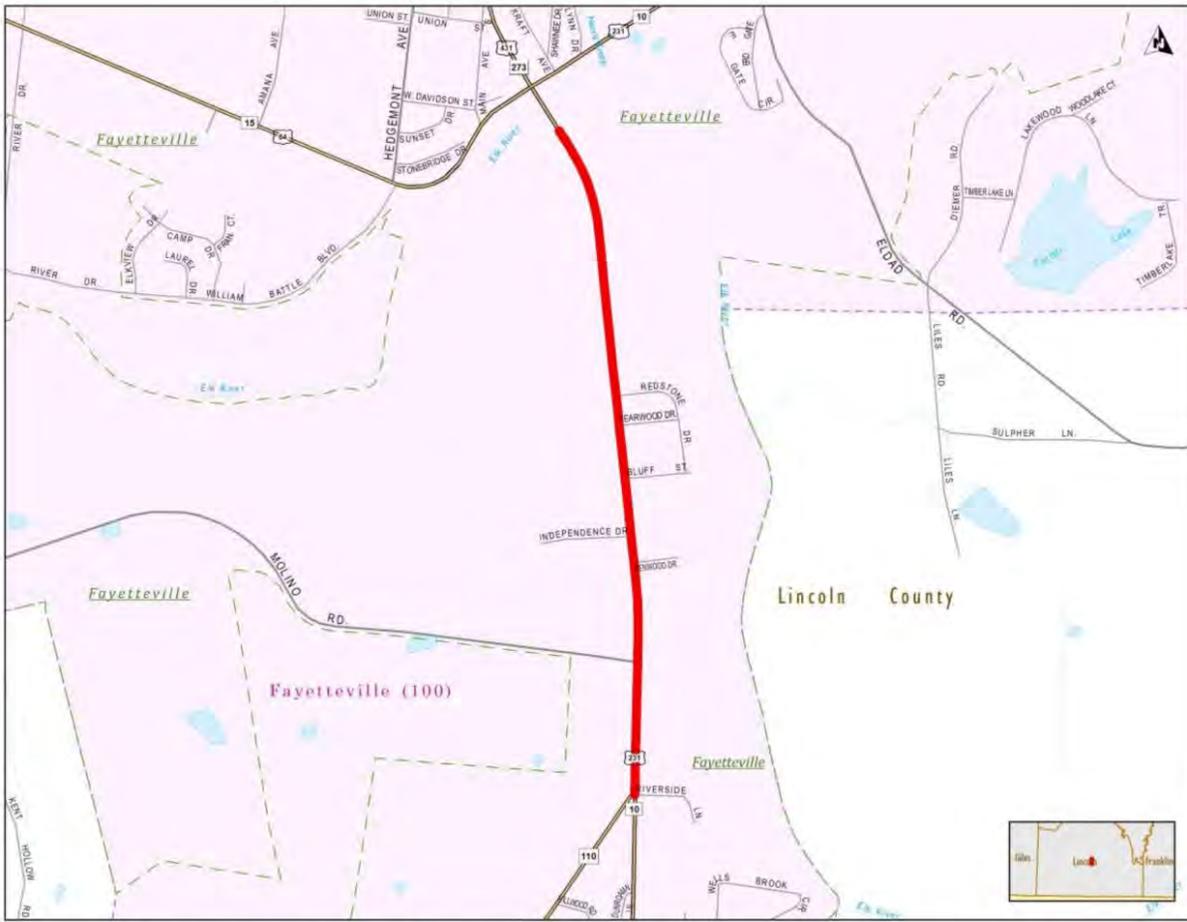
Termini:

(US-231, HUNTSVILLE HWY), FROM SR-110 TO SOUTH OF BRIDGE OVER ELK RIVER (IA)

Description:

SIGNAL SYSTEM UPGRADES AND ACCESS MANAGEMENT.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	PE-N	NHPP	\$50,000	\$40,000	\$10,000	
2021	PE-D	NHPP	\$50,000	\$40,000	\$10,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17582002579	124086.00	3.54	SR-2			TDOT

County:

MARION

Total Cost	TIP No.	Conformity Status	LRP No.
\$66,000,000			

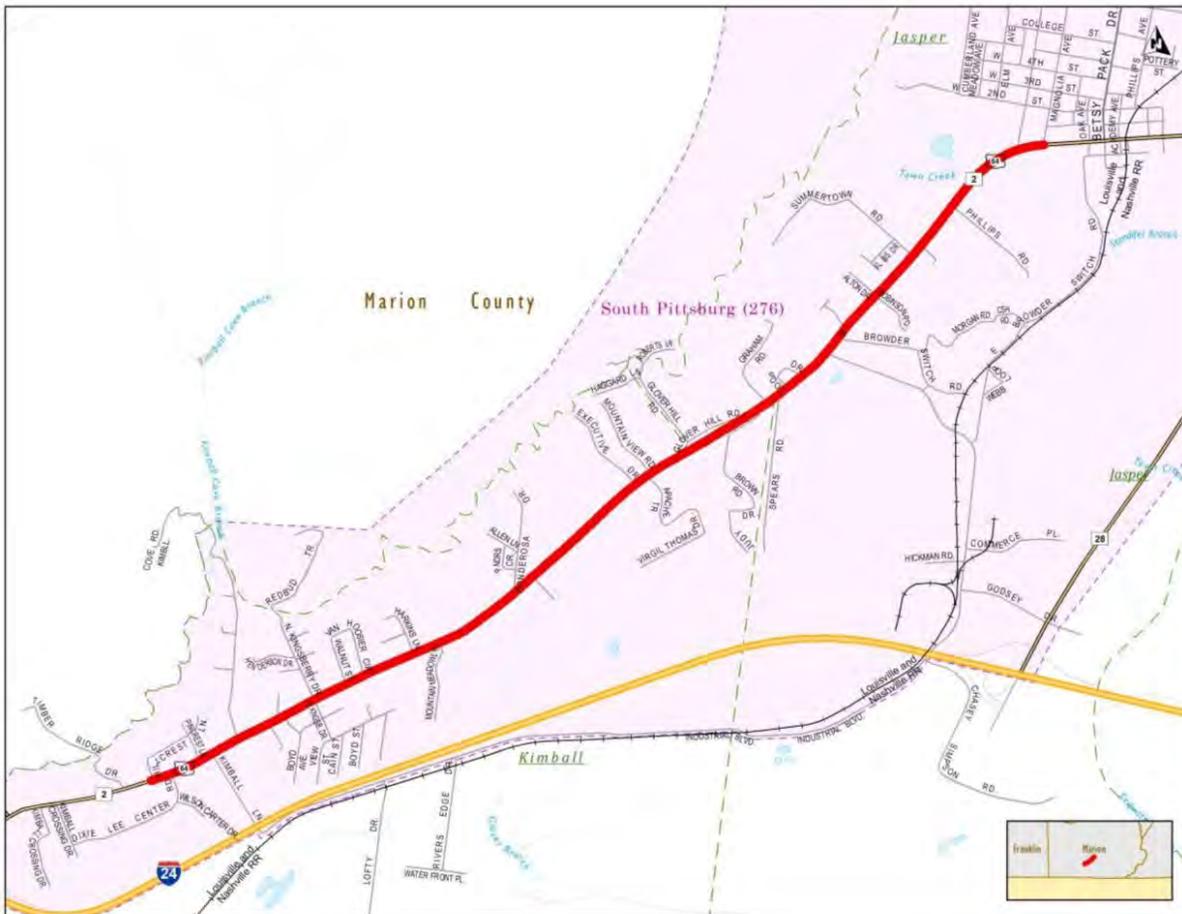
Termini:

FROM NEAR HILLCREST LANE IN KIMBALL TO NEAR MAGNOLIA AVENUE IN JASPER (IA)

Description:

RECONSTRUCT AND WIDEN EXISTING 2 LANE TO A 5 LANE FACILITY INCLUDING PAVED SHOULDERS, CURB AND GUTTER AND SIDEWALKS.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	STBG	\$1,800,000	\$1,440,000	\$360,000	
2022	ROW	STBG	\$8,900,000	\$7,120,000	\$1,780,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17622322582	102380.02	2.03	SR-322			TDOT

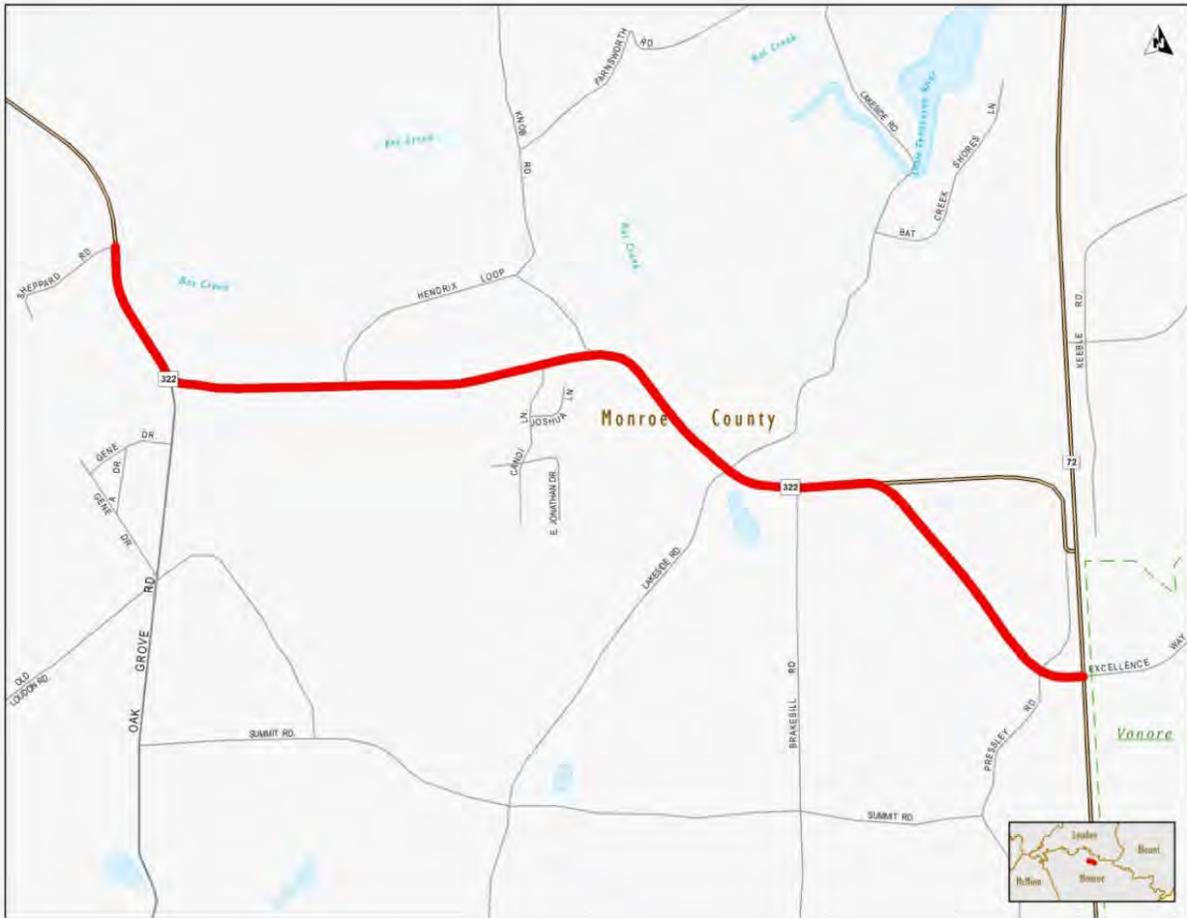
County:
MONROE

Total Cost	TIP No.	Conformity Status	LRP No.
\$10,120,000			

Termini:
FROM SHEPPARD ROAD TO SR-72 (EPD) (IA)

Description:
WIDEN EXISTING ROUTE TO 2 12' TRAVEL LANES AND 8' SHOULDERS; GEOMETRIC IMPROVEMENTS AT THE INTERSECTION OF SR-322 AND SR-72 AND INTERSECTION IMPROVEMENTS AT SR-322 AND OAK GROVE ROAD.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	R-STBG	\$362,439	\$289,951	\$72,488	
2020	ROW	STBG	\$10,137,561	\$8,110,049	\$2,027,512	
2022	CONST	STBG	\$8,500,000	\$6,800,000	\$1,700,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17622322583	102380.03	4.04	SR-322			TDOT

County:

MONROE

Total Cost	TIP No.	Conformity Status	LRP No.
\$12,000,000			

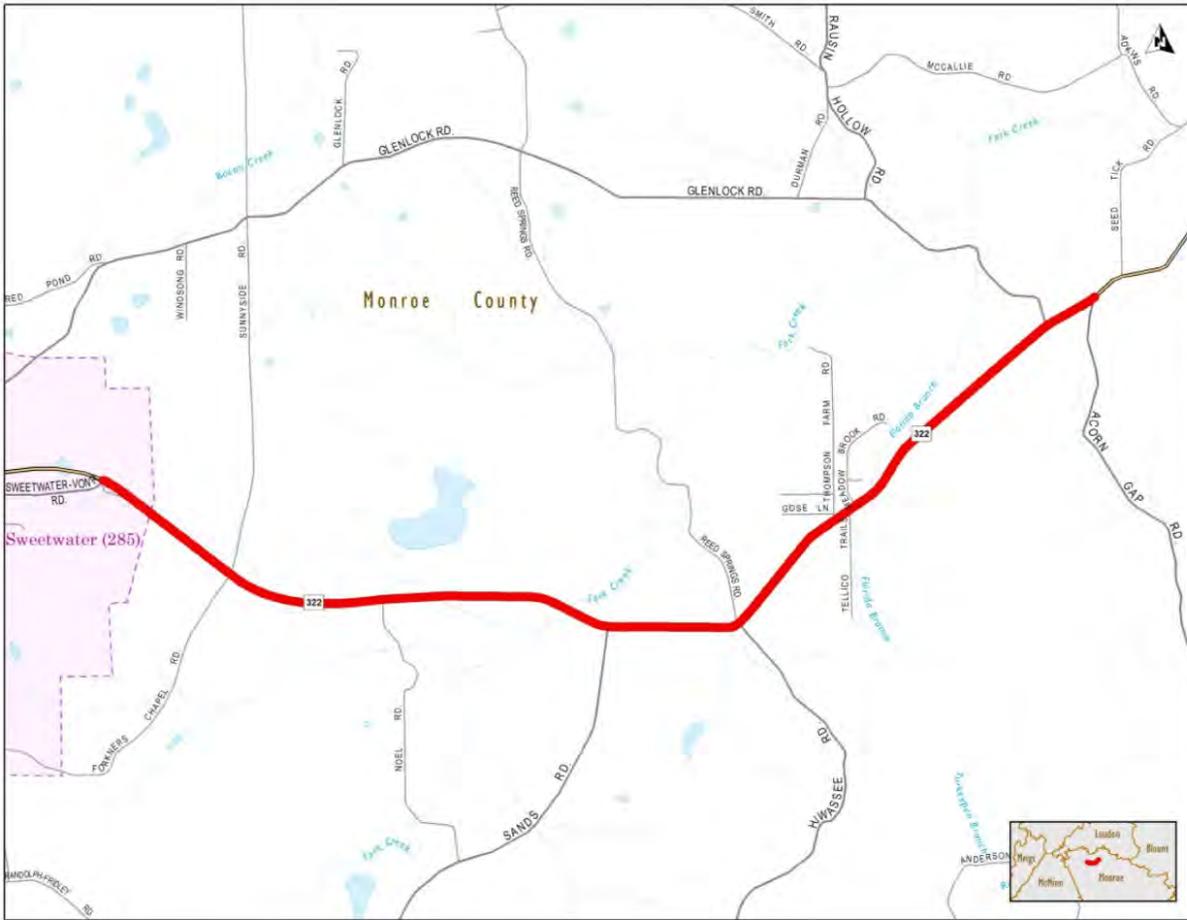
Termini:

FROM SWEETWATER-VONORE ROAD TO NEAR ACORN GAP ROAD (IA)

Description:

WIDEN EXISTING ROUTE TO 2 12' TRAVEL LANES AND 8' SHOULDERS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	ROW	STBG	\$1,150,000	\$920,000	\$230,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17622322584	102380.04	3.07	SR-322			TDOT

County:

MONROE

Total Cost	TIP No.	Conformity Status	LRP No.
\$12,000,000			

Termini:

FROM NEAR ACORN GAP ROAD TO SHEPPARD ROAD (IA)

Description:

WIDEN EXISTING ROUTE TO 2 12' TRAVEL LANES AND 8' SHOULDERS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	ROW	STBG	\$1,150,000	\$920,000	\$230,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17642055593	124669.00	1.82	SR-55			TDOT

County:

MOORE

Total Cost	TIP No.	Conformity Status	LRP No.
\$5,500,000			

Termini:

(LYNCHBURG HIGHWAY), FROM RIDDLE ROAD TO FIVE POINTS ROAD (IA)

Description:

SPOT SAFETY IMPROVEMENTS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	PE-N	NHPP	\$200,000	\$160,000	\$40,000	
2022	PE-D	NHPP	\$200,000	\$160,000	\$40,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17642055594	123070.00	0.2	SR-55			TDOT

County:

MOORE

Total Cost	TIP No.	Conformity Status	LRP No.
\$2,700,000			

Termini:

FROM NEAR THE INTERSECTION OF GOODBRANCH ROAD TO NEAR THE MOORE COUNTY HIGH SCHOOL

Description:

WIDEN FROM 2-LANES TO A 3 LANE FACILITY

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	STBG	\$110,000	\$88,000	\$22,000	
2020	ROW	STBG	\$400,000	\$320,000	\$80,000	
2022	CONST	STBG	\$2,100,000	\$1,680,000	\$420,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17652029595	101411.03	4.93	SR-29 (US-27)			TDOT

County:

MORGAN

Total Cost	TIP No.	Conformity Status	LRP No.
\$64,312,087			

Termini:

NORTH OF RAY CROSS RD/MOSSY GROVE RD(FORMERLY WESTMINSTER RD) TO SR-62 IN WARTBURG (IA)

Description:

WIDEN 2-LN TO 4-LN (PLUS CENTER TURN LANE)

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	R-NHPP	\$457,086	\$365,670	\$91,417	
2020	ROW	NHPP	\$21,542,913	\$17,234,330	\$4,308,583	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17661069596	101343.01	2.87	I-69 PROP			TDOT

County:

OBION

Total Cost	TIP No.	Conformity Status	LRP No.
\$9,000,000			

Termini:

FROM SOUTH OF SR-3(US-51) TO SOUTH OF SR-5 (IA)

Description:

PAVING OF NEW STAGE CONSTRUCTION OF I-69

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	NHPP	\$9,000,000	\$8,100,000	\$900,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17661069597	101344.01	3.98	I-69 PROP			TDOT

County:

OBION

Total Cost	TIP No.	Conformity Status	LRP No.
\$12,000,000			

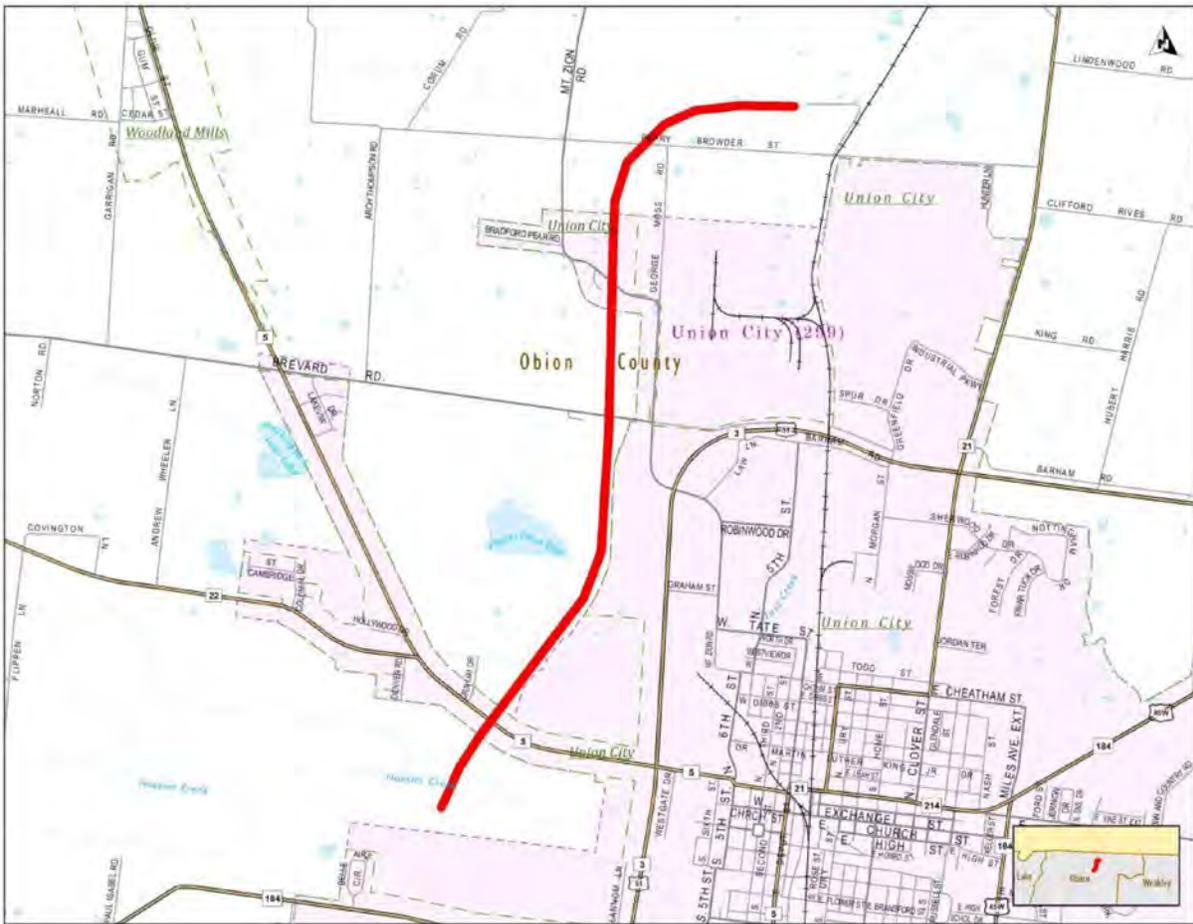
Termini:

FROM SOUTH OF SR-5/22 TO WEST OF SR-21(1A)

Description:

PAVING OF NEW STAGE CONSTRUCTION OF I-69

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	NHPP	\$12,000,000	\$9,600,000	\$2,400,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17661069598	124745.00	5.0	I-69			TDOT

County:

OBION

Total Cost	TIP No.	Conformity Status	LRP No.
\$53,500,000			

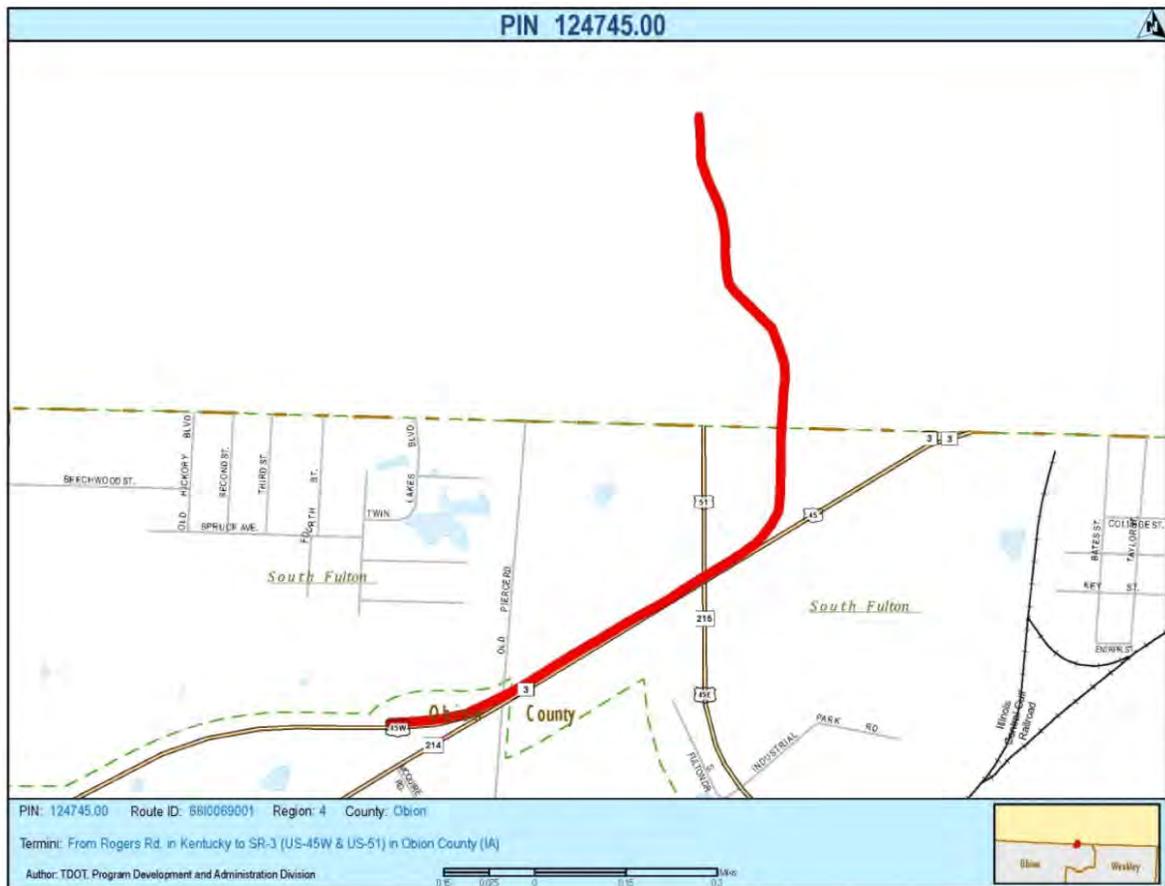
Termini:

FROM ROGERS ROAD IN KENTUCKY TO SR-3 (US-45W & US-51) IN OBION COUNTY (IA)

Description:

CONSTRUCT NEW INTERSTATE

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	PE-N	NHPP	\$2,100,000	\$1,680,000	\$420,000	
2021	PE-D	NHPP	\$900,000	\$720,000	\$180,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17672052599	103678.01	7.11	SR-52			TDOT

County:

OVERTON

Total Cost	TIP No.	Conformity Status	LRP No.
\$2,200,000			

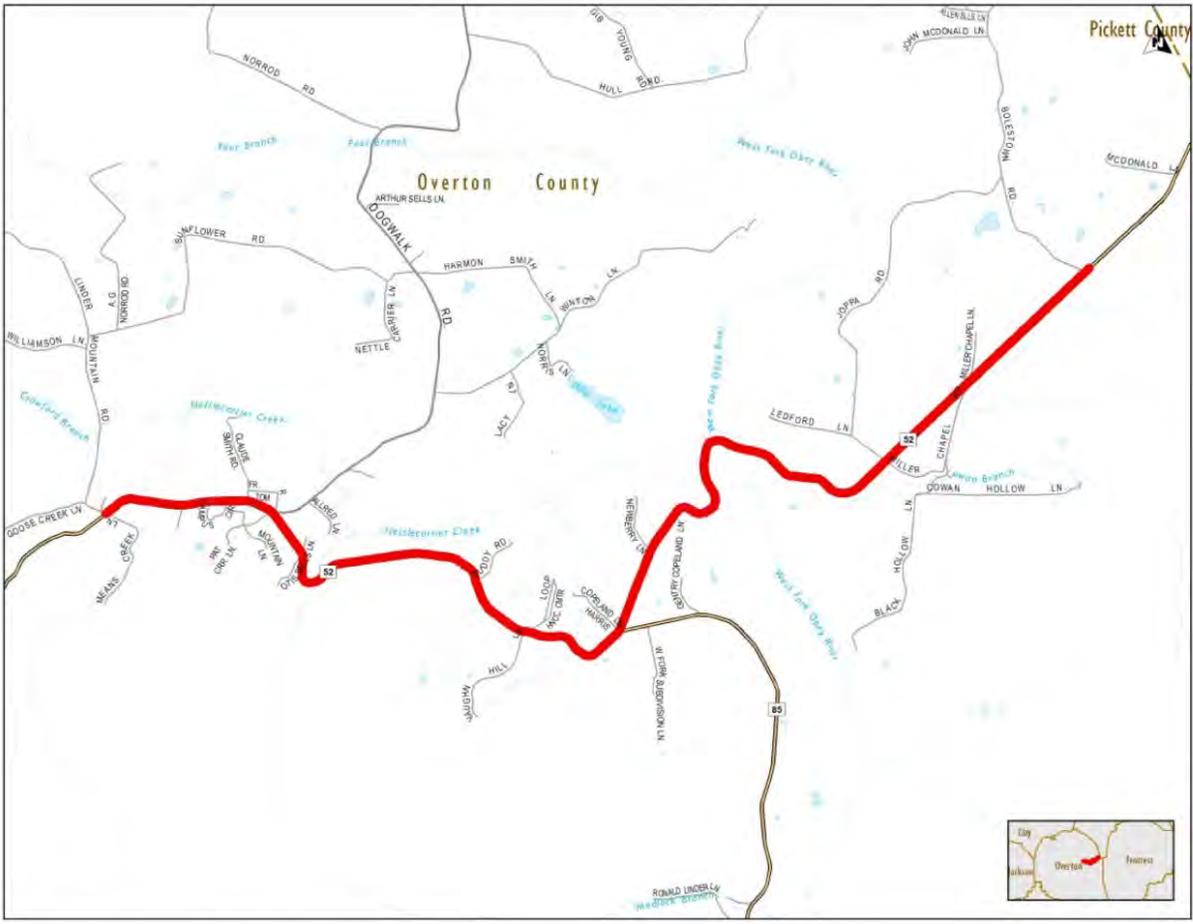
Termini:

FROM WEST OF ALPINE NEAR LINDER MOUNTAIN ROAD TO WEST OF PICKETT COUNTY LINE NEAR BOLESTOWN ROAD (EPD) (IA)

Description:

SPOT IMPROVEMENTS (RE-ALIGNING 3 INTERSECTIONS)

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	CONST	STBG	\$600,000	\$480,000	\$120,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17682013600	111109.03	2.07	SR-13			TDOT

County:

PERRY

Total Cost	TIP No.	Conformity Status	LRP No.
\$8,442,000			

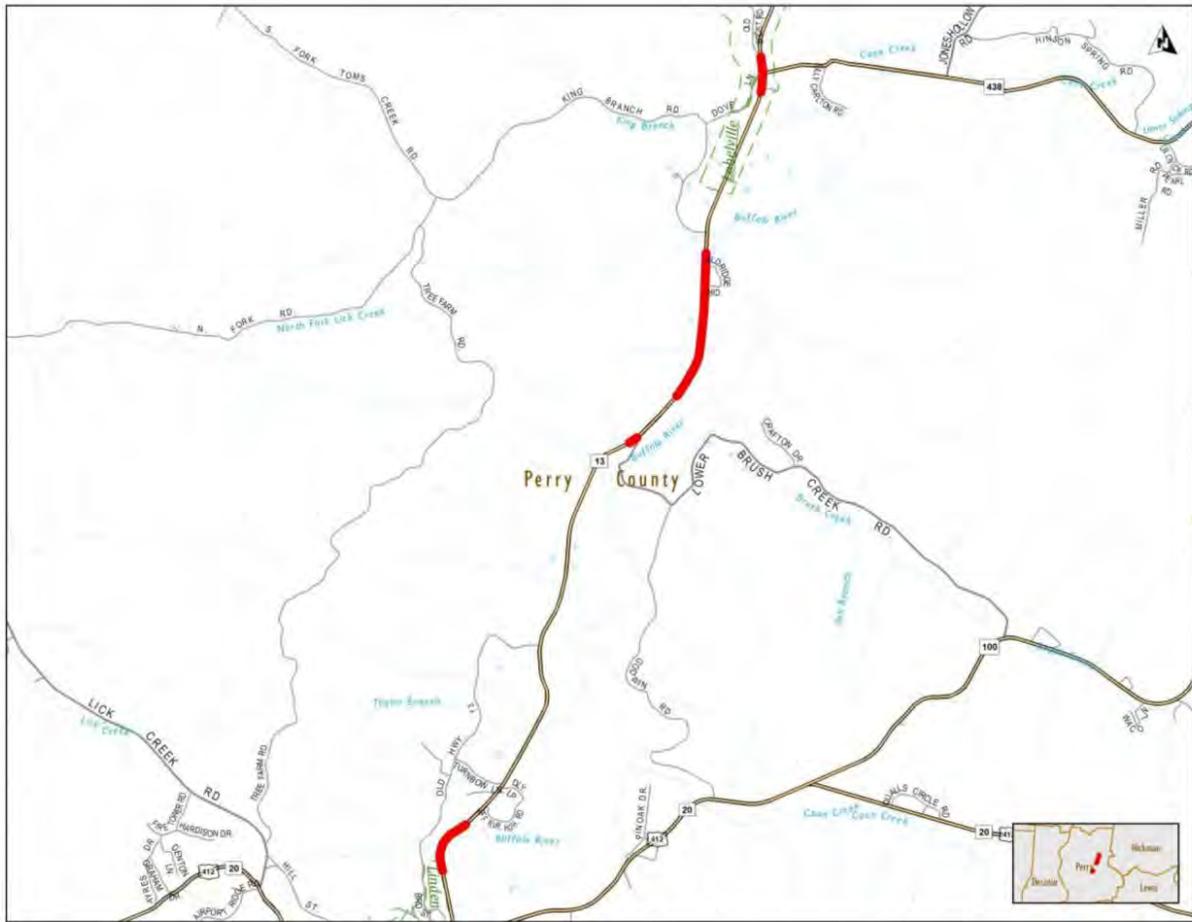
Termini:

FROM SR-20 TO SOUTH OF THE HUMPHREYS COUNTY LINE - 4 SPOT IMPROVEMENTS AT LOCATIONS B, C, D & F (IA)

Description:

WIDENING TO 3-LN. 1 THROUGH LANE IN EACH DIRECTION AND 1 THROUGH LANE IN ALTERNATING DIRECTIONS. PROJECT LENGTH IS TOTAL FOR ALL SPOT IMPROVEMENT LOCATIONS: LOCATION (B) BLM 13.020 - ELM 13.42, LOCATION (C) BLM 16.450 - ELM 16.510, LOCATION (D) BLM 16.910 - ELM 17.940, AND LOCATION (F) BLM 19.130 - ELM 19.380.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	STBG	\$6,600,000	\$5,280,000	\$1,320,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17682013601	111109.01	1.373	SR-13			TDOT

County:

PERRY

Total Cost	TIP No.	Conformity Status	LRP No.
\$7,430,000			

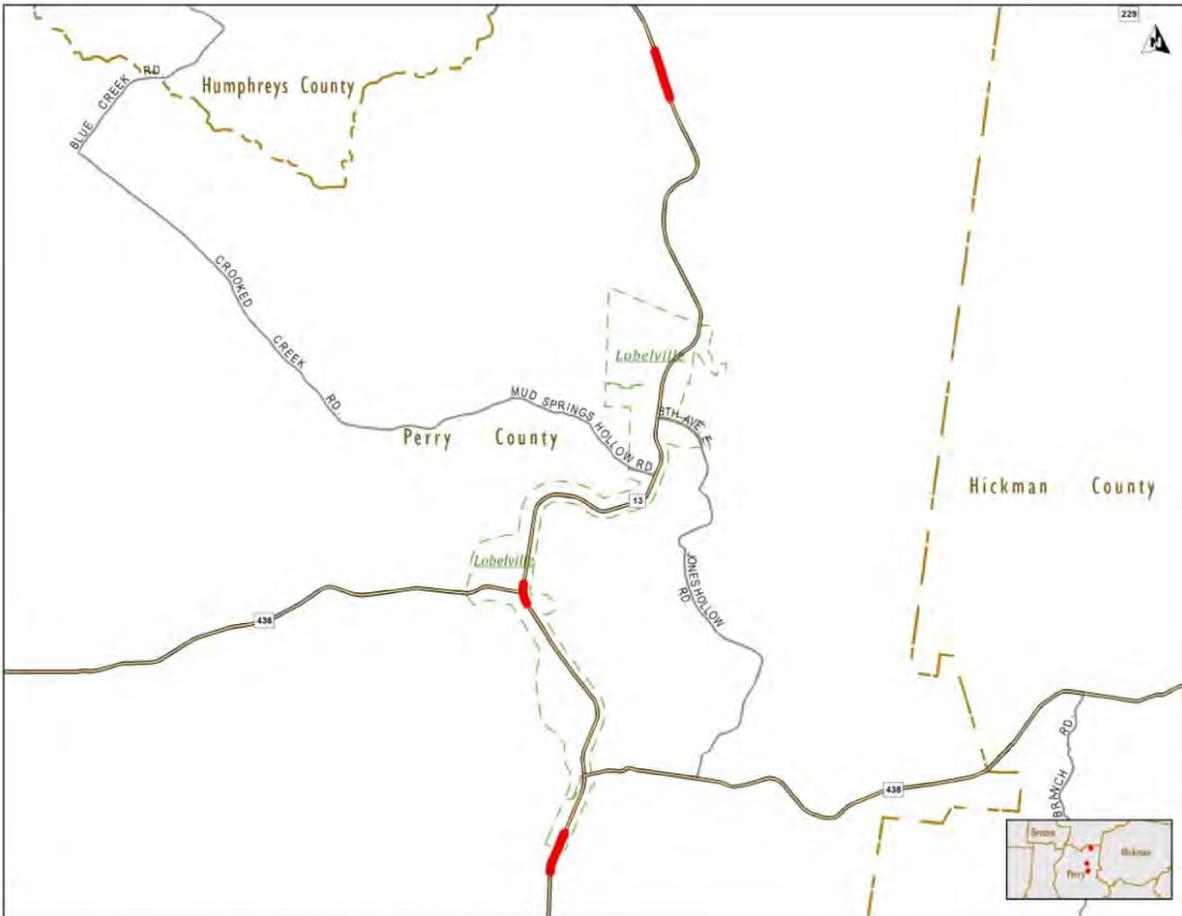
Termini:

FROM SR-20 TO SOUTH OF THE HUMPHREYS COUNTY LINE - 3 SPOT IMPROVEMENTS AT (E, H & K) (IA)

Description:

SR-13, 3 SPOT IMPROVEMENTS AT (E,H AND K), SR-20 TO S OF HUMPHREYS COL - PASSING LN, TURN LN, RD ALIGNMENT

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	STBG	\$6,500,000	\$5,200,000	\$1,300,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17712136603	112538.00	2.24	SR-136			TDOT

County:

PUTNAM

Total Cost	TIP No.	Conformity Status	LRP No.
\$40,615,100			

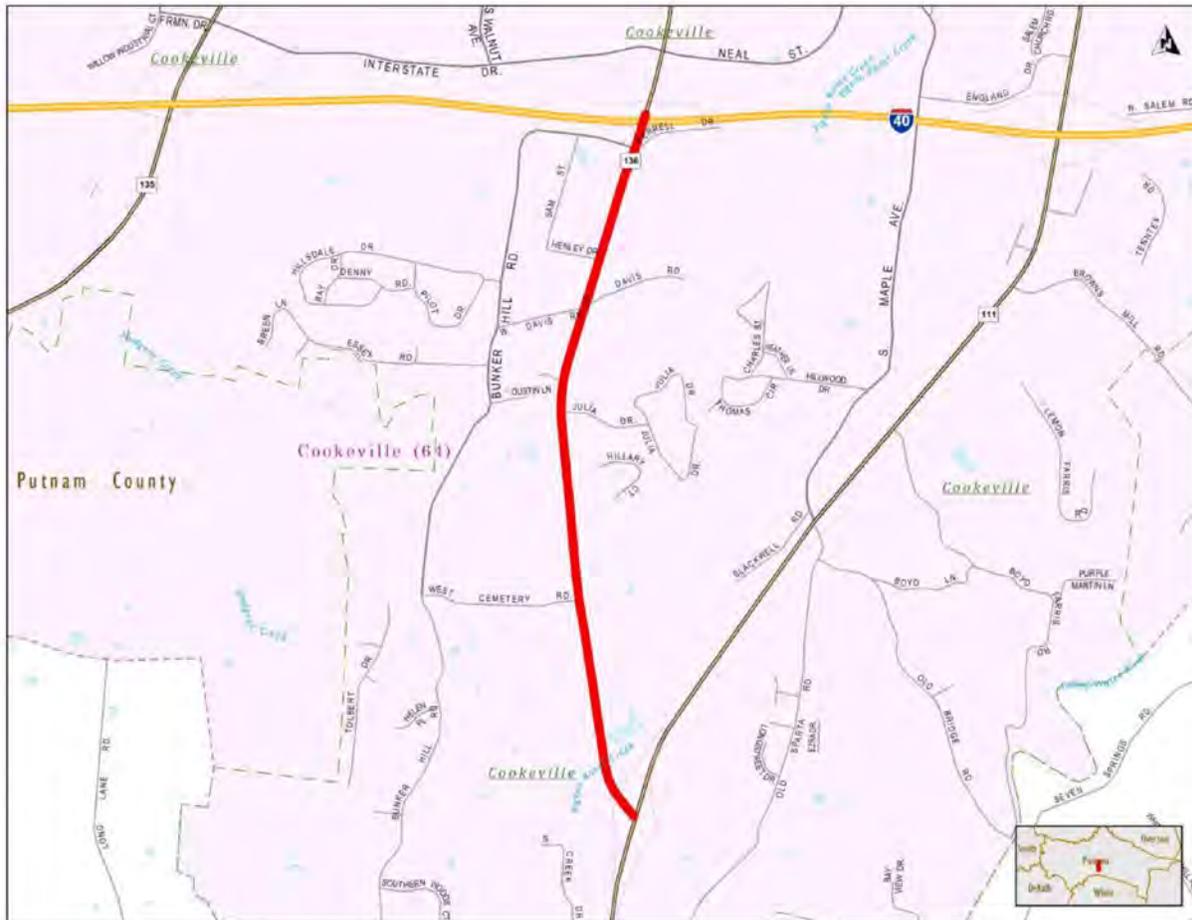
Termini:

SR-111 TO I-40 (IA)

Description:

WIDEN EXISTING 2-LN TO 5-LN WITH 11 FOOT LANES, CURB AND GUTTER, AND SIDEWALK

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	CONST	STBG	\$21,600,000	\$17,280,000	\$4,320,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17722030604	109410.00	8.0	SR-30			TDOT

County:

RHEA

Total Cost	TIP No.	Conformity Status	LRP No.
\$3,760,000			

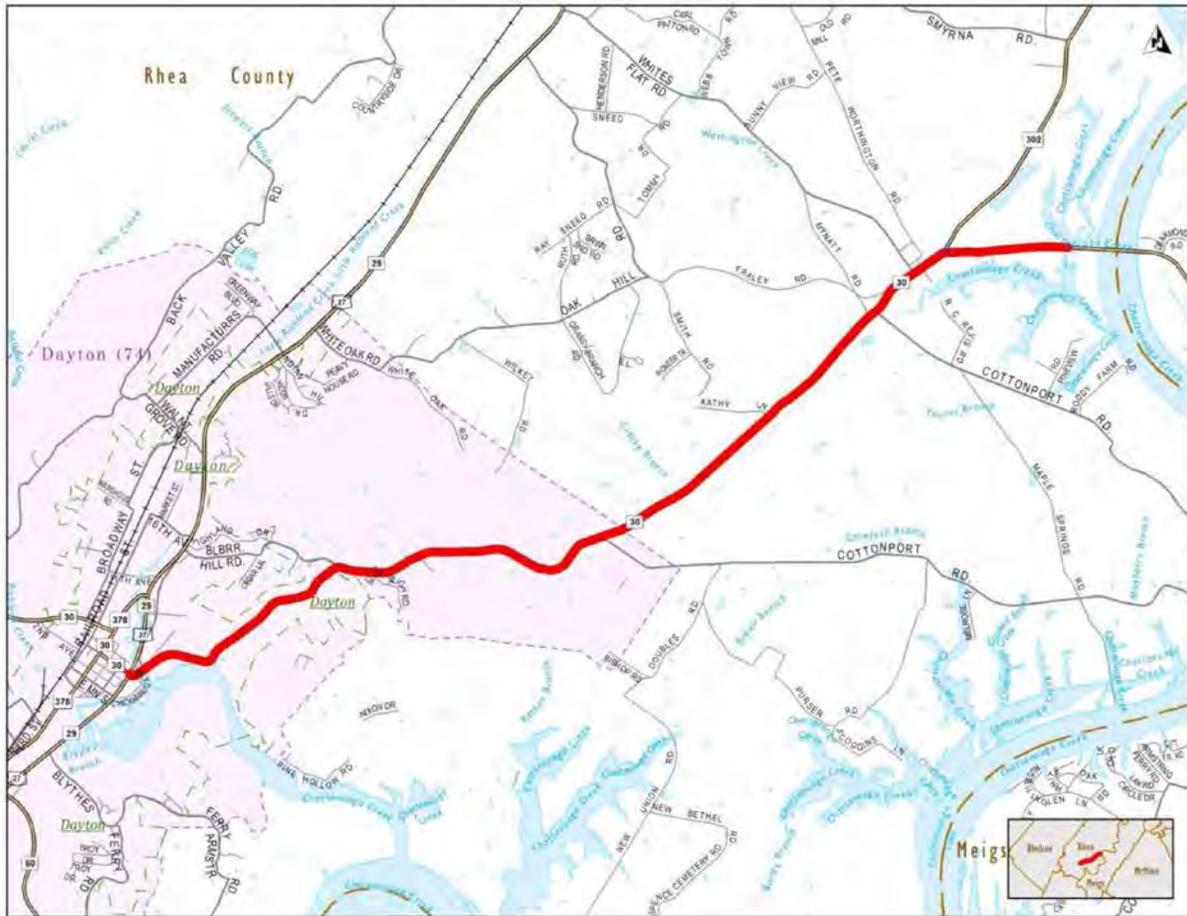
Termini:

FROM SR-29(US-27) TO WEST OF THE TENNESSEE RIVER BRIDGE (IA)

Description:

WIDEN EXISTING 2 LANE TO A 3 LANE FACILITY WITH HORIZONTAL AND VERTICAL CURVATURE IMPROVEMENTS, INTERSECTION IMPROVEMENTS, ADDITIONAL LEFT TURN LANES

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	STBG	\$3,000,000	\$2,400,000	\$600,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17722030605	109410.03	3.28	SR-30			TDOT

County:

RHEA

Total Cost	TIP No.	Conformity Status	LRP No.
\$22,300,000			

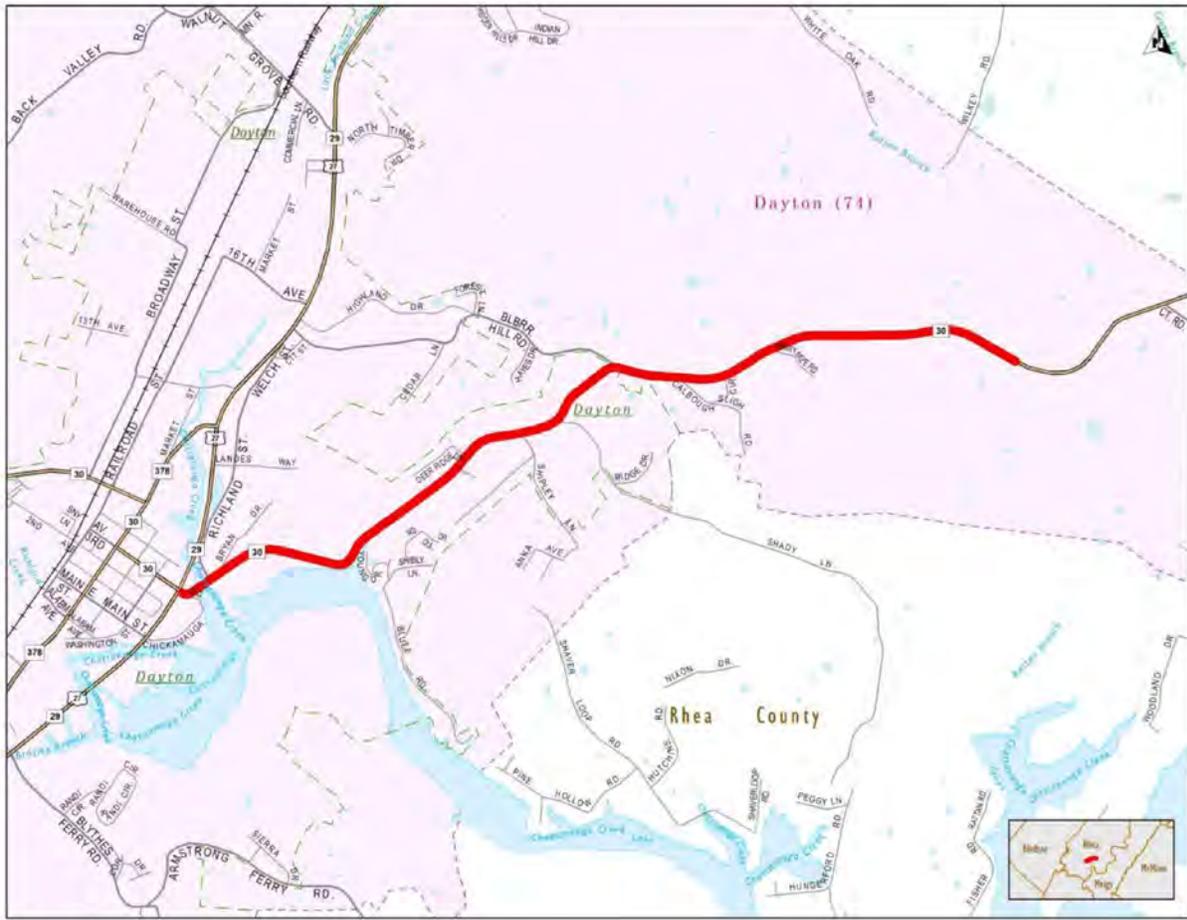
Termini:

(OLD WASHINGTON HWY.) FROM NEAR SR-29 (US-27) TO WEST OF NEW UNION RD. / WHITE OAK RD.(IA)

Description:

WIDEN FROM 2 LANES TO 3 LANES

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2021	ROW	STBG	\$3,300,000	\$2,640,000	\$660,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17732001606	101244.03	3.5	SR-1 (US-70)			TDOT

County:

ROANE

Total Cost	TIP No.	Conformity Status	LRP No.
\$44,700,000			

Termini:

FROM SR-382 TO NEAR RURITAN ROAD (IA)

Description:

WIDEN 2-LN TO 5 LN WITH CENTER TURN LANE

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ROW	STBG	\$9,100,000	\$7,280,000	\$1,820,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17762029614	101414.00	8.66	SR-29 (US-27)			TDOT

County:

SCOTT

Total Cost	TIP No.	Conformity Status	LRP No.
\$58,650,000			

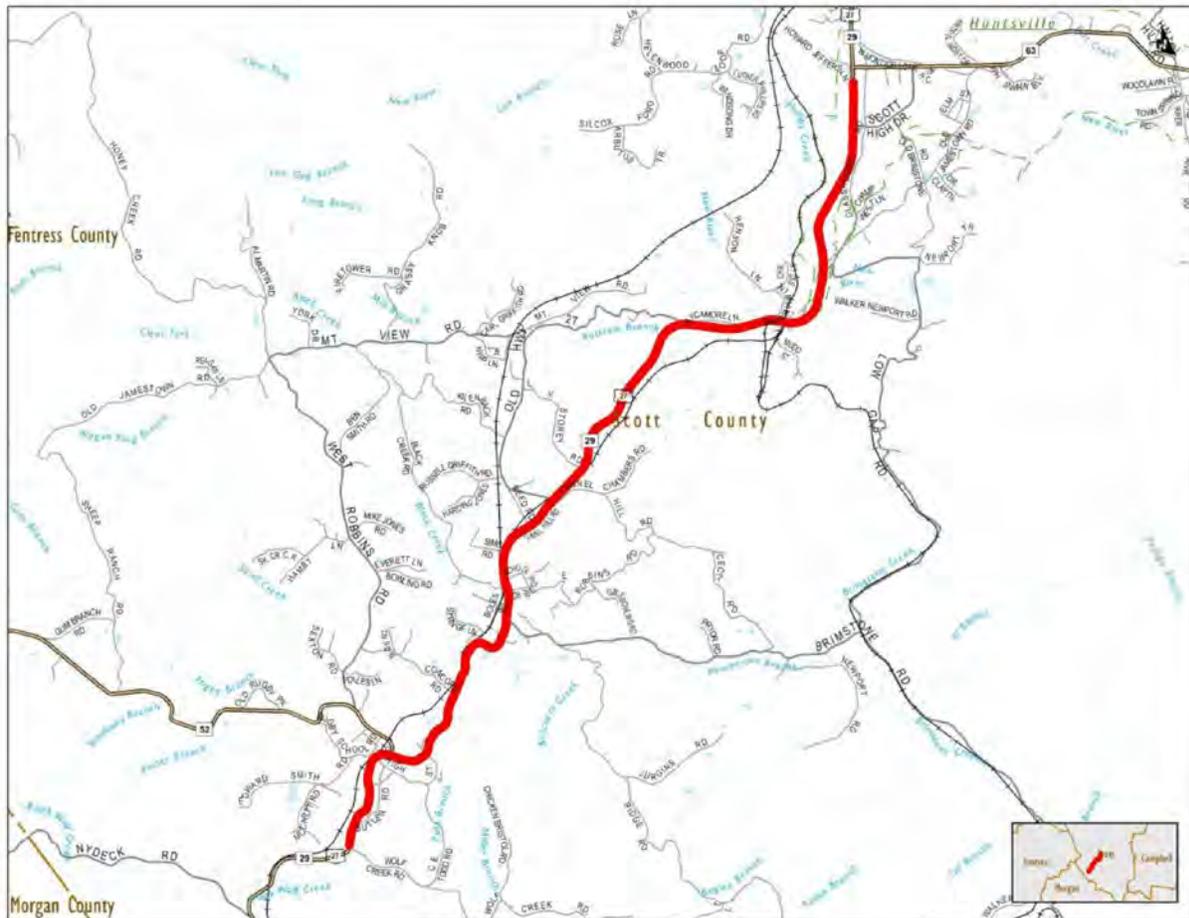
Termini:

FROM NORTH OF WOLF CREEK ROAD TO OLD US-27 AT ROBBINS (EPD)

Description:

CONSTRUCT 3-LN WITH TRUCK CLIMBING LANE ON NEW ALIGNMENT

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	ROW	NHPP	\$9,400,000	\$7,520,000	\$1,880,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17772028615	124104.00	19.06	SR-28 (US-127)			TDOT

County:
SEQUATCHIE, BLEDSOE

Total Cost	TIP No.	Conformity Status	LRP No.
\$79,700,000			

Termini:
FROM SR-8 IN SEQUATCHIE COUNTY TO SR-30, LM 15.39 IN BLEDSOE COUNTY (SUPER 2 LANE) (IA)

Description:
RECONSTRUCT AND WIDEN TO 2 12 FOOT LANES AND 10 FOOT SHOULDERS. TURN LANES, CONTINUOUS LEFT TURN LANES AND PASSING LANES WILL BE ADDED AT VARIOUS LOCATIONS, TO BE DETERMINED DURING THE DEVELOPMENT PROCESS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	STBG	\$100,000	\$80,000	\$20,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17782073616	100989.00	1.4	SR-73 (US-321)			TDOT

County:

SEVIER

Total Cost	TIP No.	Conformity Status	LRP No.
\$37,070,100			

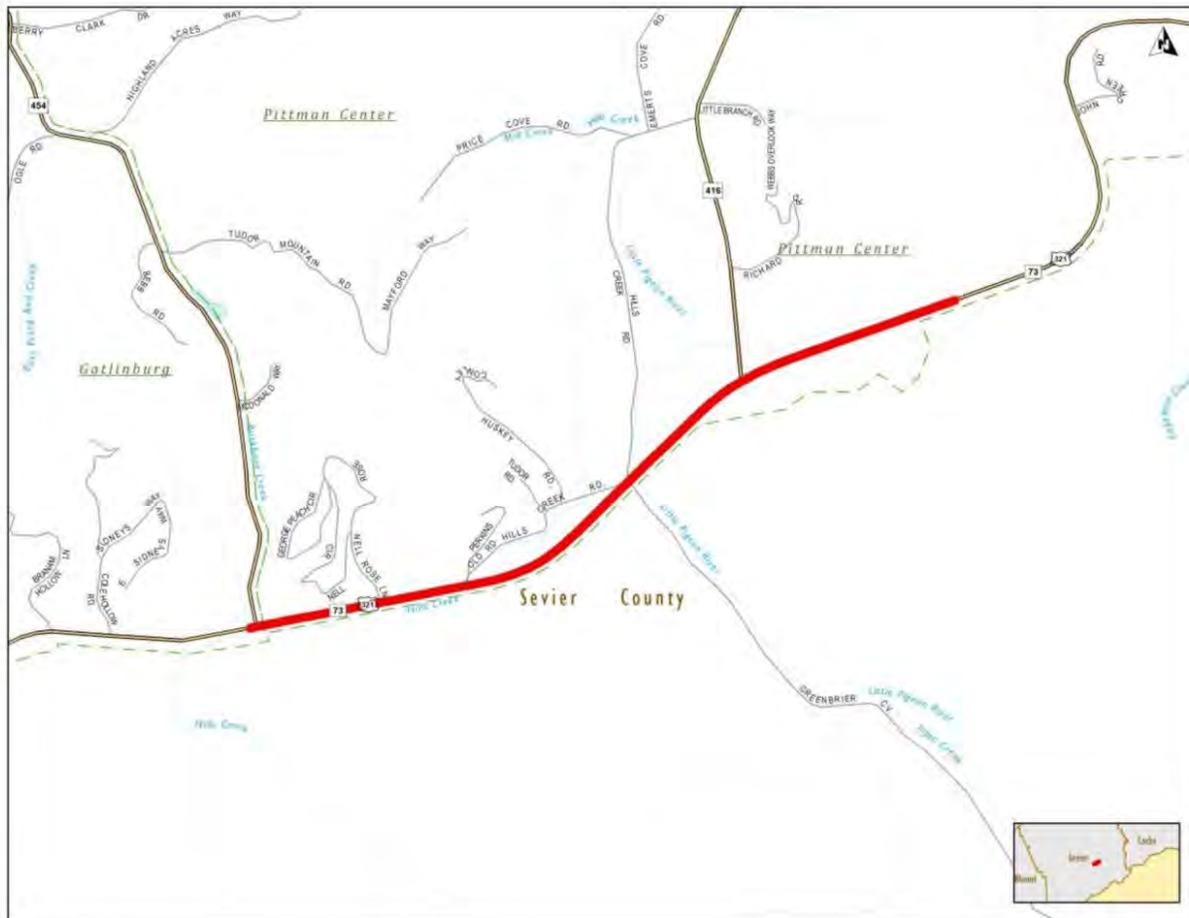
Termini:

BUCKHORN ROAD TO SR-416 (PHASE 2)(EPD) (IA)

Description:

WIDEN 2-LN TO 4-LN DIVIDED

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	STBG	\$500,000	\$400,000	\$100,000	
2021	ROW	STBG	\$3,800,000	\$3,040,000	\$760,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17782449617	124788.00	0.4	SR-449 Ext.			TDOT

County:

SEVIER

Total Cost	TIP No.	Conformity Status	LRP No.
\$17,700,000			

Termini:

(VETERANS BLVD), FROM SR-35 TO ROBERT HENDERSON ROAD (IA)

Description:

CONSTRUCT NEW 5 LANE FACILITY

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	STBG	\$1,000,000	\$800,000	\$200,000	
2021	ROW	STBG	\$3,000,000	\$2,400,000	\$600,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17783000618	124789.00	2.0	SR-XXX			TDOT

County:

SEVIER

Total Cost	TIP No.	Conformity Status	LRP No.
\$34,600,000			

Termini:

JAKE THOMAS CONNECTOR, FROM SR-71 / 73 (US-321 / 441) TO SR-449 (IA)

Description:

JAKE THOMAS ROAD EXTENSION-BETWEEN TEASTER AND NEW RIPKIN EXPERIENCE BALLPARK, WIDENING EXISTING 2-LANE ROAD TO 4-LANE MEDIAN DIVIDED SECTION. FROM BALLPARK TO VETERANS BLVD-CONSTRUCTING 5-LANE ON NEW ALIGNMENT. STIP 1778085

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	STA	\$28,000,000		\$28,000,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17872061647	104563.02	2.2	SR-61			TDOT

County:

UNION

Total Cost	TIP No.	Conformity Status	LRP No.
\$7,000,000			

Termini:

FROM NORTH DAVID DRIVE TO TATER VALLEY ROAD (EPD/PHASE 2) (IA)

Description:

WIDEN TO SUPER TWO-LANE AND SPOT IMPROVEMENTS. THREE (3) LANE &NBSP;SECTION FROM L.M. 11.55 TO L.M. 11.69

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	PE-D	HSIP	\$250,000	\$225,000	\$25,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17872061648	104563.04	4.52	SR-61			TDOT

County:

UNION

Total Cost	TIP No.	Conformity Status	LRP No.
\$7,500,000			

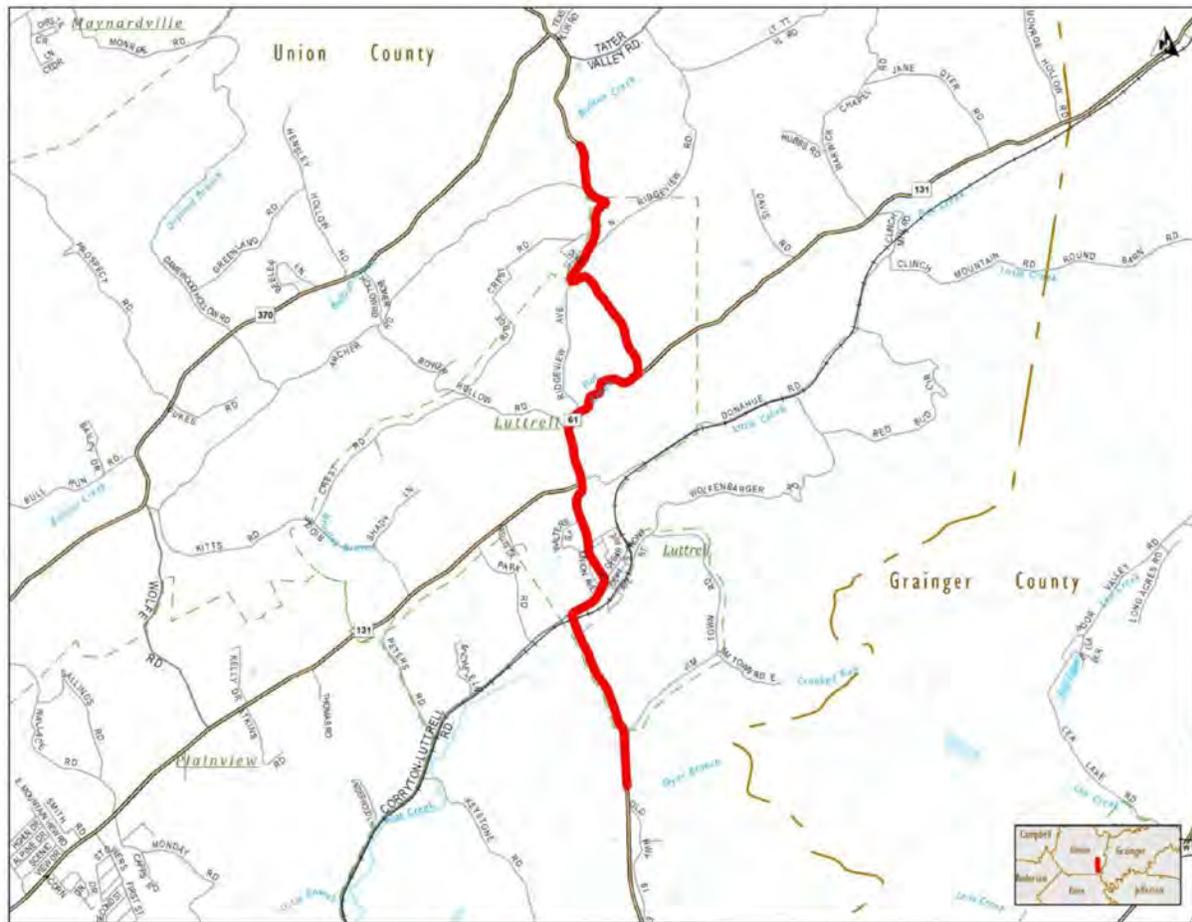
Termini:

FROM NORTH OF ARCHER ROAD TO SOUTH OF JIM TOWN ROAD (PHASE 2) (IA)

Description:

SPOT SAFETY IMPROVEMENTS BETWEEN LM 14.04 AND 18.56

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2022	PE-D	HSIP	\$250,000	\$225,000	\$25,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17993000666	126824.00					TDOT

County:

STATEWIDE - RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$467,200,000			

Termini:

SURFACE TRANSPORTATION SYSTEM PRESERVATION AND OPERATION - RURAL GROUPING

Description:

SEE APPENDIX STATE GROUPING DESCRIPTION FOR A COMPREHENSIVE LISTING OF ACTIVITIES INCLUDED BUT NOT LIMITED FOR ELIGIBILITY

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE, ROW, CONST	STBG	\$116,800,000	\$93,440,000	\$23,360,000	
2021	PE, ROW, CONST	STBG	\$116,800,000	\$93,440,000	\$23,360,000	
2022	PE, ROW, CONST	STBG	\$116,800,000	\$93,440,000	\$23,360,000	
2023	PE, ROW, CONST	STBG	\$116,800,000	\$93,440,000	\$23,360,000	

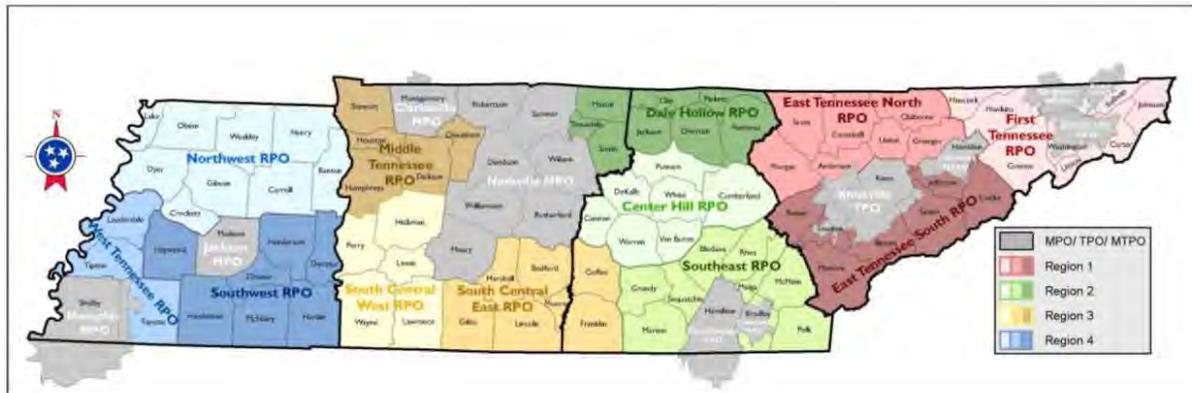


ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17993000667	126825.00					TDOT
County:						
STATEWIDE - RURAL						
Total Cost	TIP No.	Conformity Status		LRP No.		
\$671,200,000						
Termini:						
NATIONAL HIGHWAY SYSTEM PRESERVATION AND OPERATION - RURAL GROUPING						
Description:						
SEE APPENDIX STATE GROUPING DESCRIPTION FOR A COMPREHENSIVE LISTING OF ACTIVITIES INCLUDED BUT NOT LIMITED FOR ELIGIBILITY						

<u>Fiscal Year</u>	<u>Type of Work</u>	<u>Funding Type</u>	<u>Total Funds</u>	<u>Fed Funds</u>	<u>State Funds</u>	<u>Local Funds</u>
2020	PE, ROW, CONST	NHPP	\$167,800,000	\$134,240,000	\$33,560,000	
2021	PE, ROW, CONST	NHPP	\$167,800,000	\$134,240,000	\$33,560,000	
2022	PE, ROW, CONST	NHPP	\$167,800,000	\$134,240,000	\$33,560,000	
2023	PE, ROW, CONST	NHPP	\$167,800,000	\$134,240,000	\$33,560,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17993000668	126826.00					TDOT

County:

STATEWIDE - RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$34,806,000			

Termini:

TRANSPORTATION ALTERNATIVES - RURAL GROUPING

Description:

SEE APPENDIX STATE GROUPING DESCRIPTION FOR A COMPREHENSIVE LISTING OF ACTIVITIES INCLUDED BUT NOT LIMITED FOR ELIGIBILITY

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	TAP	\$8,701,500	\$6,961,200	\$1,740,300	
2021	CONST	TAP	\$8,701,500	\$6,961,200	\$1,740,300	
2022	CONST	TAP	\$8,701,500	\$6,961,200	\$1,740,300	
2023	CONST	TAP	\$8,701,500	\$6,961,200	\$1,740,300	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17032147669	040813.01					TDOT

County:
STATEWIDE - RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$3,200,000			

Termini:
FERRY SERVICE ACROSS THE TENNESSEE RIVER, SR-69A TO SR-147 (IA)

Description:
RECURRING OPERATIONS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CONST	STA	\$800,000		\$800,000	
2021	CONST	STA	\$800,000		\$800,000	
2022	CONST	STA	\$800,000		\$800,000	
2023	CONST	STA	\$800,000		\$800,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17993000670	126828.00					TDOT

County:

STATEWIDE

Total Cost	TIP No.	Conformity Status	LRP No.
\$17,800,000			

Termini:

ADVANCE ENGINEERING, FUNCTIONAL DESIGN, AERIAL SURVEY, MAPPING, ETC. - RURAL GROUPING

Description:

Advance Engineering, Functional Design, Aerial Survey, Mapping, etc.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE-D	STA	\$4,200,000		\$4,200,000	
2021	PE-D	STA	\$4,200,000		\$4,200,000	
2022	PE-D	STA	\$4,200,000		\$4,200,000	
2023	PE-D	STA	\$5,200,000		\$5,200,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17993000671	126830.00					TDEC

County:

STATEWIDE

Total Cost	TIP No.	Conformity Status	LRP No.
\$6,000,000			

Termini:

RECREATIONAL TRAILS - RURAL GROUPING

Description:

CREATION, REHABILITATION & MAINTENANCE OF MULTI-USE RECREATIONAL TRAILS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PE, ROW, CONST	RTP	\$1,500,000	\$1,200,000	\$300,000	
2021	PE, ROW, CONST	RTP	\$1,500,000	\$1,200,000	\$300,000	
2022	PE, ROW, CONST	RTP	\$1,500,000	\$1,200,000	\$300,000	
2023	PE, ROW, CONST	RTP	\$1,500,000	\$1,200,000	\$300,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17993000673	126832.00					TDOT

County:

STATEWIDE - RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$1,700,000			

Termini:

WORKFORCE DEVELOPMENT, TRAINING AND EDUCATION ACTIVITIES - RURAL GROUPING

Description:

SEE APPENDIX STATE GROUPING DESCRIPTION FOR A COMPREHENSIVE LISTING OF ACTIVITIES INCLUDED BUT NOT LIMITED FOR ELIGIBILITY

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	TRAINING	STBG	\$425,000	\$425,000		
2021	TRAINING	STBG	\$425,000	\$425,000		
2022	TRAINING	STBG	\$425,000	\$425,000		
2023	TRAINING	STBG	\$425,000	\$425,000		



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000786	129759.00					TDOT TRANSIT

County:

STATEWIDE - RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$805,200			

Termini:

DISCRETIONARY CAPITAL INVESTMENT GRANTS

Description:

THIS PROGRAM PROVIDES DISCRETIONARY CAPITAL ASSISTANCE FOR THE PURCHASE OF NEW EQUIPMENT, ACQUISITION OF PROPERTY AND THE CONSTRUCTION OF PUBLIC TRANSIT FACILITIES.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PLANNING	5309	\$805,200	\$644,160	\$161,040	
2021	PLANNING	5309	\$0	\$0	\$0	\$0
2022	PLANNING	5309	\$0	\$0	\$0	\$0
2023	PLANNING	5309	\$0	\$0	\$0	\$0



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000787	129760.00					TDOT - TRANSIT

County:
STATEWIDE - RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$841,000			

Termini:
JOB ACCESS REVERSE COMMUTE GRANTS

Description:
THIS PROGRAM PROVIDES CAPITAL, OPERATING AND ADMINISTRATIVE TRANSIT EXPENSES FOR PROJECTS THAT ARE USED TO ASSIST LOW-INCOME AND WELFARE RECIPIENTS TO TRANSITION FROM WELFARE TO WORK BY TRANSPORTATION SERVICES TO REACH NEEDED JOB TRAINING ACTIVITIES, EMPLOYMENT, AND CHILDCARE.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PLANNING	5316	\$841,000	\$419,000	\$211,000	\$211,000
2021	PLANNING	5316	\$0	\$0	\$0	\$0
2022	PLANNING	5316	\$0	\$0	\$0	\$0
2023	PLANNING	5316	\$0	\$0	\$0	\$0



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000788	129761.00					TDOT - TRANSIT

County:

STATEWIDE - RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$3,654,000			

Termini:

NEW FREEDOM GRANTS

Description:

THIS PROGRAM PROVIDES CAPITAL, OPERATING AND ADMINISTRATIVE TRANSIT EXPENSES FOR PROJECTS THAT ARE USED TO SUPPORT NEW PUBLIC TRANSPORTATION SERVICES AND TRANSPORTATION ALTERNATIVES FOR INDIVIDUALS WITH DISABILITIES BEYOND THOSE REQUIRED BY THE ADA OF 1990.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PLANNING	5317	\$3,654,000	\$1,826,000	\$914,000	\$914,000
2021	PLANNING	5317	\$0	\$0	\$0	\$0
2022	PLANNING	5317	\$0	\$0	\$0	\$0
2023	PLANNING	5317	\$0	\$0	\$0	\$0



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000788	129762.00					TDOT TRANSIT

County:

STATEWIDE - RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$4,065,000			

Termini:

BUS AND BUS FACILITIES GRANTS - DISCRETIONARY

Description:

THIS PROGRAM PROVIDES FOR CAPITAL FUNDING TO REPLACE, REHABILITATE AND PURCHASE BUSES, VANS, AND RELATED EQUIPMENT AND TO CONSTRUCT BUS RELATED FACILITIES.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PLANNING	5339(b)	\$4,065,000	\$2,479,000	\$1,179,000	\$407,000
2021	PLANNING	5339(b)	\$0	\$0	\$0	\$0
2022	PLANNING	5339(b)	\$0	\$0	\$0	\$0
2023	PLANNING	5339(b)	\$0	\$0	\$0	\$0



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000790	128586.00					TDOT

County:
STATEWIDE

Total Cost	TIP No.	Conformity Status	LRP No.
\$2,354,200			

Termini:
METROPOLITAN TRANSPORTATION PLANNING

Description:
THIS PROGRAM PROVIDES FUNDS TO METROPOLITAN PLANNING ORGANIZATIONS FOR MULTIMODAL TRANSPORTATION PLANNING.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PLANNING	5303	\$2,354,200	\$1,883,360	\$470,840	
2021	PLANNING	5303	\$0	\$0	\$0	\$0
2022	PLANNING	5303	\$0	\$0	\$0	\$0
2023	PLANNING	5303	\$0	\$0	\$0	\$0



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000791	128592.00					TDOT

County:

STATEWIDE

Total Cost	TIP No.	Conformity Status	LRP No.
\$4,828,831			

Termini:

STATEWIDE TRANSPORTATION PLANNING

Description:

THIS PROGRAM PROVIDES FUNDS TO MAKE TRANSPORTATION INVESTMENT DECISIONS IN SUPPORT OF COOPERATIVE, CONTINUOUS AND COMPREHENSIVE PLANNING NEEDS.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	PLANNING	5304	\$3,407,531	\$2,726,025	\$681,506	
2021	PLANNING	5304	\$462,500	\$370,000	\$92,500	
2022	PLANNING	5304	\$473,800	\$379,040	\$94,760	
2023	PLANNING	5304	\$485,000	\$388,000	\$97,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000792	128593.00					TDOT

County:

STATEWIDE - SMALL URBAN

Total Cost	TIP No.	Conformity Status	LRP No.
\$35,088,400			

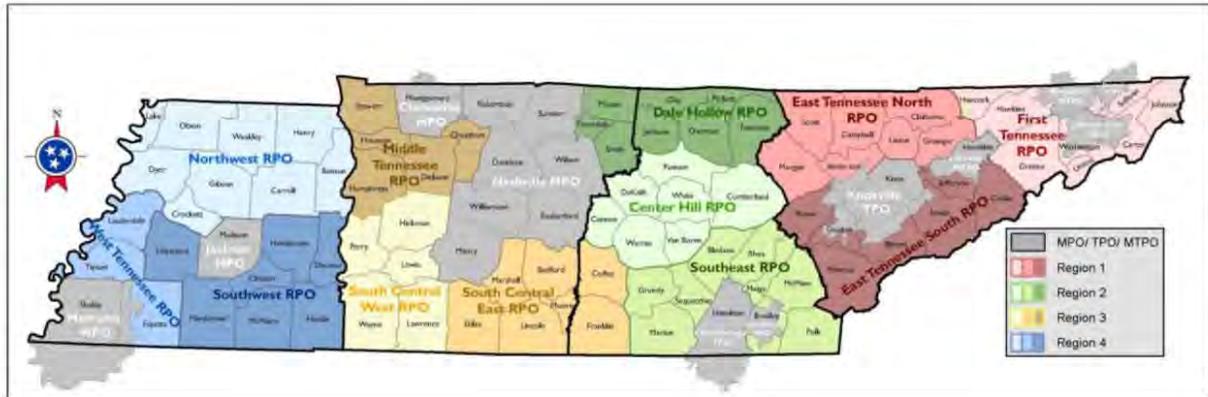
Termini:

ENHANCED MOBILITY FOR SENIORS AND INDIVIDUALS WITH DISABILITIES CAPITAL/GRANTS

Description:

THIS PROGRAM PROVIDES TRANSIT CAPITAL ASSISTANCE, THROUGH THE STATE, TO PRIVATE NON-PROFIT ORGANIZATIONS AND PUBLIC BODIES THAT PROVIDE SPECIALIZED TRANSPORTATION SERVICES TO ELDERLY AND/OR PERSONS WITH DISABILITIES. ANNUALLY, ELIGIBLE APPLICANTS APPLY TO THE DIVISION FOR FUNDING TO ACQUIRE EQUIPMENT TO PROVIDE SPECIALIZED TRANSPORTATION NEEDS OF ELDERLY AND/OR PERSONS WITH DISABILITIES. FUNDS APPORTIONED AFTER 2013 ARE ALSO ELIGIBLE FOR ALLOWABLE SAFETEA-LU 5317 ACTIVITIES. THESE ACTIVITIES ADDRESS THE TRANSPORTATION NEEDS OF PERSONS WITH DISABILITIES THAT GO BEYOND THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	ACQ/AD/CA	5310	\$20,058,400	\$16,046,720	\$4,011,680	
2021	ACQ/AD/CA	5310	\$4,721,000	\$3,776,800	\$944,200	
2022	ACQ/AD/CA	5310	\$5,004,000	\$4,003,200	\$1,000,800	
2023	ACQ/AD/CA	5310	\$5,305,000	\$4,244,000	\$1,061,000	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
17993000677	128594.00					TDOT

County:

STATEWIDE - RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$18,658,361			

Termini:

ENHANCED MOBILITY FOR SENIORS AND INDIVIDUALS WITH DISABILITIES CAPITAL/GRANTS

Description:

THIS PROGRAM PROVIDES TRANSIT CAPITAL ASSISTANCE, THROUGH THE STATE, TO PRIVATE NON-PROFIT ORGANIZATIONS AND PUBLIC BODIES THAT PROVIDE SPECIALIZED TRANSPORTATION SERVICES TO ELDERLY AND/OR PERSONS WITH DISABILITIES. ANNUALLY, ELIGIBLE APPLICANTS APPLY TO THE DIVISION FOR FUNDING TO ACQUIRE EQUIPMENT TO PROVIDE SPECIALIZED TRANSPORTATION NEEDS OF ELDERLY AND/OR PERSONS WITH DISABILITIES. FUNDS APPORTIONED AFTER 2013 ARE ALSO ELIGIBLE FOR ALLOWABLE SAFETEA-LU 5317 ACTIVITIES. THESE ACTIVITIES ADDRESS THE TRANSPORTATION NEEDS OF PERSONS WITH DISABILITIES THAT GO BEYOND THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2017	ACQ/AD/CA	5310	\$9,469,361	\$7,813,361	\$828,000	\$828,000
2018	ACQ/AD/CA	5310	\$2,777,000	\$2,359,000	\$209,000	\$209,000
2019	ACQ/AD/CA	5310	\$3,053,000	\$2,595,000	\$229,000	\$229,000
2020	ACQ/AD/CA	5310	\$3,359,000	\$2,855,000	\$252,000	\$252,000



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000793	128595.00					TDOT

County:

STATEWIDE - RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$244,226,000			

Termini:

5311 RURAL TRANSPORTATION PROGRAM

Description:

FUNDS ALLOCATED TO STATE OF TENNESSEE FOR TRANSPORTATION IN RURAL AREAS; THIS FORMULA GRANT PROGRAM PROVIDES FUNDS TO SUPPORT PUBLIC TRANSPORTATION IN RURAL AREAS WITH A POPULATION LESS THAN 50,000. THE FUNDS ARE USED FOR TRANSIT CAPITAL, OPERATING AND ADMINISTRATIVE EXPENSES FOR PROJECTS THAT MEET THE NEEDS OF THE RURAL COMMUNITIES.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	AD/CA/OP	5311	\$107,490,000	\$85,992,000	\$21,498,000	
2021	AD/CA/OP	5311	\$42,120,000	\$33,696,000	\$8,424,000	
2022	AD/CA/OP	5311	\$45,488,000	\$36,390,400	\$9,097,600	
2023	AD/CA/OP	5311	\$49,128,000	\$23,296,000	\$11,648,000	\$11,648,000



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000794	128596.00					TDOT

County:

STATEWIDE - RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$24,131,000			

Termini:

5311(F) INTERCITY BUS PROGRAM (SUB-PART OF THE RURAL TRANSPORTATION PROGRAM)

Description:

FUNDS ALLOCATED TO STATE TO HELP WITH THE INTERCITY TRAVEL NEEDS OF RESIDENTS AND PERSONS IN NON-URBANIZED AREAS OF THE STATE

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	AD/CA/OP	5311(F)	\$0	\$0	\$0	
2021	AD/CA/OP	5311(F)	\$7,432,000	\$5,945,600	\$1,486,400	
2022	AD/CA/OP	5311(F)	\$8,028,000	\$6,422,400	\$1,605,600	
2023	AD/CA/OP	5311(F)	\$8,671,000	\$6,936,800	\$1,734,200	



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000795	128597.00					TDOT

County:

STATEWIDE -RURAL

Total Cost	TIP No.	Conformity Status	LRP No.
\$2,963,100			

Termini:

RURAL TRANSPORTATION ASSISTANCE PROGRAM (SUB-PART OF THE 5311 RURAL TRANSPORTATION PROGRAM)

Description:

FUNDING ALLOCATED TO THE STATE TO PROVIDE TRAINING AND TECHICANAL ASSISTANCE IN RURAL TRANSPORTATION AREAS

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	AD/TRNG	5311(RTAP)	\$1,664,100	\$1,664,100		
2021	AD/TRNG	5311(RTAP)	\$400,000	\$400,000		
2022	AD/TRNG	5311(RTAP)	\$432,000	\$432,000		
2023	AD/TRNG	5311(RTAP)	\$467,000	\$467,000		



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000796	128598.00					TDOT

County:

STATEWIDE - APPALACHIAN AREAS

Total Cost	TIP No.	Conformity Status	LRP No.
\$13,575,800			

Termini:

APPALACHIAN DEVELOPMENT PUBLIC TRANSPORTATION ASSISTANCE PROGRAM (SUB-PART OF THE 5311 RURAL TRANSPORTATION PROGRAM)

Description:

FUNDS ALLOCATED FROM MAP 21 TO HELP THE APPALACHIAN MOUNTAIN REGION WITH TRANSPORTATION NEEDS; THIS PROGRAM PROVIDES FUNDS TO CARRY OUT A PUBLIC TRANSPORTATION ASSISTANCE PROGRAM IN THE APPALACHIAN REGION.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	AD/CA/OP	5311(APP)	\$5,162,800	\$2,579,800	\$1,291,500	\$1,291,500
2021	AD/CA/OP	5311(APP)	\$2,591,000	\$1,295,000	\$648,000	\$648,000
2022	AD/CA/OP	5311(APP)	\$2,799,000	\$1,399,000	\$700,000	\$700,000
2023	AD/CA/OP	5311(APP)	\$3,023,000	\$1,511,000	\$756,000	\$756,000



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000799	128599.00					TDOT -TRANSIT

County:

STATEWIDE - HAMILTON AND SHELBY COUNTIES

Total Cost	TIP No.	Conformity Status	LRP No.
\$3,802,000			

Termini:

STATE SAFETY OVERSIGHT PROGRAM

Description:

THIS GRANT PROGRAM PROVIDES FUNDING FOR STATES TO OVERSEE SAFETY AT RAIL TRANSIT SYSTEMS IN THEIR JURISDICTION.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	OVERSIGHT	5329	\$2,309,000	\$1,843,000	\$233,000	\$233,000
2021	OVERSIGHT	5329	\$459,000	\$367,000	\$46,000	\$46,000
2022	OVERSIGHT	5329	\$497,000	\$397,000	\$50,000	\$50,000
2023	OVERSIGHT	5329	\$537,000	\$429,000	\$54,000	\$54,000



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

STIP PROJECT REPORT FY 2020 - FY 2023

STIP ID	TDOT PIN	Length	Route:	Modification	Amendment	Lead Agency
20993000800	128600.00					TDOT - TRANSIT

County:

STATEWIDE

Total Cost	TIP No.	Conformity Status	LRP No.
\$32,966,000			

Termini:

BUS AND BUS FACILITIES GRANTS

Description:

THIS PROGRAM PROVIDES FOR CAPITAL FUNDING TO REPLACE, REHABILITATE AND PURCHASE BUSES, VANS, AND RELATED EQUIPMENT AND TO CONSTRUCT BUS RELATED FACILITIES.

Fiscal Year	Type of Work	Funding Type	Total Funds	Fed Funds	State Funds	Local Funds
2020	CAPITAL	5339	\$17,756,000	\$14,200,000	\$1,778,000	\$1,778,000
2021	CAPITAL	5339	\$4,825,000	\$3,859,000	\$483,000	\$483,000
2022	CAPITAL	5339	\$5,066,000	\$4,052,000	\$507,000	\$507,000
2023	CAPITAL	5339	\$5,319,000	\$4,255,000	\$532,000	\$532,000



ALL SCHEDULES SUBJECT TO AVAILABILITY OF FUNDS

Chapter 8: Appendices

External Resources

Metropolitan, Non-Metropolitan, and Rural Areas

A map of metropolitan, non-metropolitan, and rural areas is provided by TDOT and available at:

https://www.tn.gov/content/dam/tn/tdot/long-range-planning/maps/MPO_RPO_Map.pdf

Regional Significance and Air Quality Attainment Status

A regionally significant project is a transportation project that is located on a facility which serves regional transportation needs (such as access to and from the area outside the region; major activity centers in the region; major planned developments such as new retail malls, sports complexes, or employment centers; or transportation terminals) and would normally be included in the modeling of the metropolitan area's transportation network. At a minimum, this includes all principal arterial highways and all fixed guideway transit facilities that offer a significant alternative to regional highway travel, per 23 CFR 450.104. Regional significance is collaboratively determined by each non-attainment and/or maintenance area's respective Interagency Consultation group, per 40 CFR 93.105. The EPA's "Green Book" of non-attainment and maintenance statuses is available at:

<https://www.epa.gov/green-book>

NHPP and STBGP Funding Qualifications

National Highway Performance Program (NHPP) funds are limited to projects on the National Highway System (NHS) unless otherwise noted in Title 23 of the USC. FHWA's NHS maps are available at:

https://www.fhwa.dot.gov/planning/national_highway_system/nhs_maps/tennessee/

Surface Transportation Block Grant Program (STBG) funds can only be used on Federal-aid Highways – any functionally-classified road except local roads and rural minor collectors – unless otherwise noted in Title 23 of the USC. TDOT's functional classification maps are available at:

<https://www.tn.gov/tdot/driver-how-do-i/look-at-or-order-state-maps/maps/annual-average-daily-traffic-maps1.html>

Statewide Grouping Descriptions

Activities delivered from TDOT’s statewide groupings are limited to work types that are:

- 1) Located in non-metropolitan or rural areas – any located in a metropolitan area must be programmed in the MPO’s TIP,
- 2) Not considered to be of appropriate scale for individual identification in a given program year,
- 3) Environmentally-neutral as categorical exclusions under 23 CFR 771.117(c) and (d),
- 4) Non-regionally significant, in non-attainment and maintenance areas, and
- 5) Exempt as defined in the EPA’s transportation conformity regulations in 40 CFR Part 93, in non-attainment and maintenance areas.

Activities that do not meet these requirements must be individually identified in the STIP or respective MPO’s TIP. The following tables elaborate on the allowable work types for the statewide groupings.

Statewide Grouping Crosswalk

Grouping	Function	Allowable Work Types
Travel Congestion and Clean Air Improvement Grouping (20993000810)	Reduce traffic congestion and improve air quality for areas not meeting standards for ozone, carbon monoxide, or particulate matter.	<ul style="list-style-type: none"> • State planning and research • Installation of vehicle-to-infrastructure communications equipment • Electric vehicle and natural gas vehicle infrastructure • Diesel retrofits for PM 2.5 non-attainment or maintenance areas • Use verified technology for non-road vehicles and engines used in port- related freight operations for PM 2.5 non-attainment or maintenance areas under 23 U.S.C. or Chapter 53 of 49 U.S.C. • Install diesel emission control technology on non-road diesel equipment or such equipment that is used on highway construction projects for PM 2.5 non-attainment or maintenance areas. • Establish/operate a traffic monitoring, management, and control facility or program <ul style="list-style-type: none"> ○ Improve signalization ○ Transportation Demand Management ○ Bicycle and Pedestrian Facilities ○ Public Outreach and Education ○ Carpooling/Vanpooling

		<ul style="list-style-type: none"> • Build high occupancy vehicle (HOV) lanes • Intersection improvement. • Add turning lanes • Improve transportation systems management and operations • Implement intelligent transportation systems (ITS) • Purchase diesel retrofits • Provide information and technical assistance to owners/operators of diesel equipment and vehicles. • The most cost-effective projects to reduce emissions from port-related landside non-road or on road equipment that is operated within the boundaries of the area [23 U.S.C.149(k)(2) & (4)]
Grouping	Function	Allowable Work Types
Safety Grouping (17993000672)	Any strategy, activity or project on a public road that is consistent with the data-driven State Strategic Highway Safety Plan (SHSP) and corrects or improves a hazardous road location or feature or addresses a highway safety problem, including workforce development, training and education activities.	
Safety Grouping (17993000672)	<p>Eligibility of specific projects, strategies, and activities is generally based on:</p> <ul style="list-style-type: none"> • Consistency with SHSP, • Crash experience, crash potential, or other data-supported means, • Compliance with the requirements of Title 23 of the U.S.C., and 	<ul style="list-style-type: none"> • Intersection safety improvements • Pavement and shoulder widening (including a passing lane to remedy an unsafe condition) • Installation of rumble strips or another warning devices, if they do not adversely affect the safety or mobility of bicyclists and pedestrians • Installation of skid-resistant surface at intersections or locations with high crash frequencies • Improvements for pedestrian or bicyclist safety • Construction and improvement of a railway-highway grade crossing safety feature,

	<ul style="list-style-type: none"> • State’s strategic or performance-based safety goals to reduce fatalities and serious injuries on all public roads. • Projects to upgrade railway-highway grade crossings by eliminating hazards and installing protective devices. 	<p>including installation of protective devices</p> <ul style="list-style-type: none"> • The conduct of a model traffic enforcement activity at a railway-highway crossing • Construction of a traffic calming feature • Elimination of a roadside hazard • Installation, replacement, and other improvements of highway signage and pavement markings, or a project to maintain minimum levels of retro-reflectivity that addresses a highway safety problem consistent with the SHSP • Installation of emergency vehicle priority control systems at signalized intersections • Installation of traffic control or other warning devices at locations with high crash potential • Transportation safety planning • Collection, analysis, and improvement of safety data • Planning integrated interoperable emergency communications equipment or operational or traffic enforcement activities (including police assistance) related to work zone safety • Installation of guardrails, barriers (including barriers between construction work zones and traffic lanes), and crash attenuators. • The addition or retrofitting of structures or other measures to eliminate or reduce crashes involving vehicles and wildlife • Installation of yellow-green signs and signals at pedestrian and bicycle crossings and in school zones. • Construction and operational improvements on high risk rural roads. • Geometric improvements to a road for safety purposes that improve safety. • Road safety audits. • Roadway safety infrastructure improvements consistent with FHWA’s “Highway Design Handbook for Older Drivers and Pedestrians” (FHWA-RD-01-103) • Truck parking facilities eligible for funding
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		<p>under Section 1401 of MAP-21</p> <ul style="list-style-type: none"> • Systemic safety improvements • Installation of vehicle-to-infrastructure communication equipment. • Pedestrian hybrid beacons. • Roadway improvements that provide separation between pedestrians and motor vehicles, including medians and pedestrian crossing islands. • Other physical infrastructure projects not specifically enumerated in the list of eligible projects. • Workforce development, training, and education activities
Grouping Category	Function of Grouping Activities	Allowable Work Types
Safety Grouping (Section 130 or HSIP-R) (17993000672)	Activities included as part of the Highway Railroad Grade Crossing program:	<ul style="list-style-type: none"> • Elimination of hazards of railway-highway crossings, including the separation or protection of grades at crossings. • Reconstruction of existing railroad grade crossing structures. • Relocation of highways to eliminate grade crossings. • Installation of protective devices. • Projects authorized from this grouping may be authorized with different Federal percentages than shown in the grid based on one or more of the following: <ul style="list-style-type: none"> ○ 90% for certain Interstate projects under 23 U.S.C. 120(a); or ○ 100% for certain safety items under 23 U.S.C. 120(c); or ○ 100% for Appalachian Development Highway System (ADHS) projects under MAP-21 §1528 and 40 U.S.C. 14501.
Grouping Category	Function of Grouping Activities	Allowable Work Types
Highway Infrastructure Program (HIP)	Provide flexible funding to address State and local transportation needs through the construction of highways,	Construction of highways, bridges, tunnels, including designated routes of the Appalachian development highway system and local access

	bridges, tunnels, including designated routes of the Appalachian development highway system and local access roads under Section 14501 of Title 40.	roads under Section 14501 of Title 40.
Grouping Category	Function of Grouping Activities	Allowable Work Types
National Highway Freight Program (NHFP) Grouping	<p>Projects for the preservation and improvement of the conditions and performance of the National Highway Freight Network (NHFN), including:</p> <p>Projects for the preservation and improvement of the conditions and performance of the National Highway System (NHS), including:</p> <ul style="list-style-type: none"> • Rehabilitation, resurfacing, preservation, and operational improvements; • Traffic operations; • Environmental mitigation; • Statewide studies; • Data collection 	<ul style="list-style-type: none"> • Development phase activities, including planning, feasibility analysis, revenue forecasting, environmental review, preliminary engineering and design work, and other preconstruction activities; • Construction, reconstruction, rehabilitation, acquisition of real property (including land relating to the project and improvements to the land), construction contingencies, acquisition of equipment, and operational improvements directly relating to improving system performance; • Intelligent transportation systems and other technology to improve the flow of freight, including intelligent freight transportation systems; • Efforts to reduce the environmental impacts of freight movement; • Environmental and community mitigation for freight movement; • Railway-highway grade separation; • Geometric improvements to interchanges and ramps; • Truck-only lanes; • Climbing and runaway truck lanes; • Adding or widening shoulders; • Truck parking facilities eligible for funding under section 1401 of MAP-21; • Real-time traffic, truck parking, roadway condition, and multimodal transportation information systems; • Electronic screening and credentialing systems for vehicles, including weigh-in-motion truck inspection technologies; • Traffic signal optimization, including synchronized and adaptive signals; • Work zone management and information systems;

		<ul style="list-style-type: none"> • Highway ramp metering; • Electronic cargo and border security technologies that improve truck freight movement; • Intelligent transportation systems that would increase truck freight efficiencies inside the boundaries of intermodal facilities; • Additional road capacity to address highway freight bottlenecks; • Physical separation of passenger vehicles from commercial motor freight; • Enhancement of the resiliency of critical highway infrastructure, including highway infrastructure that supports national energy security, to improve the flow of freight; • A highway or bridge project, other than a project previously described, to improve the flow of freight on the National Highway Freight Network; • Any other surface transportation project to improve the flow of freight into and out of a public or private freight rail or water facility (including ports), and facilities that provide surface transportation infrastructure necessary to facilitate direct intermodal interchange, transfer, and access into or out of the facility;
Grouping Category	Function of Grouping Activities	Allowable Work Types
<p>National Highway System Preservation And Operation Grouping (17993000667)</p> <p>National Highway System Preservation And Operation Grouping</p>	<ul style="list-style-type: none"> • Rehabilitation, resurfacing, restoration, preservation, and operational improvements, • Traffic operations, • Bridge and tunnel improvements, • Safety improvements, • Bicycle and pedestrian improvements, and <p>Environmental mitigation.</p>	<ul style="list-style-type: none"> • Minor rehabilitation, pavement resurfacing, preventative maintenance, restoration, and pavement preservation treatments to extend the service life of highway infrastructure, including pavement markings and improvements to roadside hardware or sight distance • Highway improvement work including slide repair, rock fall mitigation, drainage repairs, or other preventative work necessary to maintain or extend the service life of the existing infrastructure in a good operational condition • Minor operational and safety improvements to intersections and interchanges such as

(17993000667)

adding turn lanes, addressing existing geometric deficiencies, and extending on/off ramps

- Capital and operating costs for intelligent transportation systems (ITS) and traffic monitoring, management, and control facilities and programs:
- Infrastructure-based intelligent transportation systems (ITS) capital improvements.
- Traffic Management Center (TMC) operations and utilities.
- Freeway service patrols.
- Traveler information.
- Bridge and tunnel construction (no additional travel lanes), replacement, rehabilitation, preservation, protection, inspection, evaluation, and inspector training and inspection and evaluation of other infrastructure assets, such as signs, walls, and drainage structures.
- Development and implementation of a State Asset Management Plan including data collection, maintenance and integration, software costs, and equipment costs that support the development of performance-based management systems for infrastructure.
- Rail-highway grade crossing improvements.
- Highway safety improvements:
- Installation of new or improvement of existing guardrail.
- Installation of traffic signs and signals/lights.
- Spot safety improvements.
- Sidewalk improvements.
- Pedestrian and/or bicycle facilities.
- Traffic calming and traffic diversion improvements.
- Noise walls,
- Wetland and/or stream mitigation,
- Environmental restoration and pollution abatement,

		<ul style="list-style-type: none"> Control of noxious weeds and establishment of native species. Projects authorized from this grouping may be authorized with different Federal percentages than shown in the grid based on one or more of the following: <ul style="list-style-type: none"> 90% for certain Interstate projects under 23 U.S.C. 120(a); or 100% for certain safety items under 23 U.S.C. 120(c); or 100% for Appalachian Development Highway System (ADHS) projects under MAP-21 §1528 and 40 U.S.C. 14501.
Grouping Category	Function of Grouping Activities	Allowable Work Types
Surface Transportation System Preservation And Operation Grouping (17993000666)	Projects and programs for the preservation and improvement of the conditions and performance of Federal-aid highways and public roads, including:	Activities previously authorized under the Surface Transportation Program (STP):
	<p>Projects and programs for the preservation and improvement of the conditions and performance of Federal-aid highways and public roads, including:</p> <ul style="list-style-type: none"> Rehabilitation, resurfacing, restoration, preservation, and operational improvements on Federal-aid highways and designated routes of the Appalachian Development Highway System (ADHS) and local access roads under 40 U.S.C. 14501, Traffic operations on Federal-aid highways, 	<p>Activities previously authorized under the Surface Transportation Program (STP):</p> <ul style="list-style-type: none"> Minor rehabilitation, pavement resurfacing, preventative maintenance, restoration, and pavement preservation treatments to extend the service life of highway infrastructure, including pavement markings and improvements to roadside hardware or sight distance Highway improvement work including slide repair, rock fall mitigation, drainage repairs, or other preventative work necessary to maintain or extend the service life of the existing infrastructure in a good operational condition Minor operational and safety improvements to intersections and interchanges such as adding turn lanes, addressing existing

	<ul style="list-style-type: none"> • Bridge and tunnel improvements on public roads, • Safety improvements on public roads, • Environmental mitigation • Scenic and historic highway programs, • Landscaping and scenic beautification, • Historic preservation, • Infrastructure projects for improving non-driver access to public transportation and enhanced mobility, <p>Community improvement activities,</p>	<p>geometric deficiencies, and extending on/off ramps.</p> <ul style="list-style-type: none"> • Capital and operating costs for intelligent transportation systems (ITS) and traffic monitoring, management, and control facilities and programs: <ul style="list-style-type: none"> ○ Infrastructure-based intelligent transportation systems (ITS) capital improvements. ○ Traffic Management Center (TMC) operations and utilities. ○ Freeway service patrols, ○ Traveler information. • Bridge and tunnel construction (no additional travel lanes), replacement, rehabilitation, preservation, protection, inspection, evaluation, and inspector training and inspection and evaluation of other infrastructure assets, such as signs, walls, and drainage structures • Development and implementation of a State Asset Management Plan including data collection, maintenance and integration, software costs, and equipment costs that support the development of performance-based management systems for infrastructure. • Rail - Highway grade crossing improvements • Highway safety improvements: <ul style="list-style-type: none"> ○ Installation of new or improvement of existing guardrail. ○ Installation of traffic signs and signals/lights. ○ Spot safety improvements. • Sidewalk improvements, • Pedestrian and/or bicycle facilities, • Traffic calming and traffic diversion improvements, • Transportation Alternatives as defined by 23 U.S.C. 213(B), 23 U.S.C. 101(A)(29), and
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		<p>Section 1122 of MAP-21.</p> <ul style="list-style-type: none"> Noise walls, Wetland and/or stream mitigation, Environmental restoration and pollution abatement, <p>Control of noxious weeds and establishment of native species</p>
Grouping Category	<ul style="list-style-type: none"> Function of Grouping Activities 	Allowable Work Types
Surface Transportation System Preservation And Operation Grouping (17993000666)	Transportation Enhancement projects,	<p>Activities previously authorized under the Transportation Enhancement Program:</p> <ul style="list-style-type: none"> Pedestrian and bicycle facilities, safety, and educational activities. Acquisition of scenic easements and scenic or historic sites. Scenic or historic highway programs, Landscaping and other scenic beautification activities, Historic preservation, Rehabilitation and operation of historic transportation buildings, structures, or facilities, Preservation of abandoned railway corridors, Advertising, Archaeological planning and research, Environmental mitigation to address water pollution due to highway runoff or reduce vehicle-caused wildlife mortality while maintaining habitat connectivity. Establishment of transportation museums, Activities under the Tennessee Roadscapes grant program, including landscaping, irrigation, benches, trash cans, paths, and signage.
Grouping Category	<ul style="list-style-type: none"> Function of Grouping Activities 	Allowable Work Types
	Safe Routes to School (SRTS) projects,	<p>Infrastructure related activities:</p> <ul style="list-style-type: none"> Sidewalk improvements Traffic calming and speed reduction improvements

		<ul style="list-style-type: none"> • Pedestrian and bicycle crossing improvements • On-street bicycle facilities • Off-street bicycle and pedestrian facilities • Secure bicycle parking facilities • Traffic diversion improvements approximately within 2 miles of a school location.
Surface Transportation System Preservation And Operation Grouping (17993000666)	<ul style="list-style-type: none"> • Safe Routes to School (SRTS) projects, 	<p>Non-infrastructure related activities:</p> <ul style="list-style-type: none"> • Public awareness campaigns and outreach to press and community leaders. • Traffic education and enforcement in the vicinity of schools <ul style="list-style-type: none"> ○ Student sessions on bicycle and pedestrian safety, health, and environment ○ Funding for training, volunteers, and managers of safe routes to school program.
Grouping Category	• Function of Grouping Activities	Allowable Work Types
Surface Transportation System Preservation And Operation – TAP Grouping (17993000668)	<ul style="list-style-type: none"> • Transportation Alternatives projects, • On- and off-road pedestrian and bicycle facilities, 	<p>Activities previously authorized under the Transportation Alternatives Program (TAP):</p> <ul style="list-style-type: none"> • Transportation Alternatives projects, construction, planning, and design of on-road and off-road trail facilities for pedestrians, bicyclists, and other non-motorized forms of transportation, including: <ul style="list-style-type: none"> ○ Sidewalk improvements. ○ Bicycle infrastructure. ○ Pedestrian and bicycle signals. ○ Traffic calming techniques. ○ Lighting and other safety-related infrastructure. ○ Transportation projects to achieve compliance with the Americans with Disabilities Act of 1990
Surface Transportation System Preservation And Operation – TAP	<ul style="list-style-type: none"> • Transportation Alternatives projects, 	<ul style="list-style-type: none"> • Construction, planning, and design of infrastructure-related projects and systems that will provide safe routes for non-drivers, including children, older adults, and individuals with disabilities to access daily

<p>Grouping (17993000668)</p>		<p>needs</p> <ul style="list-style-type: none"> • Conversion and use of abandoned railroad corridors for trails for pedestrians, bicyclists, or other non-motorized transportation users • Construction of turnouts, overlooks, and viewing areas
<p>Surface Transportation System Preservation And Operation – TAP Grouping (17993000668)</p>	<ul style="list-style-type: none"> • Transportation Alternatives projects, 	<p>Community improvement activities, which include but are not limited to:</p> <ul style="list-style-type: none"> • Inventory, control, or removal of outdoor advertising. • Historic preservation and rehabilitation of historic transportation facilities. • Vegetation management in transportation rights-of-way to improve roadway safety, prevents invasive species, and provides erosion control. <p>Archaeological activities relating to impacts from implementation of a transportation project eligible under Title 23 of the U.S.C.</p>
<p>Surface Transportation System Preservation And Operation – TAP Grouping (17993000668)</p>	<ul style="list-style-type: none"> • Transportation Alternatives projects, 	<p>Any environmental mitigation activity, including pollution prevention and pollution abatement activities and mitigation to:</p> <ul style="list-style-type: none"> • Address storm water management, control, and water pollution prevention or abatement related to highway construction or due to highway runoff. • Reduce vehicle-caused wildlife mortality or to restore and maintain connectivity among terrestrial or aquatic habitats
<p>Grouping Category</p>	<ul style="list-style-type: none"> • Function of Grouping Activities 	<p>Allowable Work Types</p>
<p>Surface Transportation System Preservation and Operation – SRTS Grouping</p>	<ul style="list-style-type: none"> • Projects for the creation, rehabilitation, and maintenance of multi-use recreational trails. 	<ul style="list-style-type: none"> • SRTS Program infrastructure-related projects, non-infrastructure-related activities (such as pedestrian and bicycle safety and educational activities advanced under the SRTS program), and SRTS Coordinator positions. • Planning, designing, or constructing boulevards and other roadways largely in the right-of-way of former Interstate System

		routes or other divided highways
Grouping Category	Function of Grouping Activities	Allowable Work Types
Surface Transportation System Preservation And Operation – RTP Grouping (17993000671)	<ul style="list-style-type: none"> Recreational Trail Program projects. 	<p>Recreational Trails Program activities under 23 U.S.C. 206.</p> <ul style="list-style-type: none"> Maintenance and restoration of existing recreational trails Development and rehabilitation of trailside and trailhead facilities and trail linkages for recreational trails Purchase and lease of recreational trail construction and maintenance equipment Construction of new recreational trails Acquisition of easements and fee simple title to property for recreational trails or recreational trail corridors Assessment of trail conditions for accessibility and maintenance Development and dissemination of publications and operation of educational programs to promote safety and environmental protection Payment of costs to the State incurred in administering the program Projects authorized from this grouping may be authorized with different Federal percentages than shown in the grid based on one or more of the following: <ul style="list-style-type: none"> 90% for certain Interstate projects under 23 U.S.C. 120(a); or 100% for certain safety items under 23 U.S.C. 120(c); or 100% for Appalachian Development Highway System (ADHS) projects under MAP-21 §1528 and 40 U.S.C. 14501.
Grouping Category	Function of Grouping Activities	Allowable Work Types
Surface Transportation	Surface transportation workforce development, training, and education	<ul style="list-style-type: none"> Direct educational expenses (not including salaries) in connection with the education and

System Preservation and Operation - Workforce Development, Training, and Education Grouping (126832.00)	activities.	training of transportation employees <ul style="list-style-type: none"> • National Highway Institute (NHI) course participation • College and University cooperative education programs relating to surface transportation including student internships, outreach to develop interest and promote participation in transportation careers, or activities that will help students prepare for a career in transportation • Local technical assistance programs (LTAP) • Projects authorized from this grouping may be authorized with different Federal percentages than shown in the grid based on one or more of the following: <ul style="list-style-type: none"> ○ 90% for certain Interstate projects under 23 U.S.C. 120(a); or ○ 100% for certain safety items under 23 U.S.C. 120(c); or ○ 100% for Appalachian Development Highway System (ADHS) projects under MAP-21 §1528 and 40 U.S.C. 14501.
Grouping Category	<ul style="list-style-type: none"> • Function of Grouping Activities 	Allowable Work Types

Public Involvement Documentation

As mentioned previously, TDOT advertised the review and comment period for the draft STIP as well as public meeting times in various newspapers across the state. Specifically, these items were advertised in the following publications:

- Chattanooga Times Free Press (Hamilton Co.)
- Knoxville News Sentinel (Knox Co.)
- The Commercial Appeal (Shelby Co.)
- The Kingsport Times-News (Sullivan Co.)
- The Tennessean (Davidson Co.)
- The Daily News Journal (Rutherford Co.)
- The Leaf-Chronicle (Montgomery Co.)
- Chattanooga Courier (Hamilton Co.)
- Clarksville Press (Montgomery Co.)
- Knoxville Enlightener (Knox Co.)
- Murfreesboro Vision (Rutherford Co.)
- Nashville Pride (Davidson Co.)
- El Crucero De Tennessee (Davidson Co.)
- Tri-State Defender (Shelby Co.)

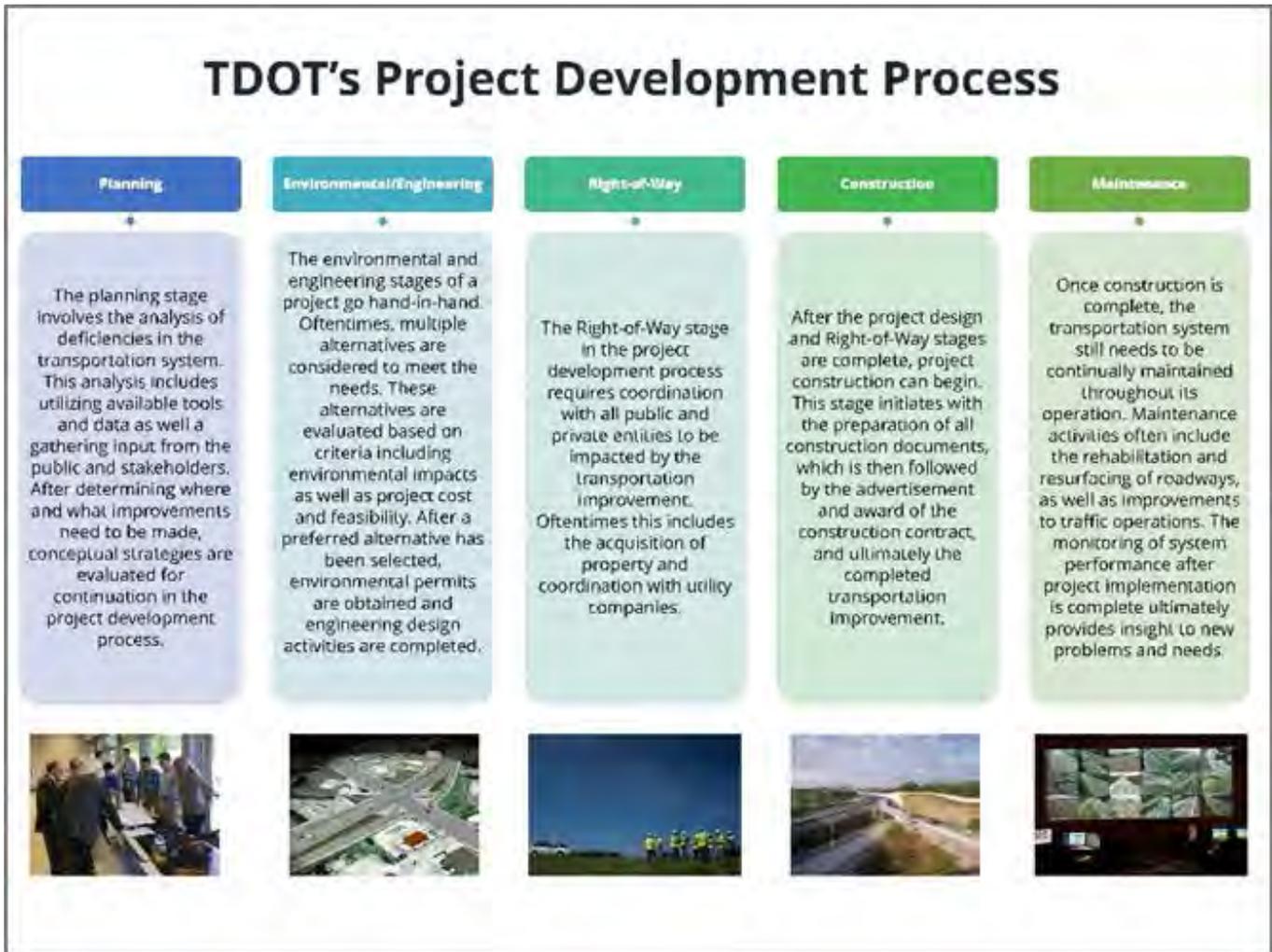
The public review and comment period was advertised to be held from August 5, 2019 through September 13, 2019. Residents were able to review paper copies of the draft STIP, which were made available at each of the TDOT Region offices, and provide written comments. Additionally, an online survey was provided through TDOT's website that allowed anyone reviewing the document electronically to provide input on topics such as priorities for investment as well as provide comments related to specific projects. There were no written or electronic comments received throughout the public comment period.

In addition to providing the draft STIP online and at the TDOT Region offices, meetings were held across the state to provide an opportunity for residents to review the document and ask questions. These public meetings were held in all four TDOT regional offices at the following locations from 4:00 to 6:00 local time:

- TDOT Region 1: 7345 Region Lane, Knoxville, TN 37914
- TDOT Region 2: 7512 Volkswagen Drive, Chattanooga TN 37421
- TDOT Region 3: 6601 Centennial Blvd, Nashville, TN 37243
- TDOT Region 4: 300 Benchmark Place, Jackson, TN 38301

These meetings utilized a variety of methods to present information and solicit input from attendees. Google earth was utilized at each meeting so that residents would be able to look at the distribution of projects spatially using aerial photography. Display boards, such as the ones seen below, were created for each TDOT Region in order to convey how much TDOT is investing in the rural areas across the state, the distribution of allocations for the different project phases being funded, the project allocations within the MPO areas, and the allocations for various groupings included in the STIP. Additionally, TDOT's project development process was displayed to

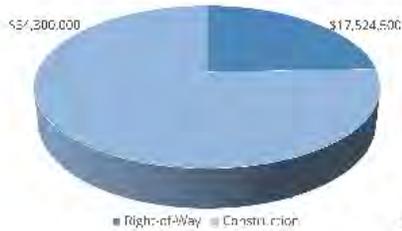
convey the complexities of seeing a project carried from concept to construction. Sign-in sheets showing the participants in these meetings are located on the following pages. No comments were received during the public comment period.



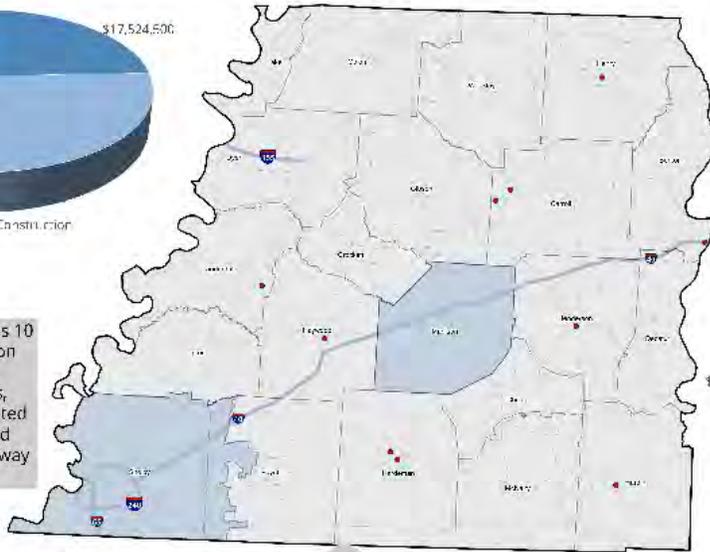
TDOT's Project Development Process (Fig. A.)

TDOT Region 4 consists of 21 counties in west Tennessee, the majority of which are considered rural. Nearly \$179 million worth of specific transportation projects are currently underway in these rural counties.

Region 4



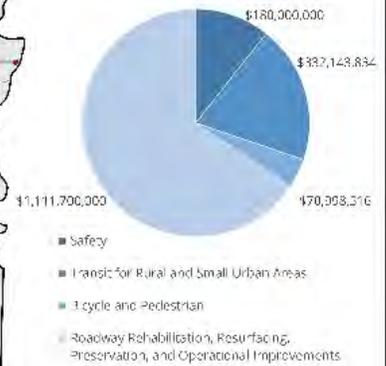
The 2017-2020 STIP contains 10 projects totaling \$71.8 million in allocations to the rural counties in Region 4. Of this, approximately 75% is allocated to construction activities and 25% is allocated to right-of-way activities.



Legend
 TDOT's STIP Projects
 MPO TIP Area
 Rural STIP Area

Projects in the urban areas are included in Metropolitan Planning Organization (MPO) Transportation Improvement Programs (TIPs). For Region 4, approximately \$372 million are allocated to TDOT project activities within the Jackson and Memphis MPO areas.

In addition to project-specific investments, the 2017-2020 STIP includes nearly \$1.7 billion for statewide rural programs to address issues such as mobility, safety, maintenance, transit operations, and other needs.



Statewide Transportation Improvement Program (STIP)
 Fiscal Years 2017-2020



TDOT's Project Development Process (Fig. B.)

Public Relation Notification to the Public for Comments on the STIP

Voucher # 00910419
 TX00063043
 Dept ID 4036330010



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Agency	Chelsea Bell Tennessee Dept - TDOT Community Relations Division Attn: Chelsea Bell James K. Polk Building, 7th Floor 505 Deaderick Street Nashville, TN 37243-	Invoice Date	8/27/2019
		PO Number	
		Order	S19.9342
Client	TDOT Community Relations Division		
Reps	State	Earl Goodman	

Newspaper

Caption	Run Date	Ad Size	Rate	Rate Name	Color	Disc.	Total
Chattanooga - Chattanooga Courier Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/08/2019	2 x 3.5	\$30.00	Classified National	\$0.00	0.0000%	\$210.00
Chattanooga - Chattanooga Times Free Press Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/06/2019	2 x 3.5	\$48.55	Public Notice Classified	\$0.00	0.0000%	\$339.85
Clarksville - Clarksville Press Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/07/2019	2 x 3.5	\$26.25	Classified National	\$0.00	0.0000%	\$183.75
Clarksville - The Leaf-Chronicle Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/06/2019	2 x 3.5	\$41.48	Public Notice Classified	\$0.00	0.0000%	\$290.36
Jackson - The Jackson Sun Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/06/2019	2 x 3.5	\$44.63	Public Notice Classified	\$0.00	0.0000%	\$312.41
Kingsport - The Kingsport Times-News Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/06/2019	2 x 3.5	\$32.90	Classified National	\$0.00	0.0000%	\$230.30
Knoxville - Knoxville News Sentinel Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/06/2019	2 x 3.5	\$57.30	Public Notice Classified	\$0.00	0.0000%	\$401.10

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Agency	Chelsea Bell Tennessee Dept - TDOT Community Relations Division Attn: Chelsea Bell James K. Polk Building, 7th Floor 505 Deaderick Street Nashville, TN 37243-	Invoice Date	8/27/2019
		PO Number	
		Order	S19.9342
Client	TDOT Community Relations Division		
Reps	State	Earl Goodman	

Newspaper

Caption	Run Date	Ad Size	Rate	Rate Name	Color	Disc.	Total
Knoxville - The Enlightener Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/07/2019	2 x 3.5	\$30.00	Classified National	\$0.00	0.0000%	\$210.00
Memphis - The Commercial Appeal Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/06/2019	2 x 3.5	\$168.00	Public Notice Classified	\$0.00	0.0000%	\$1,176.00
Memphis - The Tri-State Defender Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/08/2019	5.437 x 5.25	\$820.00	Public Notice Classified	\$0.00	0.0000%	\$820.00
Murfreesboro - Murfreesboro Vision Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/08/2019	2 x 3.5	\$26.25	Classified National	\$0.00	0.0000%	\$183.75
Murfreesboro - The Daily News Journal Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/06/2019	2 x 3.5	\$41.13	Public Notice Classified	\$0.00	0.0000%	\$287.91
Nashville - El Crucero De Tennessee Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/09/2019	2 x 3.5	\$42.50	Classified National	\$0.00	0.0000%	\$297.50
Nashville - Nashville Pride Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/09/2019	2 x 3.5	\$42.50	Classified National	\$0.00	0.0000%	\$297.50

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Agency	Chelsea Bell Tennessee Dept - TDOT Community Relations Division Attn: Chelsea Bell James K. Polk Building, 7th Floor 505 Deaderick Street Nashville, TN 37243-	Invoice Date	8/27/2019
Client	TDOT Community Relations Division	PO Number	
Reps	State Earl Goodman	Order	S19.9342

Newspaper

Caption	Run Date	Ad Size	Rate	Rate Name	Color	Disc.	Total
Nashville - The Tennessean Notice Of Public Review/2020-2023 State Transportation Improvement Program	08/06/2019	2 x 3.5	\$175.00	Public Notice Classified	\$0.00	0.0000%	\$1,225.00



Total Advertising	\$6,465.43
Discounts	\$0.00
Tax: USA	\$0.00
Total Invoice	\$6,465.43
Payments	\$0.00
Adjustments	\$0.00
Balance Due	\$6,465.43

Approved: Chelsea Bell

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AFFIDAVIT

Attn: Chelsea Bell
TDOT Community Relations Division
James K. Polk Building
505 Deaderick Street, 7th Floor
Nashville, TN 37243

To Whom It May Concern:

The enclosed itemized statement is for publication of "Notice of Public Review/2020-2023 State Transportation Improvement Program" in the list of newspaper(s) on invoice **S19.9342** on the date (s) stated therein as authorized by your office.

As requested, clipping of advertisement are submitted herewith as further proof of publication.

I, *Alisa Subhakul*, Advertising Assistant of Tennessee Press Service, Inc., do hereby certify that the attached is true and correct.

This Twenty-sixth day of August, 2019

Alisa Subhakul, Advertising Assistant

Sworn before me this 26th day of August, 2019

Earl Goodman, Notary Public

My commission expires August 02, 2020



NOTICE OF PUBLIC REVIEW

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70-200112094

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The ENLIGHTENER

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MC-00824407-01

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The New Tri-State Defender

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TN-00112019

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Nashville PRIDE

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NOTICE OF PUBLIC REVIEW

The Tennessee Department of Transportation (TDOT) announces the beginning of a 30-day review and comment period on its 2020-2023 State Transportation Improvement Program (STIP). The STIP establishes projects for major construction and Public Transit over the four-year period, which will utilize available and projected Federal funds. The review period is to provide citizens, affected public agencies, employees of transportation agencies, various stakeholder groups and other interested parties a reasonable opportunity to comment on the STIP. The STIP will be available in hard copy for review at TDOT's four Regional Offices during normal business hours, Monday-Friday, from August 5, 2019 through September 13, 2019. The locations are as follows:

- Region 1: 7345 Region Lane, Knoxville, TN 37914, (865) 594-2400
- Region 2: 7512 Volkswagen Drive, Chattanooga, TN 37421, (423) 892-3430
- Region 3: 6601 Centennial Blvd., Building R, Nashville, TN 37243, (615) 350-4300
- Region 4: 300 Benchmark Place, Jackson, TN 38301, (731) 935-0100

It can also be accessed by going to TDOT's home page www.tn.gov/tdot or at https://www.tn.gov/content/dam/tn/tdot/programdevelopment/stateprograms/SPDraftSTIP2020-2023_Draft_07192019R.pdf. Written comments may be submitted at any time during the comment period to: Mr. Brian Hurst, Transportation Manager II, Suite 600, James K. Polk Bldg, 505 Deaderick Street, Nashville, TN 37243-0341, (615) 253-1110, www.TDOT.Comments@tn.gov. In addition, appropriate TDOT staff will be available at the Regional Offices to answer questions and accept comments on the following dates:

- Region 1: Wednesday, August 21, 2019, 2:00 – 4:30 p.m. EDT
- Region 2: Monday, August 26, 2019, 2:00 – 4:30 p.m. EDT
- Region 3: Tuesday, August 27, 2019, 2:00 – 4:30 p.m. CDT
- Region 4: Tuesday, August 20, 2019, 2:00 – 4:30 p.m. CDT

TDOT will respond to all comments. The Tennessee Department of Transportation is an Equal Opportunity Employer and does not discriminate on the basis of race, age, sex, religion, color, disability or national origin.

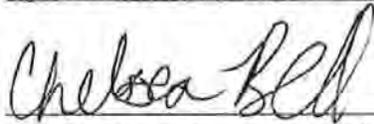
TN-00112019

General Services Local Purchase Exempt Items Certification

General Services has determined a list of items which qualify for Local Purchase Exemption as per the General Services Procurement Procedures Manual section 6.7 dated 03/24/2016. Under this exemption, only a voucher is required when processing an invoice for the Exempt Items listed below.

The exempt items must meet the following four criteria: (a) they are not available on a statewide or agency term contract; (b) they are supported by an invoice from the vendor for the goods or services; (c) performance occurs in no more than ninety (90) days; and (d) the purchase does not exceed ten thousand dollars (\$10,000). The ten thousand dollar (\$10,000) limit does not apply to items with an asterisk.

To process this Local Purchase Exemption the buyer must **circle** the exempt item(s) being invoiced, **sign** this form and **insert a check mark** by all four criteria boxes below certifying the invoiced items fully meet General Services Procurement Procedures. Please attach this form, unless otherwise noted, along with a signed invoice to the voucher in PDF format.



I certify that this item(s) does not require a Purchase Order per General Services Policy. I verify all criteria below are met by placing a check mark in each box.

- not available on a statewide or agency term contract;
- supported by an invoice from the vendor for the goods or services;
- performance occurs in no more than ninety (90) days; and
- purchase does not exceed ten thousand dollars (\$10,000) unless the item is noted by an asterisk *

Exempt Items please circle item(s) being invoiced.

Landfill Charges

Late Fees

Books, periodicals, or publications

Newspaper, Radio, Television, or Internet Advertisements*

Freight Charges (not incurred with the purchase of supplies and equipment)

Postage Charges*

Bonding Fees or Notary Public Fees

Building Permits

Deed Registration Fees

Court Fees

Title fees, Title Insurance*, or Title Search

Tuition Fees and Supplies for Training

Meeting Expenses (Reservation Fees, Refreshments, Attendant expenses)

Rental of Vehicle (while on approved travel)

Chemical and Pesticide Samples (mandated by Federal and State testing by Dept. of Agriculture)

The following items also are considered **Exempt Items** according to General Services Procurement Procedures; however this certification is not required to be completed or attached to the voucher for the processing of payment:

Telephone Billings*

Utility Bills, including Connection Fees*

08/22/2016

Public Comments Region Sign-in Sheets



TENNESSEE DEPARTMENT OF TRANSPORTATION
 Public Review of 2020-2023 State Transportation Improvement Program (STIP)
 Region 3
 August 27, 2019

Name	Address	Phone	Email
Jonathan Russell	TDOT - LRP	615-306-5028	Jonathan.Russell@tdot.gov
Melanie Murphy	TDOT User	615-253-2158	melanie.murphy@tdot.gov

Public Comments Notification to FLMA's

From: Robert Hayzlett

Sent: Wednesday, October 9, 2019 10:52 AM

To: barry_boyd@nps.gov; Effie_ <Houston@nps.gov>; tadd.s.potter@usace.army.mil; ttilley@fs.fed.us; William Douglas White <wdwhite0@tva.gov>; jmorris@fs.fed.us

Cc: 'sean.santalla@dot.gov' <sean.santalla@dot.gov>; Watkins, Elizabeth (FHWA) (elizabeth.watkins@dot.gov) <elizabeth.watkins@dot.gov>; Claxton, Theresa (FHWA) <Theresa.Claxton@dot.gov>; Brian Terrell <Brian.Terrell@tn.gov>; Brian Hurst <Brian.Hurst@tn.gov>; Deborah Fleming <Deborah.Fleming@tn.gov>; Kim Y. Brymer <Kim.Y.Brymer@tn.gov>

Subject: Draft of Proposed FY 2020-2023 State Transportation Improvement Program Sent to FLMA

The Tennessee Department of Transportation (TDOT) invites you to comment on the 2020-2023 State Transportation Improvement Program (STIP). The STIP establishes projects for major construction and public transit over the four-year period, which will utilize available and projected Federal funds. This document may be accessed on the TDOT website at

https://www.tn.gov/content/dam/tn/tdot/programdevelopment/stateprograms/SPDraftSTIP2020-2023_Draft_07192019R.pdf. This posting provides citizens, affected public agencies, employees of transportation agencies, various stakeholder groups and other interested parties a reasonable opportunity to comment on the STIP.

TDOT staff was available at its four Regional Offices in August 2019 to answer questions and accept comments on the proposed STIP. The Federal Highway Administration (FHWA) directed that TDOT reach out to Federal Land Management Agencies and ask them to review the document.

Written comments may be submitted to:

Mr. Brian Hurst, Transportation Manager II
Suite 600, James K. Polk Bldg.
505 Deaderick Street
Nashville, TN 37243-0341
(615) 741-5312
www.TDOT.Comments@tn.gov

TDOT will respond to all comments.

Sincerely,



Bob Hayzlett | Transportation Program Supervisor
Program Development and Administration Division
James K. Polk Building, 6th Floor
505 Deaderick St., Nashville, TN 37243
p. 615-741-5321
robert.hayzlett@tn.gov

Tennessee RPO Contacts

East Tennessee RPO North	Don Brown, RPO Coordinator East Tennessee Development District P.O. Box 249 (Mailing) Alcoa TN 37701-0249 216 Corporate Place (Physical) Alcoa, TN 37701	Phone: (865) 273-6003 Fax: (865) 273-6010 Email: dbrown@etdd.org
East Tennessee RPO South	Don Brown, RPO Coordinator East Tennessee Development District P.O. Box 249 (Mailing) Alcoa TN 37701-0249 216 Corporate Place (Physical) Alcoa, TN 37701	Phone: (865) 273-6003 Fax: (865) 273-6010 Email: dbrown@etdd.org
First Tennessee RPO	Chase Milner, RPO Coordinator First TN Development District 3211 North Roan Street Johnson City, TN 37601	Phone: (423) 722-5217 Fax: (423) 928-5209 Email: cmilner@ftdd.org
Middle Tennessee RPO	Karyssa Helton Mid-Cumberland Human Resource Agency 1101 Kermit Drive, Suite 300 Nashville, TN 37217	Phone: (615) 560-0211 Fax: (615) 833-2585 Email: khelton@mchra.com
West Tennessee RPO	Kamillah Kelly Memphis Area Association of Governments 8289 Cordova Road Ste. 103 Cordova, TN 38016	Phone: (901) 729-2871 Fax: (901) 729-4107 Email: kkelly@maagov.org
Northwest Tennessee RPO	Ben Bradberry, RPO Coordinator PO Box 963 124 Weldon Dr. Martin, TN 38237-0963	Phone: (731) 587-4213 ext.230 Fax: (731) 587-4587 Email: ben.bradberry@nwtd.org

South Central East RPO	Lisa Cross, RPO Coordinator South Central TN Development District 101 Sam Watkins Blvd. Mt. Pleasant, TN 38474	Phone: (931) 379-2914 Fax: (931) 379-2640 E-mail: lcross@sctdd.org
South Central West RPO	Lisa Cross, RPO Coordinator South Central TN Development District 101 Sam Watkins Blvd. Mt. Pleasant, TN 38474	Phone: (931) 379-2914 Fax: (931) 379-2640 Email: lcross@sctdd.org
Southeast Tennessee RPO	Chad Reese, RPO Coordinator, Southeast TN Development District, 1000 Riverfront Parkway, 37402 PO Box 4757 Chattanooga, TN 37405-0757	Phone: (423) 424-4263 Fax: (423) 267-7705 Email: CReese@sedev.org
Southwest Tennessee RPO	Shelton Merrell, RPO Coordinator, Southwest TN Development District, 102 E. College Street Jackson, TN 38301	Phone: (731) 668-6430 Fax: (731) 668-6421 Email: smerrrell@swtdd.org
Center Hill RPO	Virginia Solimine, RPO Coordinator Upper Cumberland Development District 1225 South Willow Avenue Cookeville, TN 38506	Phone: (931) 476-4111 Fax: (931) 432-6010 Email: vsolimine@ucdd.org
Dale Hollow RPO	Mark Dudney, RPO Coordinator Upper Cumberland Development District 1225 South Willow Avenue Cookeville, TN 38506	Phone: (931) 476-4152 Fax: (931) 432-6010 Email: mdudney@ucdd.org

Tennessee MPO Contacts

Bristol MPO	Rex Montgomery Transportation Planning Coordinator P. O. Box 1189 Bristol, TN 37621-1189	Phone: (423) 989-5519 Fax: (423) 989-5717 Email: rmontgomery@bristoltn.org Website: https://www.bristoltn.org
Chattanooga TPO	Karen Rennich Transportation Planning Coordinator Chattanooga-Hamilton County RPA 1250 Market Street, Suite 2000 Development Resource Center Chattanooga, TN 37402	Phone: (423) 643-5903 Fax: (423) 757-5532 Email: tpo@chattanooga.gov Website: https://chcrpa.org
Clarksville MPO	Stan Williams Transportation Planning Coordinator, Clarksville-Montgomery County Regional Planning Commission 329 Main Street Clarksville, TN 37040	Phone: (931) 645-7448 Ext. 16 Fax: (931) 645-7481 Email: stanwilliams@cityofclarksville.com Website: http://www.cuampo.com/
Cleveland MPO	Greg Thomas Transportation Planning Coordinator 185 2nd St., N.E. Cleveland, TN 37311	Phone: (423) 479-1913 Ext. 223 Fax: (423) 559-3373 Email: gthomas@clevelandtn.com Website: http://www.clevelandtn.gov
Jackson MPO	James Matthews Transportation Planning Coordinator Jackson Municipal Regional Planning Commission 111 East Main Street, Suite 201 Jackson, TN 38301	Phone: (731) 425-8275 Fax: (731) 425-8281 Email: jmatthews@cityofjackson.net Website: http://www.cityofjackson.net

Johnson City MTPO	Glenn Berry Transportation Planning Coordinator Johnson City MTPO 137 West Market Street Johnson City, TN 37601	Phone: (423) 434-6272 Fax: (423) 434-2702 Email: glennberry@jcmpto.org Website: https://www.jcmpto.org/
Kingsport MTPO	Bill Albright Transportation Planning Coordinator Kingsport Transportation Department 201 West Market Street Kingsport, TN 37660	Phone: (423) 224-2670 Fax - (423) 224-2756 Email: BillAlbright@KingsportTN.gov Website: https://www.kingsporttn.gov
Knoxville TPO	Jeffrey Welch Transportation Planning Coordinator, Knoxville Regional TPO 400 Main Avenue, Suite 403 Knoxville, TN 37902-2476	Phone: (865) 215-3790 Fax: (865) 215-2068 E-mail: Jeff.Welch@knoxtrans.org Website: https://knoxtpo.org/
Lakeway MTPO	Rich DesGroseilliers Transportation Planning Coordinator Lakeway Area MTPO P. O. Box 1499 Morristown, TN 37816-1499	Phone: (423) 581-6277 Fax: (423) 585-4679 Email: richd@mymorristown.com Website: https://www.lamtpo.com/
Memphis MPO	Ms. Pragati Srivastava Transportation Planning Coordinator, Memphis-Shelby County Department Of Regional Services 125 North Main St., Suite 450 Memphis, TN 38103	Phone: (901) 576-7198 Fax: (901) 576-7272 Email: Pragati.Srivastava@memphistn.gov Website: www.memphismpo.org

Nashville MPO	Sean Pfalzer Transportation Planning Manager Nashville Metropolitan Planning Organization Ste. 300, 138 Second Ave N Nashville, TN 37228	Phone: (615) 862-7186 Fax: (615) 862-7209 Email: tip@nashvillempo.org Website: www.nashvillempo.org
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TDOT Region Contacts

Region 1	Steve Borden, Director 7345 Region Lane Knoxville, TN 37914	Phone: (865) 594-2400 Fax: (865) 594-2642 Email: Steve.Borden@tn.gov
Region 2	Joe Deering, Director 7512 Volkswagen Drive Chattanooga, TN 37416	Phone: (423) 892-3430 Fax: (423) 899-1636 Email: Joe.Deering@tn.gov
Region 3	David Layhew, Director 6601 Centennial Boulevard Nashville, TN 37243	Phone: (615) 350-4300 Fax: (615) 350-4396 Email: david.layhew@tn.gov
Region 4	Jason Baker, Director 300 Benchmark Place Jackson, TN 38301	Phone: (731) 935-0193 Fax: (731) 935-0195 Email: Jason.Baker@tn.gov

<https://www.tn.gov/tdot/about/tdot-organizational-charts/tdot-key-staff-directory.html>

Important Links

ITS Statewide Architecture Plan

[ITS Statewide Architecture Plan](#)

TDOT Strategic Direction

<https://www.tn.gov/tdot/strategic-planning-home/tdot-strategic-direction.html>

ADA Transition Plan and Certification Status

<https://www.tn.gov/tdot/government/g/ada-office0/transition-plan.html>

<https://www.tn.gov/content/tn/tdot/government/g/ada-office0/local-agency-ada-self-evaluation--transition-plan--and-self-certification-status.html>

Multimodal Transportation Project Scoping Manual

<https://www.tn.gov/content/dam/tn/tdot/multimodaltransportation/TDOT%20Multimodal%20Project%20Scoping%20Manual.pdf>

Eastern Federal Lands Highway Division TIP

The Office of Federal Lands Highway (FLH) of the US Department of Transportation (USDOT), Federal Highway Administration (FHWA), was established to promote effective, efficient, and reliable administration for a coordinated program of federal public roads and bridges; to protect and enhance the Nation's natural resources; and to provide needed transportation access for Native Americans. The primary purpose is to provide financial resources and transportation engineering assistance for public roads that service the transportation needs of Federal and Indian lands. The FLH provides these services in all 50 states, the District of Columbia, Puerto Rico, and US Territories through its Headquarters, Eastern, Central, and Western Federal Lands Highway Division offices. Projects in Tennessee are administered through the Eastern Division office in Sterling, Virginia.

FLH is enabled and entrusted to administer many different programs and types of funds to facilitate transportation improvements for many Federal Land Management Agency Partners such as the National Park Service (NPS); USDA Forest Service (Forest Service); U.S. Fish and Wildlife Service (USFWS); Bureau of Indian Affairs (BIA) and Tribal Governments; Bureau of Land Management (BLM); Department of Defense (DOD); U.S. Army Corps of Engineers (USACE); and the Bureau of Reclamation (BOR).

Contact Information:

Lewis G. Grimm, P.E.

Eastern Federal Lands Highway Division

21400 Ridgetop Circle

Sterling, VA 20166

Phone: 703-404-6289

Email: Lewis.Grimm@dot.gov

Website: <https://flh.fhwa.dot.gov/>

FY2020-FY2023 Transportation Improvement Program
 Federal Highway Administration Eastern Federal Lands Highway Division

PROJECT NAME	FY2020 FISCAL YEAR	STATE	COUNTY	FEDERAL AGENCY	PROJECT DESCRIPTION	TYPE OF PROJECT	PROJ. NO.	TOTAL PROJECT COST	CHG. EST.	PROJECT STATUS	CONTRACT/AGENCY	
TENNESSEE												
NP CHOH 300(1), 501(1), ETC	2021	GA TN	Walker and Calusa, GA, Hamblen, TN	Chickamauga-Chatahoochee National Park	Repair, Rehabilitation, Reconstruction of Multiple roads near CHOH National Park	3RH	FLTP	\$3,596,392	Title 23	EFLHD	Planned	VARIOUS NPS SE
TENNESSEE												
GRSM 15(2), 18(1)	2018	TN	Blount, Sevier	Great Smoky Mountains National Park	Pavement Preservation for Laurel Creek, Trenton and Townsend Entrance Rds.	2R	FLTP	\$3,250,000	Title 23	EFLHD	In Design	TN-02 NPS SE
SO CE Perry Pres(1)	2018	TN	Davidson	J Perry Plant Dam, Anderson Road Campground, Seven Pines Campground	Repairing resurfacing pavement Anderson Road Campground on J Perry Presist Lake.	3RL, 2R	FLTP	\$621,500	Title 23	USACE	Under Construction	TN-05 USACE_GLO RD
SOTINERFO-PV CHIC 2016-(1)	2018	TN	Lauderdale & Tipton	Chickasaw & Lower Fentles National Wildlife Refuge	Repair Ed Jones Rd & Shankle Lale Rd. ERFODSR # CHIC3-100-R-ARR-0-0-0 & CH44-102-SHR-0-0-0.	3RH	FLTP	\$199,810	Title 23	FWS	Planned	TN-08 FWS R4
SOTINERFO PV CRCR 2016-(1)	2018	TN	Sewanee	Cross Creeks National Wildlife Refuge	Repair Routes 13 & 100 ERFODSR #CC06-013-MSRA-8-6 CC7-100L-PMA-0-3 CC8-100L-PMA-0-5.	3RH	FLTP	\$32,739	Title 23	FWS	Planned	TN-07 FWS R4
SOTINERFO PV TENN 2016-(1)	2018	TN	Henry, Humphreys & Benton	Tennessee National Wildlife Refuge	ERFODSR # TN1-106 TN10-108 (MP 0-7), TN12-106 (MP 1-3,1-35), TN17-112 TN21-914, TN31-15, TN35-11	3RH	FLTP	\$267,611	Title 23	FWS	Planned	TN-08 FWS R4
FS Bald River Falls BH def(1)	2019	TN	Monroe	Cherokee National Forest	Bridge Replacement	3RH BRRP	FLTP	\$125,000	Title 23	USFS	In Design	TN-01 USFS R8
GRSM 15(1)	2019	TN	Blount	Great Smoky Mountains National Park	Repair Boie Mountain Tunnel	TUN	FLTP	\$1,300,000	Title 23	EFLHD	Planned	TN-01 NPS SE
NP NATR 351-0-463P	2019	TN	Williamson	Natchez Trace Parkway	Pedestrian Barrier on Double Arch Bridge (St. #657)-463P to reduce suicide fatalities	BRRP	NPS	\$1,250,000	Title 23	EFLHD	In Design	TN-07 NPS SE
NP STR1 TRAF/ENC Impact Study	2019	TN	Rutherford	Stones River National Battlefield	Develop SQM, ICE & proposal STR1 Traffic Impact Safety Study (Coordinate ABE & award EFL serve as CON	MISC	FLPP	\$130,000	Title 23	EFLHD	Planned	TN-06 NPS SE
NP BISO 10(2)	2019	TN	Scott, Fentness	Big South Fork National River and Reservoir Area	Leathwood Ford Road Resurfacing, MP 0.00 to MP 7.56. Replace existing signs and posts and stripe.	3RL	FLTP	\$3,319,966	Title 23	EFLHD	Planned	TN-03 NPS SE
NP GRSM 13(1)4(2)18 (1)	2019	TN	Sevier, Blount	Great Smoky Mountains National Park	Pavement Preservation of Fighting Creek Gap, Little River Gorge and Elmont Rds.	2R	FLTP	\$2,800,000	Title 23	EFLHD	Planned	TN-02 NPS SE

FY2020-FY2023 Transportation Improvement Program
Federal Highway Administration Eastern Federal Lands Highway Division

TN FLAP MLGTM AST1 1)	2019	TN	Shelby	Dept of the Navy, Naval Support Activity (NSA) Mid-South	Construct an extension of a two-lane access route from the Millington Depot to the Navy Base	3RH NEW	FLAP	\$2,386,390	Title 23	LOCAL	In Design	TN-48	X NA OTH
NP GRSM 10N(1)	2020	TN	Sear	Great Smoky Mountains National Park	Permanent Preservation of Newfound Gap Road, TN Side (MP 6.5 to 19)	ZR	FLTP	\$2,163,298	Title 23	EFJHD	Planned	TN-42	NPS SE
NP GRSM 126(1)	2020	TN	Various	Great Smoky Mountains National Park	Replace five bridges on Forge Creek Road	BRRC	FLTP	\$2,799,026	Title 23	EFJHD	Planned	TN-41	NPS SE
TN FLAP HMMN SHER(1)	2020	TN	Hickman	NPS/Natchez Trace Parkway	Resurfacing of New Shabazz Road to the concrete ditch lines.	3RL	FLAP	\$956,912	Title 23	LOCAL	In Design	TN-47	NPS SE
TN FLAP LEWIS KEG(1)	2020	TN	Lewis	NPS/Natchez Trace Parkway	Resurfacing of Keg Springs Road and Falk Branch Rd to improve safety.	3RL	FLAP	\$450,148	Title 23	LOCAL	In Design	TN-45	NPS SE
TN FLAP TECC NEMAOBR(1)	2020	TN	Meigs	National Park Service/ Old Wild and Scenic River	Restoration of culturally, historic vehicular bridge.	BRRP	FLAP	\$1,020,000.00	Title 23	STATE	Planned	TN-43	NPS SE
TN FLAP WAAYNE PIWET(1)	2020	TN	Wayne	NPS/Natchez Trace Parkway	Resurfacing and widening of Prayey Road to improve safety.	3RL	FLAP	\$542,120.00	Title 23	LOCAL	In Design	TN-49	NPS SE
TN FLAP CHAT MEG(1)	2020	TN	Hamilton	NPS, Chickamauga and Chattanooga National Military Park	Extend the Moccasin Bend Connector in Chattanooga, TN.	3RH, 3RL	FLAP	\$1,631,828.00	Title 23	LOCAL	In Design	TN-43	NPS SE
TN FLAP DOT 461(1)	2020	TN	Stewart	USFS - Land Between the Lakes National Recreation Area	Resurfacing 3 miles of Route 461 from intersection with Route 76 heading north	3RL	FLAP	\$553,500.00	Title 23	STATE	Planned	TN-47	USFS R4
TN FLAP FRINN BRLN(1)	2020	TN	Franklin	Department of the Air Force, Arnold Eng. Development, Cir.	Resurfacing and widening of UTSI Road for the accommodation of new bike lanes in Franklin County, TN.	3RH	FLAP	\$572,183.00	Title 23	LOCAL	In Design	TN-44	X NA OTH
TN FLAP HARRY RHE(1)	2020	TN	Henry	USFWS, Tennessee National Wildlife Refuge	Rehabilitate portions of Bobcat Den Rd & Elkhorn Nobles Rd within TN National Wildlife Refuge	3RH, 3RL	FLAP	\$283,024.00	Title 23	LOCAL	In Design	TN-48	FWS R4
TN FLAP MTCARVLA(1)	2020	TN	Montgomery	Department of the Army, Fort Campbell	Reconstruction and widening of 1,100 feet of Lafayette Road.	3RH NEW	FLAP	\$1,968,000.00	Title 23	LOCAL	In Design	TN-47	X NA OTH
TN FLAP POLK LADD(1)	2020	TN	Polk	USFS - Cherokee National Forest	Resurface and widen Ladd Springs Road to improve safety in Polk County, TN.	3RL	FLAP	\$941,011.00	Title 23	STATE	Planned	TN-43	USFS R8
TN FLAP POLK SGLF(1)	2020	TN	Polk	USFS - Cherokee National Forest	Resurface and widen Sugarbait Drive to improve safety in Polk County, TN.	3RL	FLAP	\$270,872.00	Title 23	STATE	Planned	TN-43	USFS R8
TN FLAP SMART WARR(1)	2020	TN	Stewart	FWS/Cross-Creek National Wildlife Refuge	Upgrading Walker Rd. & Wadd Hollow Rd. to paved surface.	3RL	FLAP	\$567,292.47	Title 23	LOCAL	Planned	TN-47	FWS R4
TN FLAP WAAYNE WC RM(1)	2020	TN	Wayne	NPS, Natchez Trace Parkway	Resurface and widen Weatherford Creek Road from 18 ft to 22 ft. in Wayne County, TN.	3RL	FLAP	\$432,192.00	Title 23	LOCAL	In Design	TN-47	NPS SE

FY2020-FY2023 Transportation Improvement Program
 Federal Highway Administration Eastern Federal Lands Highway Division

TN_FLAP_MAYNE_WR	2020	TN	Wayne	National Park Service (NPS) Natchez Trace Parkway	Resurface and widen Wright Ridge Road from 18 ft to 22 ft in Wayne County, TN	3RL	FLAP	\$290,004.00	Title 23	LOCAL	Planned	TN-07	NPS SE
TN_FLAP_WMNCH_TWNML	2020	TN	Franklin	Twin Creeks Marina	Construct an asphalt road leading to Twin Creeks Marina	NEW	FLAP	\$958,627.00	Title 23	LOCAL	In Design	TN-05	X, IA, OTH
NP_FOOT_PMSIS1	2021	TN	various	Great Smoky Mountains National Park	FY21 Pavement Preservation of Foothills Parkway, West MP 35 to 71	3RH 3RL	FLTP	\$5,921,693.00	Title 23	ERLHD	Planned	TN-02	NPS SE
NP_FOOT_PMSIS2	2021	TN	various	Great Smoky Mountains National Park	FY21 Pavement Preservation of Foothills Parkway, East	3RH 3RL	FLTP	\$2,946,624.00	Title 23	ERLHD	Planned	TN-02	NPS SE
NP_GFSM_14(3)_112(1)	2021	TN	Sewer	Great Smoky Mountains National Park	Rehabilitate & Repair 8 Bridges on TN Side of the Park	3RH	FLTP	\$2,634,465.00	Title 23	ERLHD	Planned	TN-01	NPS SE
ETC	2021	TN	Sewer	Great Smoky Mountains National Park	\$460,413P-042P-048P-050P-080P-079P	3RL 2R	FLTP	\$2,737,158.00	Title 23	ERLHD	Planned	TN-02	NPS SE
NP_GFSM_17(1)	2021	TN	Sewer	Great Smoky Mountains National Park	Pavement Preservation of Cigarman's dome Road	3RL 2R	FLTP	\$2,121,292.00	Title 23	ERLHD	Planned	TN-02	NPS SE
NP_GFSM_24(2)_137(1)	2021	TN	Blount	Great Smoky Mountains National Park	Pavement Preservation of Cades Cove Loop road and Campground entrance road	3RL 2R	FLTP	\$1,081,340.80	Title 23	LOCAL	In Design	TN-07	FWS Rd
TN_FLAP_SWMT_BELL1	2021	TN	Stewart	Religie	Resurfacing Baldwood Hollow & Baldwood Landing Rd.	3RL	FLAP	\$502,744.00	Title 23	LOCAL	Planned	TN-07	NPS SE
TN_FLAP_MAYNE_BGC	2021	TN	Wayne	NPS, Natchez Trace Parkway	Resurfacing and widening Big Cypress Road	3RL	FLAP	\$3,736,658.00	Title 23	ERLHD	Planned	TN-02	NPS SE
NP_GFSM_21(1)	2022	TN	Cobby	Great Smoky Mountains National Park	Rehabilitate Cobby Campground Entrance	3RL	FLTP						