

GENERAL NOTES

- SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION. (MARCH 1, 1995 EDITION)
- DESIGN SPECIFICATIONS: AASHTO 2002 EDITION WITH ADDENDA INCLUDING THE STANDARD SPECIFICATIONS FOR SEISMIC DESIGN OF HIGHWAY BRIDGES. (SEISMIC PERFORMANCE CATEGORY A WITH ACCELERATION COEFFICIENT <0.075)
- LOADING: HS20-44 AND MILITARY LOADING
- REINFORCING STEEL: SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE. SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.
- CLASS "D" CONCRETE FOR BRIDGE DECKS SHALL BE IN ACCORDANCE WITH SECTION 604 OF THE STANDARD SPECIFICATIONS.
- NO PNEUMATICALLY PLACED CONCRETE ALLOWED.
- BRIDGE DECK SURFACE FINISH: SHALL BE IN ACCORDANCE WITH NOTE (C) IN SUBSECTION 604.23 OF THE STANDARD SPECIFICATIONS.
- CONCRETE CURING: ALL CONCRETE IN REPAIR AREAS SHALL BE CURED ACCORDING TO THE STANDARD SPECIFICATIONS.
- DEMOLITION: THE CONTRACTOR SHALL TAKE SPECIAL CARE TO PROTECT ANY PARTS OF THE STRUCTURE THAT ARE NOT TO BE REMOVED SPECIFICALLY. THE CONTRACTOR IS NOT ALLOWED TO USE A HYDRAULIC RAM MOUNTED ON A BACKHOE (COMMONLY CALLED A HOE-RAM) OR OTHER SIMILARLY HEAVY EQUIPMENT FOR CONCRETE REMOVAL. PNEUMATIC HAMMERS MAY BE USED TO REMOVE UNSOUND CONCRETE. FOR FULL DEPTH CONCRETE SLAB REMOVAL EXCEPT OVER BEAMS, THE MAXIMUM HAMMER SIZE IS 90 POUND CLASS. FOR PARTIAL DEPTH OF CONCRETE SLAB REMOVAL AND ANY WORK OVER BEAMS, THE MAXIMUM HAMMER SIZE IS 60 POUND CLASS. SAWING OR CUTTING OF THE CONCRETE IS ACCEPTABLE SO LONG AS ANY SPECIFIED PROJECTION OF THE EXISTING REINFORCING STEEL IS MAINTAINED. ALL DEVICES PROPOSED FOR CONCRETE DEMOLITION SHALL MEET THE APPROVAL OF THE ENGINEER.
- GROUTED BARS IN DRILLED HOLES: HORIZONTALLY DRILLED HOLES SHALL BE DRILLED 1/2" IN DIAMETER LARGER THAN THE BAR, CLEANED, PACKED WITH NON-SHRINK GROUT AND THE BAR ROTATED (NOT DRIVEN) TO ITS SEAT. VERTICALLY DRILLED HOLES SHALL BE DRILLED 1/4" IN DIAMETER LARGER THAN THE BAR, CLEANED, PACKED WITH EPOXY GROUT AND BAR DRIVEN TO ITS SEAT. ALL GROUTING MATERIAL SHALL BE APPROVED BY T.D.O.T. MATERIALS AND TESTS.
- SHOP DRAWINGS: SHALL BE SUBMITTED ACCORDING TO SPECIAL PROVISION NO. 105A. SHOP DRAWINGS SHALL BE SUBMITTED TO THE BRIDGE REPAIR OFFICE OF THE DIVISION OF STRUCTURES.
- CRACK SEALING WITH METHACRYLATE: APPLY HIGH MOLECULAR WEIGHT METHACRYLATE (HMWM) CRACK SEAL IN TRANSVERSE AND LONGITUDINAL CONSTRUCTION JOINTS. SEALER SHALL BE APPLIED AFTER ALL CONCRETE HAS BEEN IN PLACE A MINIMUM OF TEN (10) DAYS.
- CONTAINMENT AND DISPOSAL OF EXISTING PAINT SYSTEM: ACCORDING TO OUR RECORDS THE EXISTING PAINT SYSTEM CONTAINS LEAD SILICO CHROMATE PAINT, WHICH HAS BEEN DESIGNATED AS A HAZARDOUS MATERIAL. THE CONTRACTOR SHALL CONTAIN AND DISPOSE OF ALL PAINT REMOVAL WASTE IN ACCORDANCE WITH THE TENNESSEE STANDARD SPECIFICATION 603.13.
- WORKER PROTECTION: OUR MAINTENANCE RECORDS INDICATE THAT THE BEARINGS ON THIS BRIDGE WERE ORIGINALLY PAINTED WITH MATERIALS CONTAINING LEAD AND OR CHROMATES AND THE CONTRACTOR SHALL BE REQUIRED TO PROCEED ACCORDINGLY TO MAKE ALL MANDATORY SAFEGUARDS PRESCRIBED BY THE STATE AND FEDERAL LAW FOR WORKER PROTECTION AND HAZARDOUS MATERIAL DISPOSAL.
- PAINTING OF STEEL: ALL STRUCTURAL STEEL SHALL BE BLAST CLEANED AND PAINTED. BLAST CLEANING SHALL BE IN ACCORDANCE WITH TENNESSEE STANDARD SPECIFICATION 603.05 (B). PAINT SHALL BE SYSTEM "A", INORGANIC ZINC PAINT SYSTEM, AS STATED IN STANDARD SPECIFICATION 603.06 (A). COLOR OF THE URETHANE FINISH COAT SHALL COMPLY WITH FEDERAL STANDARD NO. 595A, 2-4-110 BRIGHT GREEN. SEE SECTIONS 603 AND 910 OF THE STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION. ALL NEW STEEL SHALL RECEIVE A SHOP ZINC PRIMER.

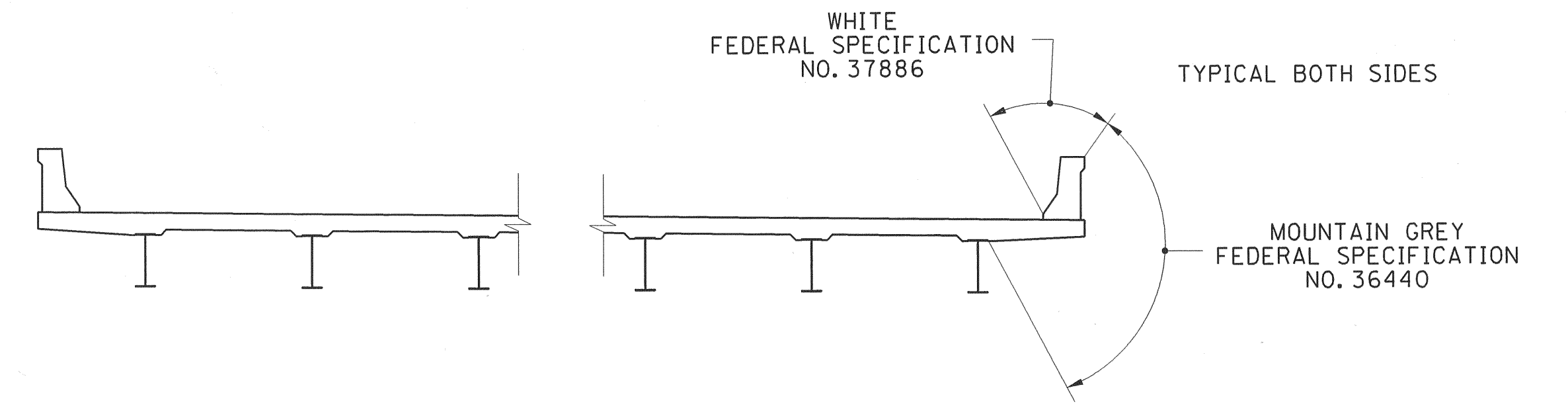
- NOTE: THE CONTRACTOR SHALL PROVIDE 100% CONVENTIONAL FALL PROTECTION FOR WORKERS INSTALLING DECKING ABOVE 15 FEET.
- ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- BRIDGE DECK FORMS: BRIDGE DECK FORMS FOR CONCRETE DECKS SHALL BE CONSTRUCTED USING EITHER REMOVABLE FORMS OR PERMANENT FORMS. PERMANENT FORMS MAY BE EITHER REMAIN-IN-PLACE STEEL OR PRECAST, PRESTRESSED CONCRETE PANELS. IN EITHER CASE, FORMS SHALL BE ATTACHED BY MEANS OTHER THAN WELDING TO MAIN STRUCTURAL MEMBERS OR REINFORCING STEEL. TEMPORARY ERECTION DIAPHRAGMS MUST BE PROVIDED IN ACCORDANCE WITH THE SPECIFICATIONS TO PREVENT GIRDER ROTATION. SEE STANDARD DRAWING STD-4-1 THRU 4 AND ARTICLE 604.05 OF THE STANDARD SPECIFICATIONS.
- FORMS AND FALSEWORK: CONCRETE FORM WORK, FALSEWORK, AND TEMPORARY SUPPORTS SHALL BE REMOVED AFTER REPAIRS ARE COMPLETED. COST OF FORMS, FALSEWORK, AND TEMPORARY SUPPORT REMOVAL SHALL BE INCLUDED IN ITEMS BID ON. THIS WORK SHALL BE COMPLETED BEFORE FINAL PAYMENT IS APPROVED.
- FINISHING CONCRETE SURFACES: CONCRETE FINISHING SHALL BE IN ACCORDANCE WITH SECTION 604.22 OF THE TENNESSEE STANDARD SPECIFICATION. A CLASS I FINISH FOLLOWED BY AN APPLIED TEXTURE FINISH SHALL BE USED IN LIEU OF A CLASS II FINISH. NO TEXTURE FINISH SHALL BE APPLIED PRIOR TO COMPLETION OF PAVING AND HAULING OPERATIONS AT THE BRIDGE SITE. THE APPLIED TEXTURE FINISH SHALL BE MEASURED AND PAID FOR UNDER ITEM NO. 604-04.02.
- FALSE WORK OVER TRAFFIC: SEE SECTION 604.06 OF THE STANDARD SPECIFICATIONS.
- BRIDGE RAIL SYSTEM: BUILD BRIDGE RAILINGS ACCORDING TO STANDARD DRAWING STD-1-1.
- NOTE: A MINIMUM OF 24 HRS. DRY TIME IS REQUIRED BETWEEN EACH COAT OF PAINT.



PROJECT NO.	YEAR	SHEET NO.	
80001-4178-04	2004	2A	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	11-16-04	TAK	UPDATED PAINT NOTES

REQUIREMENTS AND RESTRICTION FOR PHASE CONSTRUCTION

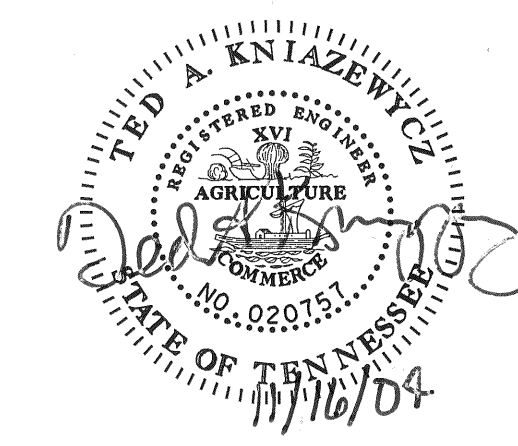
- 1) SEE ROADWAY TRAFFIC CONTROL SHEETS FOR OTHER RESTRICTIONS.
- 2) SEE SPECIAL PROVISION 108B FOR TRAFFIC CONTROL RESTRICTIONS AND PROJECT COMPLETION REQUIREMENTS.
- 3) THE LOCATION THE LONGITUDINAL CONSTRUCTION JOINT SHALL NOT BE CHANGED TO ACCOMMODATE REMAIN-IN-PLACE DECK FORMS.
- 4) NO SHEET PILING OF BEARING PILES MAY BE DRIVEN FROM THE EXISTING OF PROPOSED STRUCTURE.



DETAIL SHOWING LIMITS OF TEXTURE FINISH

- NOTE: IN ADDITION TO PORTIONS SHOWN IN THE TEXTURE FINISH SKETCH, THE FOLLOWING EXPOSED CONCRETE IS TO RECEIVE AN APPLIED TEXTURE FINISH: ABUTMENT BEAM AND WINGS AND ENTIRE BENT TO GROUND LINE.
- NOTE: COST OF TEXTURE COATING SHALL BE INCLUDED IN ITEM NO. 604-04.02. ALSO INCLUDES EXPOSED SURFACES OF BENTS, WING WALLS AND ABUTMENTS.
- NOTE: BEFORE APPLYING ANY TEXTURE FINISH, ALL SURFACES SHALL BE COMPLETELY CLEANED OF ALL DEBRIS AND FOREIGN MATERIALS.
- NOTE: THE CONTRACTOR SHALL USE CONTAINMENT SCREENS OR OTHER MEASURES AS NECESSARY TO PREVENT ANY TEXTURE COATING FROM ENTERING THE ENVIRONMENT. CONTAINMENT MEASURES SHALL BE APPROVED BY THE ENGINEER AND COST SHALL BE INCLUDED IN ITEMS BID ON.
- NOTE: CONCRETE FINISHING SHALL BE IN ACCORDANCE WITH SECTION 604.22 OF THE TENNESSEE STANDARD SPECIFICATION. A CLASS I FINISH FOLLOWED BY AN APPLIED TEXTURE FINISH SHALL BE USED IN LIEU OF A CLASS II FINISH. THE COLOR OF THE FINISH SHALL BE SIMILAR TO MOUNTAIN GRAY, FEDERAL SPECIFICATION NO. 36440, FEDERAL COLOR STANDARD NO. 595A, EXCEPT THAT THE INSIDE FACE AND TOP OF THE PARAPET SHALL BE WHITE, FEDERAL SPECIFICATION NO. 37886. A COLOR SAMPLE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL. NO TEXTURE FINISH SHALL BE APPLIED PRIOR TO COMPLETION OF PAVING AND HAUL OPERATIONS AT THE BRIDGE SITE. THE APPLIED TEXTURE FINISH SHALL BE MEASURED AND PAID FOR UNDER ITEM 604-04.02.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION



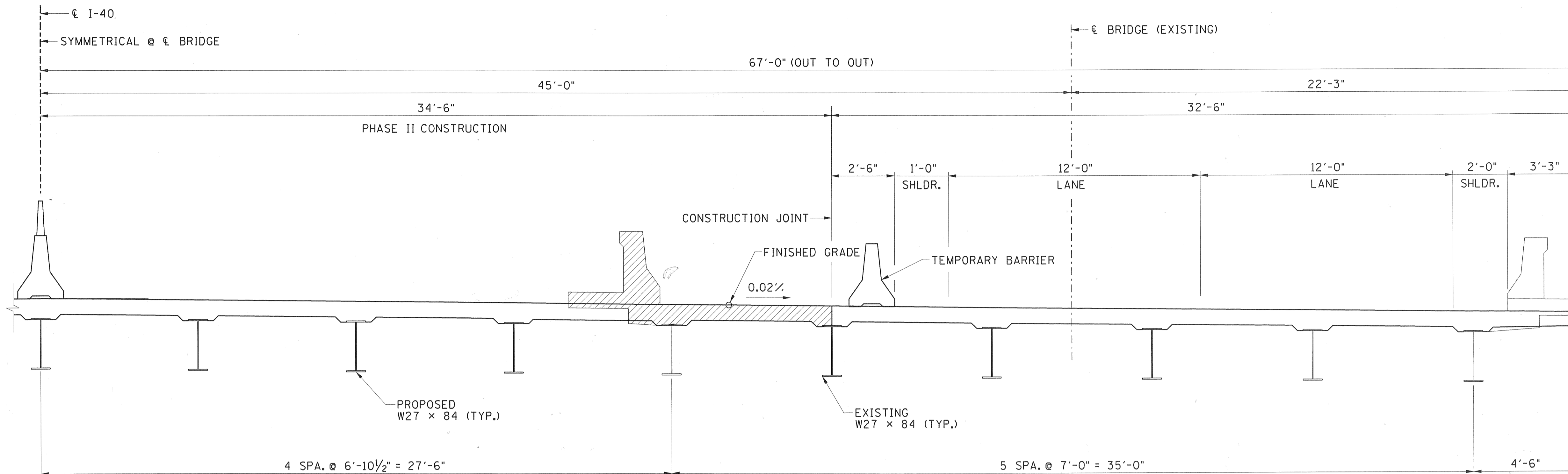
GENERAL NOTES
I-40 OVER S.R. 53
STA. 48+65.68
BRIDGE NO. 80-I40-8.25 (LL & RL)
SMITH COUNTY
2004

DESIGNED BY GSP-KTC DATE 05/04
DRAWN BY KTC DATE 05/04
SUPERVISED BY TAK DATE 05/04
CHECKED BY TAK DATE 05/04

GRESHAM SMITH AND PARTNERS

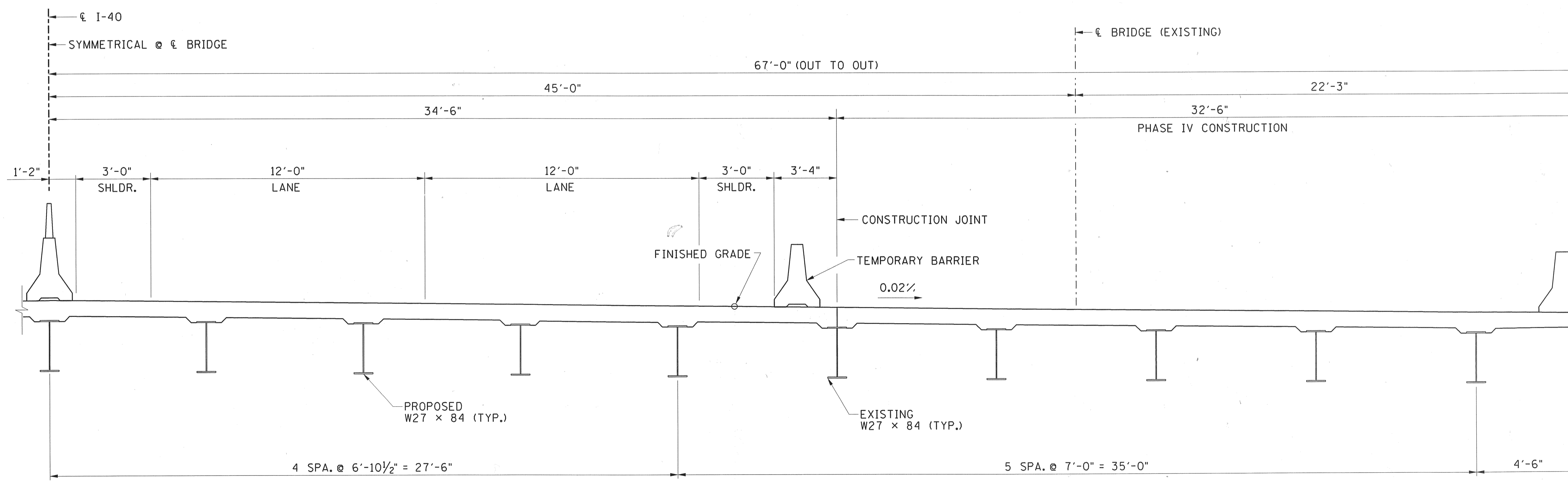
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PROJECT NO.	YEAR	SHEET NO.	
80001-4178-04	2004		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



PHASE II CONSTRUCTION

SCALE: 3/8" = 1'-0"



PHASE IV CONSTRUCTION

SCALE: 3/8" = 1'-0"



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

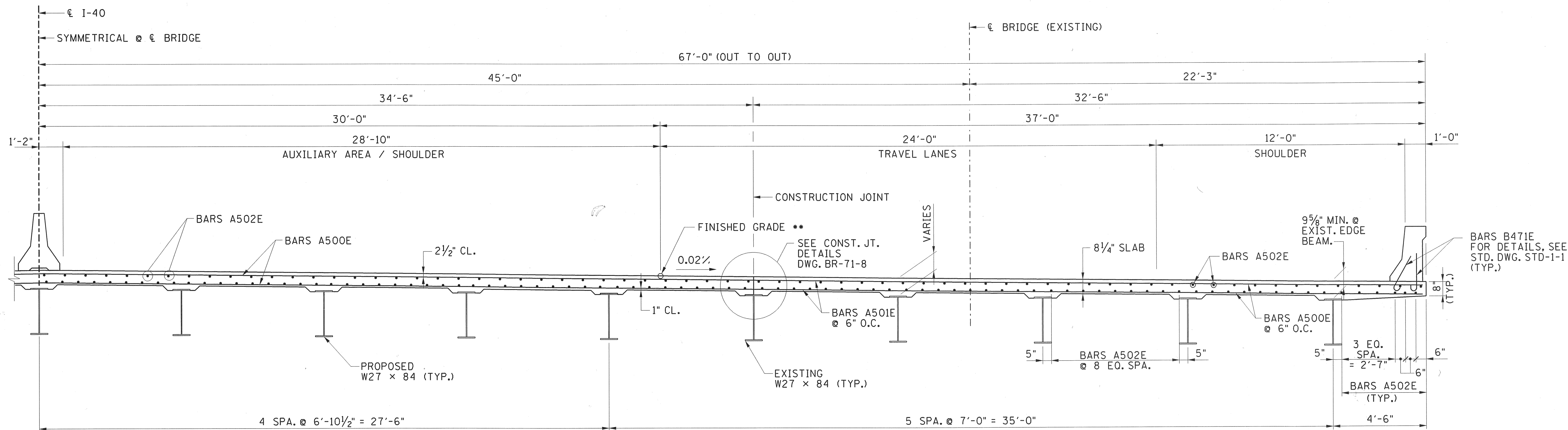
PHASE CONSTRUCTION
I-40 OVER S.R. 53
STA. 48+65.68
BRIDGE NO. 80-I40-8.25 (LL @ RL)
SMITH COUNTY
2004

DESIGNED BY GSP-KTC DATE 05/04
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TYPICAL CROSS SECTION @ MIDSPAN

(FOR REINFORCING BAR SPACING SEE PLAN DWG. NO. B-71-9A)
SCALE: 3/8" = 1'-0"

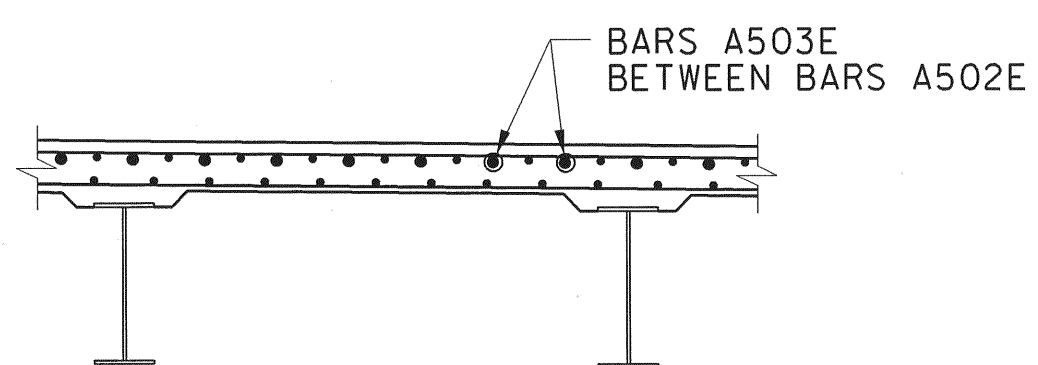
**NOTE: PROPOSED FINISHED GRADE IS BASED ON EXISTING GRADE PLUS 0.53 FEET. RATE OF PROPOSED CROSS SLOPE IS DIFFERENT THAN EXISTING CROSS SLOPE.

NOTES

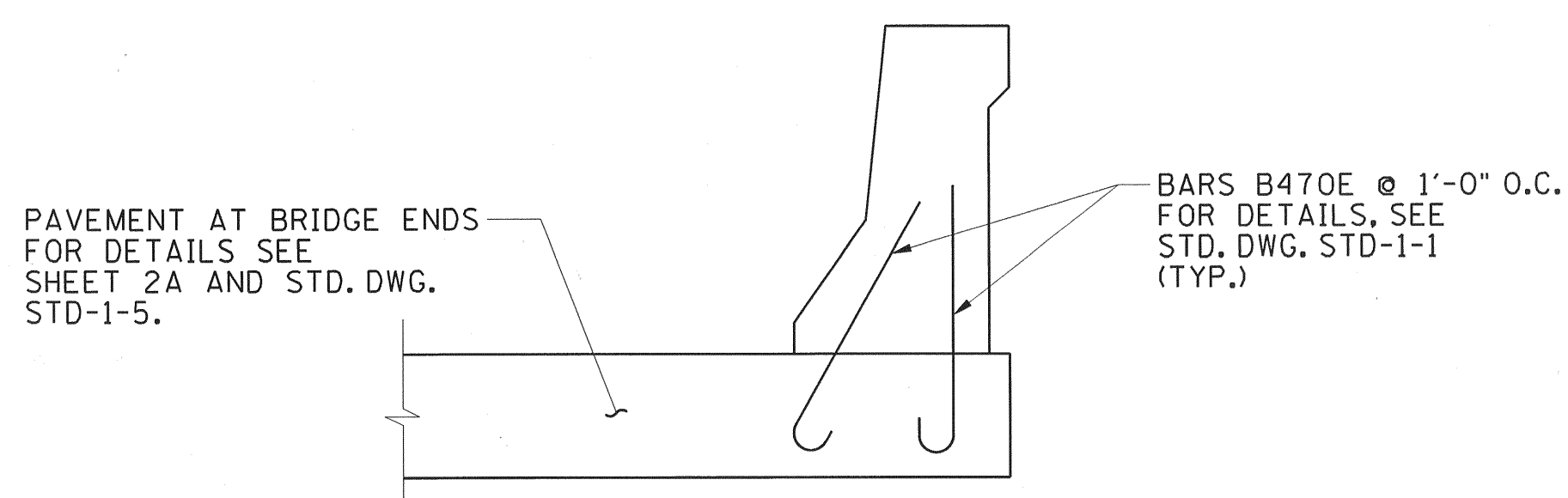
1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING SLAB REPLACEMENT. DESIGN CALCULATION AND DETAILS OF TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE ANY DEMOLITION IS BEGUN. COST OF STABILIZING THE STRUCTURE SHALL BE INCLUDED IN ITEM NO. 602-10.05, BRACING REPAIRS, L.S.
2. FOR STANDARD REINFORCING DETAILS OF PARAPET SEE STD-1-1.
3. WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR THE PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.
4. THE COST OF CONCRETE REQUIRED FOR THE NEW BRIDGE DECK SLAB AND GIRDER ENCASEMENT AT ABUTMENTS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-03.09 CLASS "D" CONCRETE (BRIDGE DECK), C.Y.
5. ALL REINFORCING STEEL FOR THE NEW CONCRETE SLAB AT SHALL BE EPOXY COATED. REINFORCING STEEL TO BE INCLUDED IN THE PRICE BID FOR ITEM NO. 604-02.03 EPOXY COATED REINFORCING STEEL, LBS.
6. THE COST OF REMOVING AND DISPOSING OF THE EXISTING CONCRETE SLAB, WITHIN THE LIMITS SHOWN SHALL BE INCLUDED IN ITEM NO. 202-04.01. REMOVAL OF STRUCTURES, L.S.

PART TYPICAL CROSS SECTION @ BENTS

(FOR REINFORCING BAR SPACING SEE PLAN DWG. NO. B-71-9A)
SCALE: 3/8" = 1'-0"

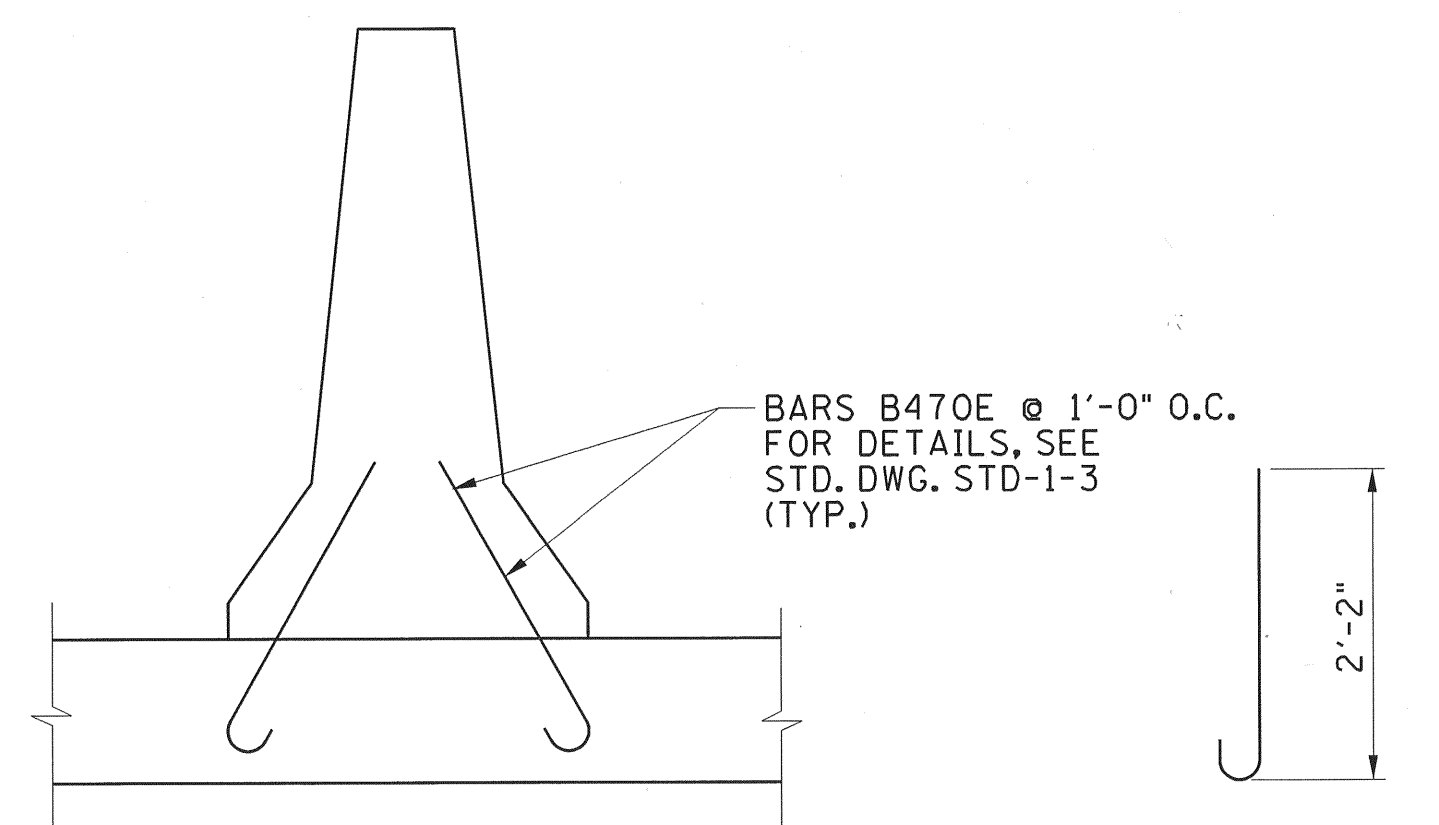


NOTE: WHEN POURING P.A.B.E., PROVISIONS SHALL BE MADE FOR SETTING PARAPET REINFORCING.



ENDPOST ATTACHMENT DETAIL AT P.A.B.E.

SCALE: 3/4" = 1'-0"



BARS B470E
QUANTITY = 714

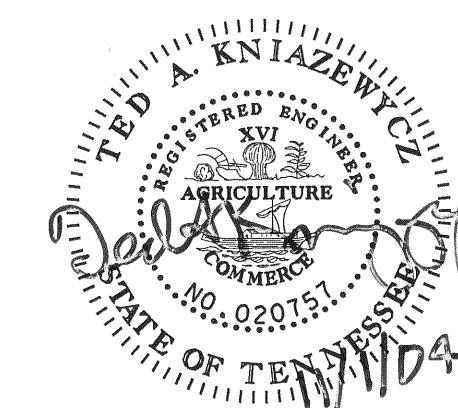
PERMANENT MEDIAN BARRIER

SCALE: 3/4" = 1'-0"

ESTIMATED QUANTITIES

CLASS "D" CONCRETE (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL (BRIDGES) LB.
420	102,313

NOTE: CONCRETE FOR GIRDER ENCASEMENT AT ABUTMENTS INCLUDED ABOVE. SEE SHEET BR-71-12 FOR DETAILS.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE
I-40 OVER S.R. 53
STA. 48+65.68
BRIDGE NO. 80-140-8.25 (LL & RL)
SMITH COUNTY
2004

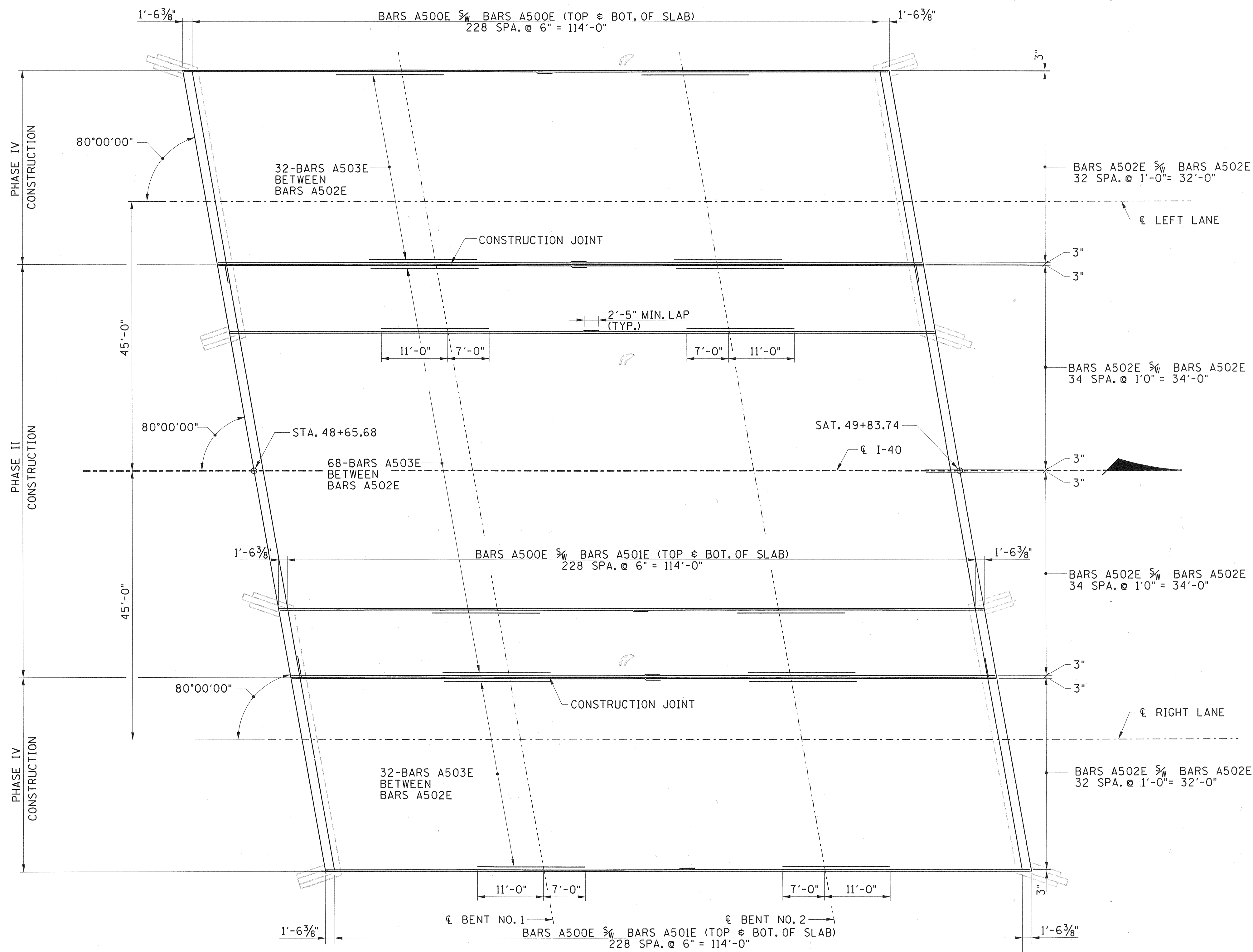
DESIGNED BY GSP-KTC
DRAWN BY TAK
SUPERVISED BY TAK
CHECKED BY TAK



GRESHAM
SMITH AND
PARTNERS

SCANNED

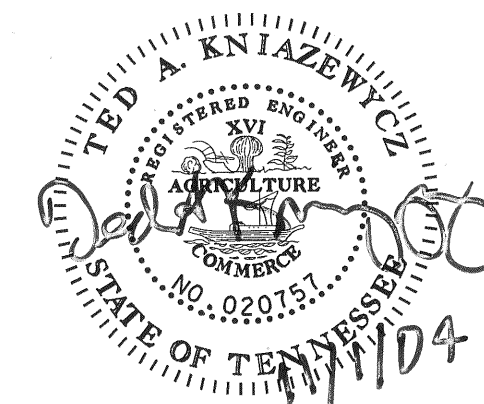
PROJECT NO.	YEAR	SHEET NO.	
80001-4178-04	2004		
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PLAN

SCALE: 1'-0"=10'-0"

NOTE: CONTRACTOR MAY PUT OPTIONAL LONGITUDINAL JOINT UNDER CENTER OF PERMANENT MEDIAN BARRIER.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS
(1 OF 2)

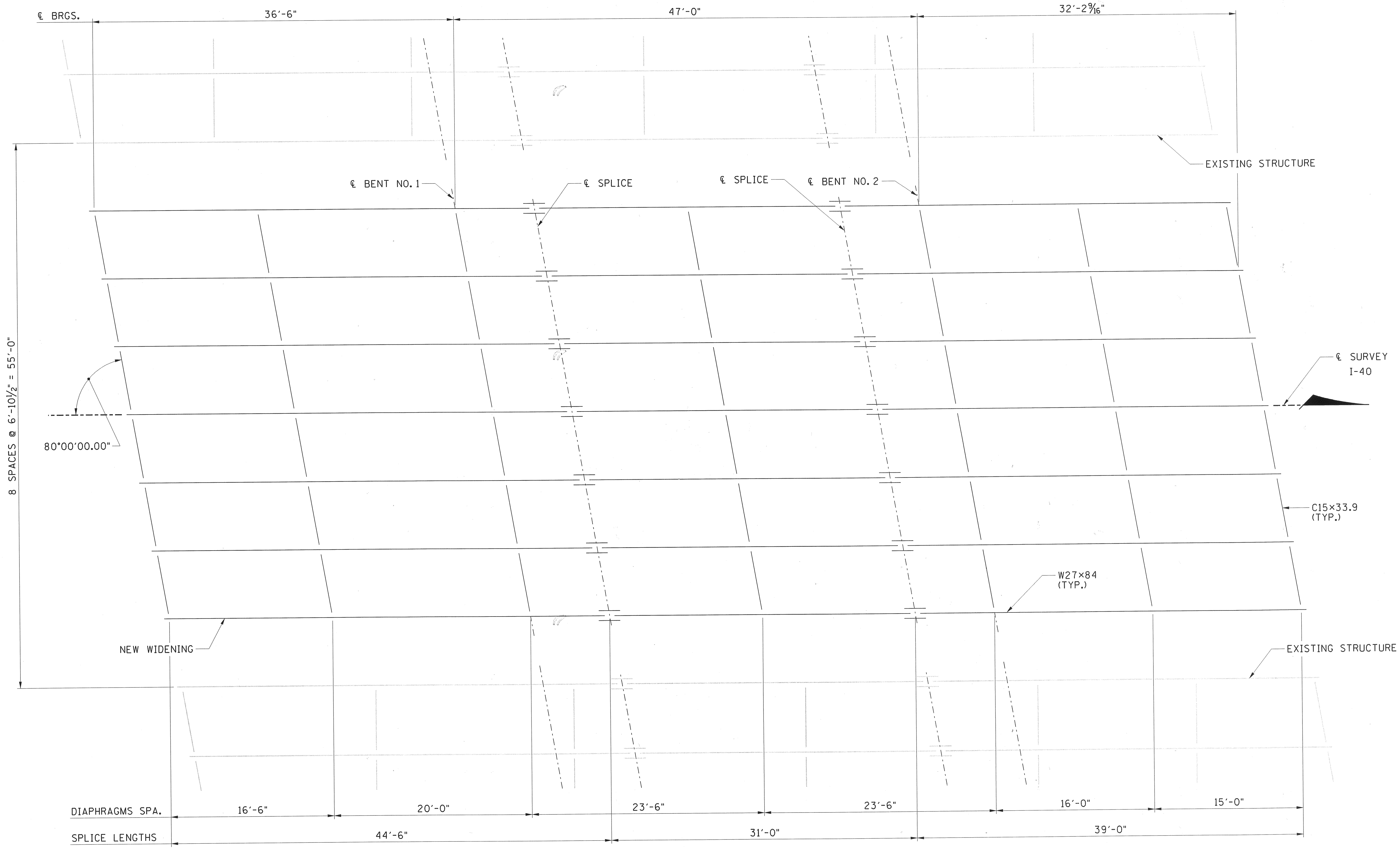
I-40 OVER S.R. 53
STA. 48+65.68
BRIDGE NO. 80-I40-8.25 (LL & RL)
SMITH COUNTY
2004

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GRESHAM
SMITH AND
PARTNERS

PROJECT NO.	YEAR	SHEET NO.	
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FRAMING PLAN
SCALE: 1/8" = 1'-0"

SUPERSTRUCTURE GENERAL NOTES

NOTE: THE CONCRETE DECK SHALL NOT BE POURED UNTIL ALL STRUCTURAL STEEL IS ERECTED AND ALL WELDING AND/OR BOLTING COMPLETE.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB (PER PHASE) IS IN PLACE AND CURED.

APPROVAL OF MATERIALS: NO FABRICATION SHALL BE STARTED UNTIL THE MATERIALS INVOLVED HAVE BEEN APPROVED BY THE TENNESSEE DEPARTMENT OF TRANSPORTATION, DIVISION OF MATERIALS AND TESTS.

IDENTITY OF MAIN MATERIALS: SEE SECTION 602 OF THE STANDARD SPECIFICATIONS.

STRUCTURAL STEEL: SHALL CONFORM TO ASTM 709 GRADE 50 UNLESS OTHERWISE NOTED. ALL STRUCTURAL STEEL FOR GIRDERS SHALL MEET THE SUPPLEMENTAL REQUIREMENTS FOR LONGITUDINAL CHARPY V-NOTCH TEST SPECIFIED IN THE ASTM SPECIFICATIONS. ZONE 2 OF NON-FRACTURE CRITICAL CRITERIA SHALL APPLY.

WELDING: ANSI/AASHTO/AWS D1.5-2002 BRIDGE WELDING CODE AND SECTION 602 OF THE STANDARD SPECIFICATIONS.

FIELD CONNECTIONS: SHALL BE 7/8" DIAMETER HIGH TENSILE STRENGTH BOLTS ASTM-A325 UNLESS OTHERWISE SHOWN. SEE AASHTO SPECIFICATIONS ART. 11.3.2 DIVISION II AND SECTION 602 OF THE STANDARD SPECIFICATIONS.

SHOP ASSEMBLY: PROGRESSIVE SHOP ASSEMBLY FOR FRAMING WILL BE ALLOWED. SEE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, ART. 11.5.3.1 DIVISION II.

STEEL BEAM DESIGN DATA (PER BEAM):
LIVE LOAD DISTRIBUTION FACTOR = 1.14 WHEELS
COMPOSITE DEAD LOAD = 300 LBS/FT
COMPOSITE SLAB DESIGN STRENGTH = 3,000 PSI.


PAINTING STRUCTURAL STEEL: ALL SURFACES OF STRUCTURAL STEEL GIRDERS MUST BE PAINTED. HOWEVER, THE FAYING SURFACES OF FIELD SPLICE, THE TOP OF THE TOP FLANGE, SURFACES EMBEDDED IN CONCRETE, AND BEARING AREAS SHALL RECEIVE ONLY 1 MIL THICKNESS OF SHOP COAT. BOLTED FAYING SURFACES FOR THIS PROJECT ARE DESIGNED FOR A CLASS B SURFACE. (SEE AASHTO TABLE 10.32.3C). THE CONTRACTOR SHALL INSURE THE FAYING SURFACES ARE PREPARED TO PROVIDE A SLIP COEFFICIENT AT LEAST EQUAL TO THAT REQUIRED FOR THIS CLASS. THE FAYING SURFACES ARE TO BE PAINTED, AS NOTED ABOVE, AND THE CONTRACTOR SHALL SUPPLY A PAINT MANUFACTURE'S CERTIFICATION THAT THE PAINT TO USED WILL PROVIDE THE REQUIRED RESISTANCE AT THE PROPOSED THICKNESS WHEN TESTED IN ACCORDANCE WITH AASHTO 10.32.3.2.3.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION



SUPERSTRUCTURE DETAILS
(2 OF 2)
I-40 OVER S.R. 53
STA. 48+65.68
BRIDGE NO. 80-140-8.25 (LL & RL)
SMITH COUNTY
2004

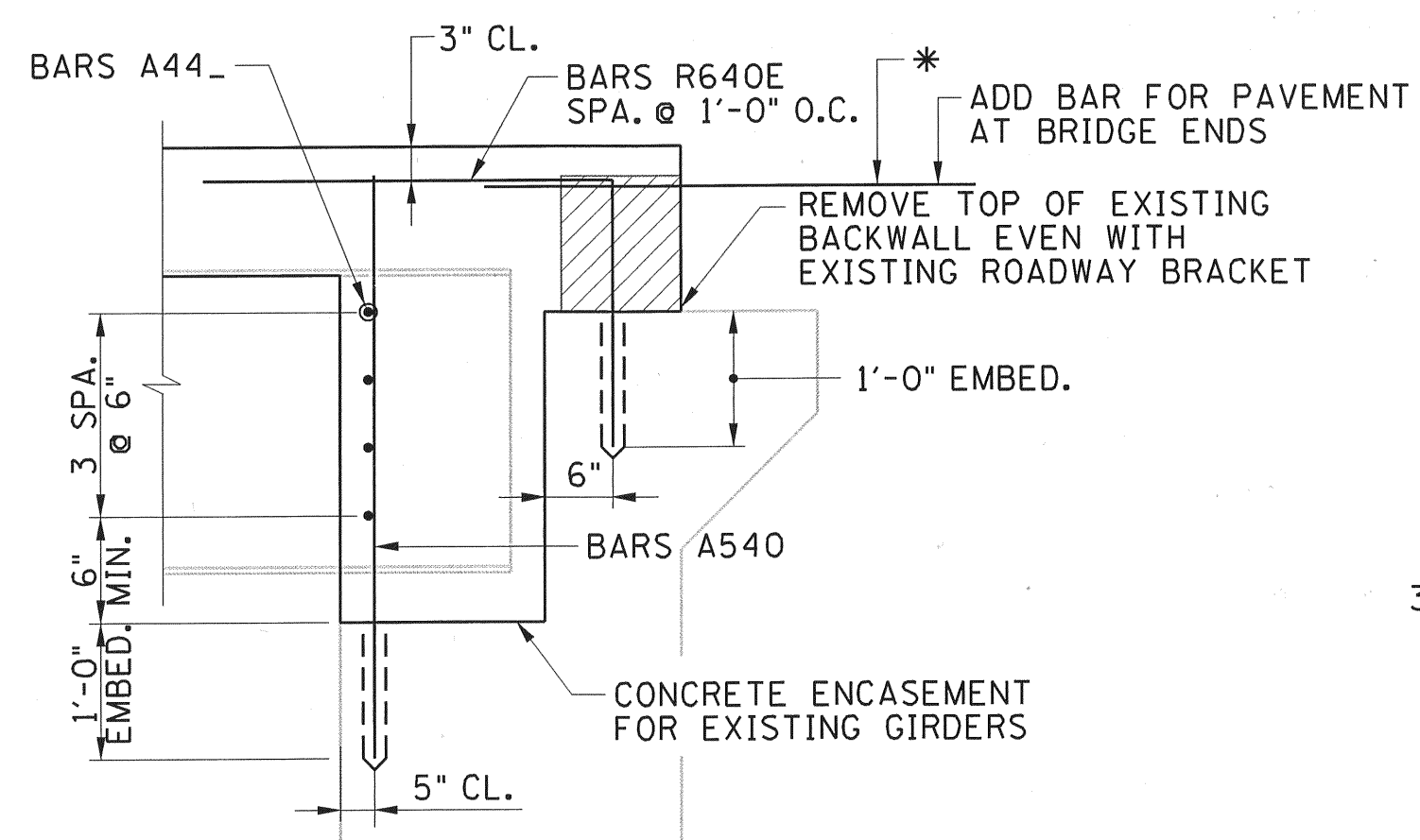
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DRAWN BY		DATE	05/04
SUPERVISED BY	TAK	DATE	05/04
CHECKED BY	TAK	DATE	05/04



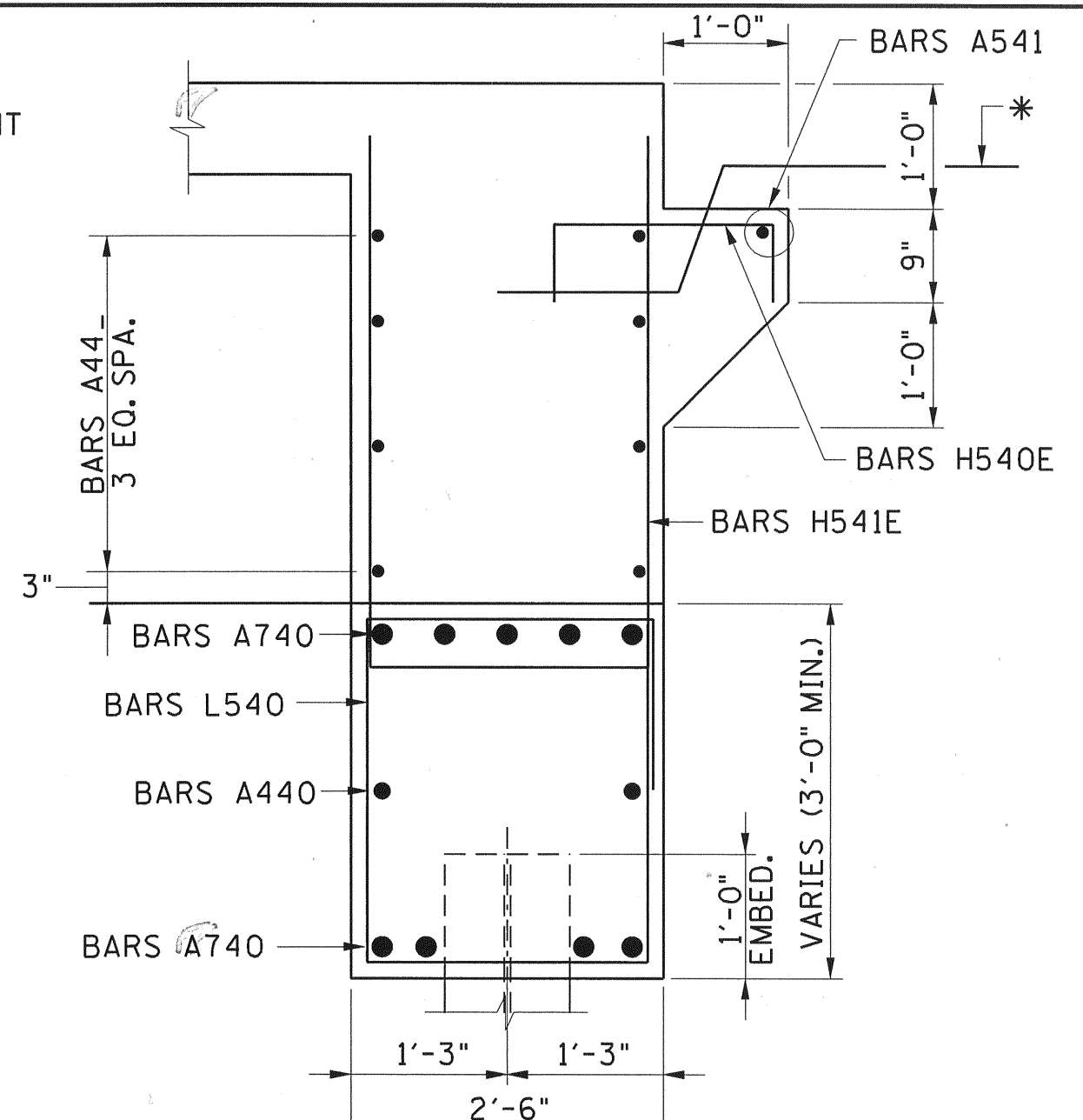
**GRESHAM
SMITH AND
PARTNERS**

SCANNED

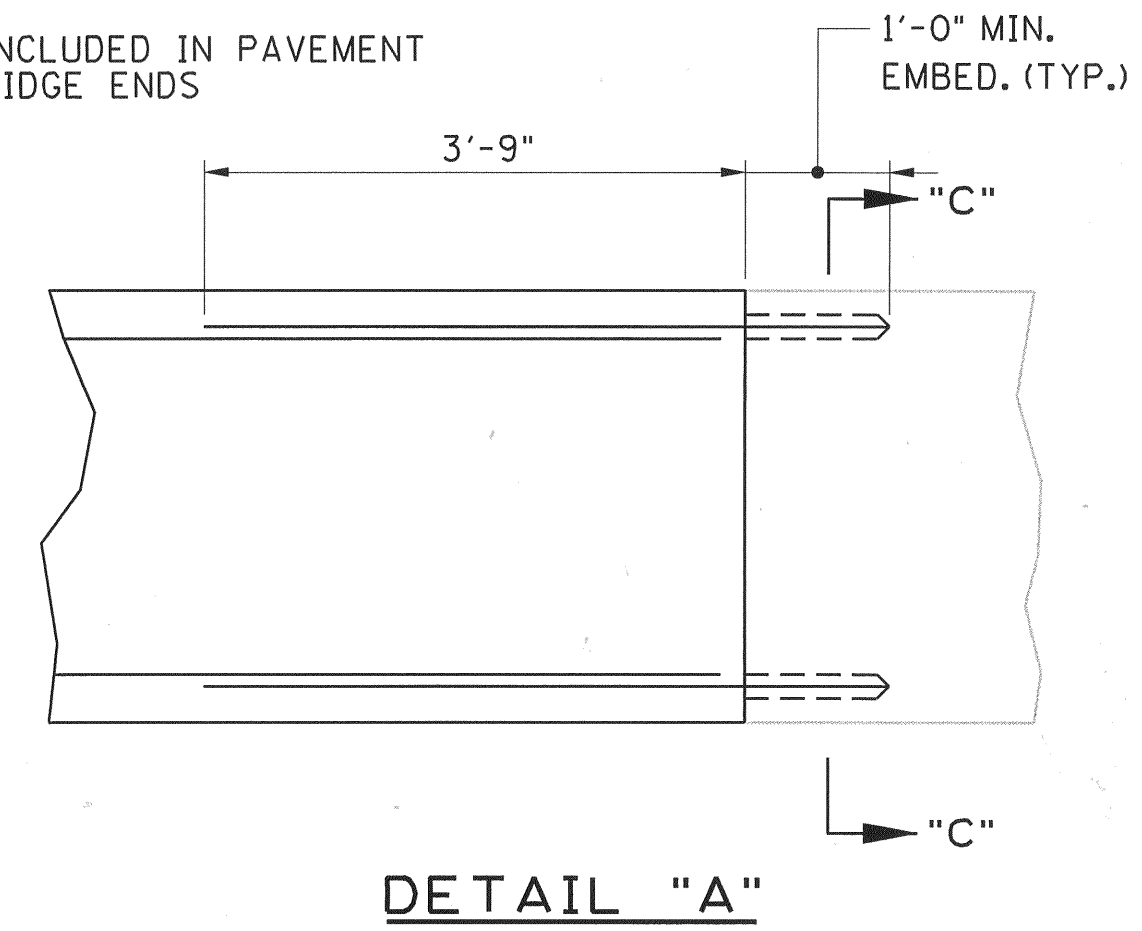
PROJECT NO.	YEAR	SHEET NO.	
80001-4178-04	2004		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	5/20/05	JAC	REVISED ELEVATIONS



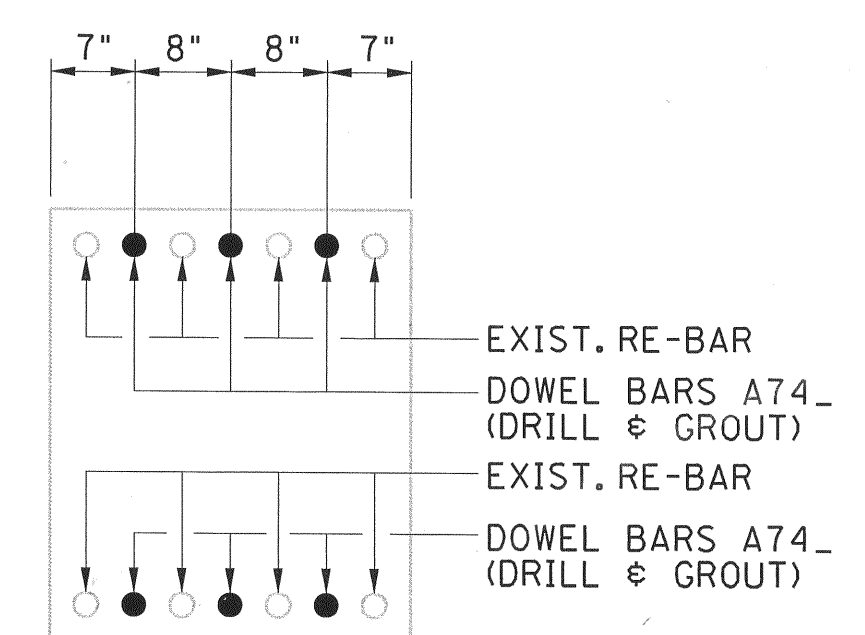
SECTION "A-A"
SCALE: 3/4" = 1'-0"



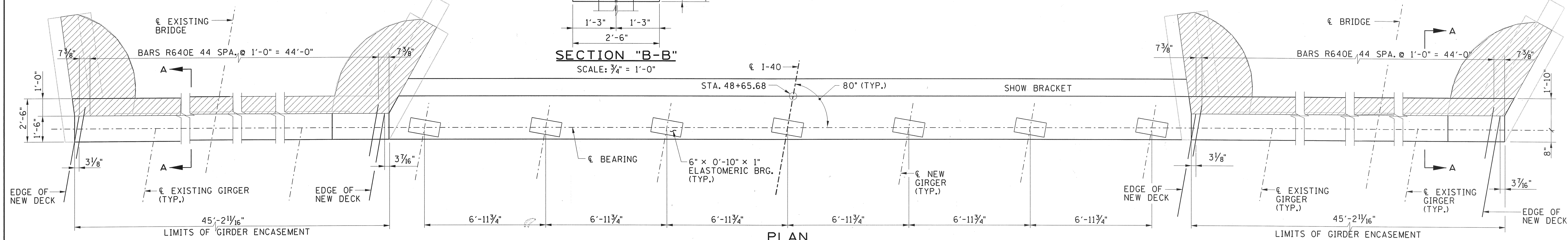
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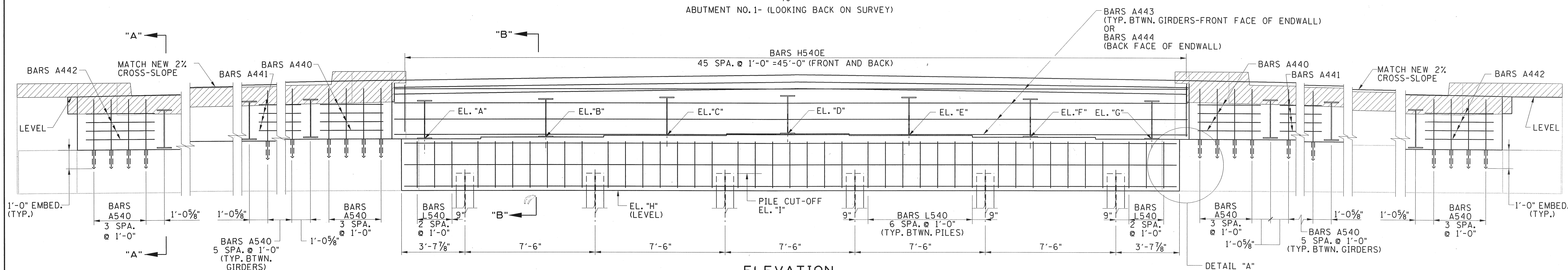
DETAIL "A"



SECTION "C"



PLAN
SCALE: 3/8" = 1'-0"
ABUTMENT NO. 1- (LOOKING BACK ON SURVEY)



ELEVATION
SCALE: 3/8" = 1'-0"

NOTE: SEE STD DWG SBR-2-125 FOR DETAILS OF BRIDGE RAIL TRANSITION AND ENDPOST.

NOTE: TOP OF BACKWALL TO CONFORM TO EXISTING CROSS-SLOPE AND MATCH NEW FINISHED GRADE OF SLAB FOR ENTIRE LENGTH. TOP OF WINWALLS SHALL BE REMOVED TO AN ELEVATION MATCHING FINISHED GRADE AT THE ABUTMENT BACKWALL.

NOTE: COST OF DRILL/GROUT TO BE INCLUDED IN ITEMS BID ON.

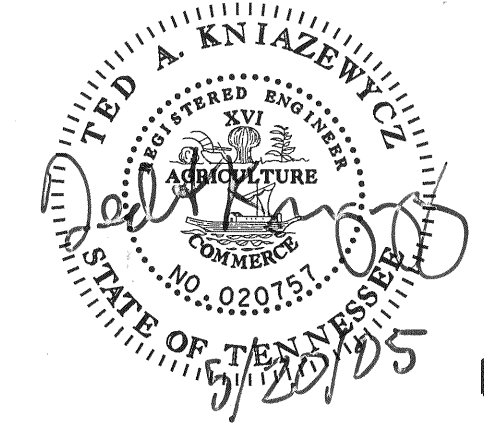
▨ DENOTES: AREA TO BE REMOVED.

TABLE OF ELEVATIONS

ELEV. "A"	ELEV. "B"	ELEV. "C"	ELEV. "D"	ELEV. "E"	ELEV. "F"	ELEV. "G"	ELEV. "H"	ELEV. "I"
573.90	574.03	574.16	574.29	574.15	574.01	573.86	570.86	571.86

ESTIMATED QUANTITIES

CONCRETE (BRIDGES) C.Y.	EPOXY COATED REINFORCING STEEL (BRIDGES) LB.	STEEL BAR REINFORCEMENT (BRIDGES) LB.
40	1135	1819

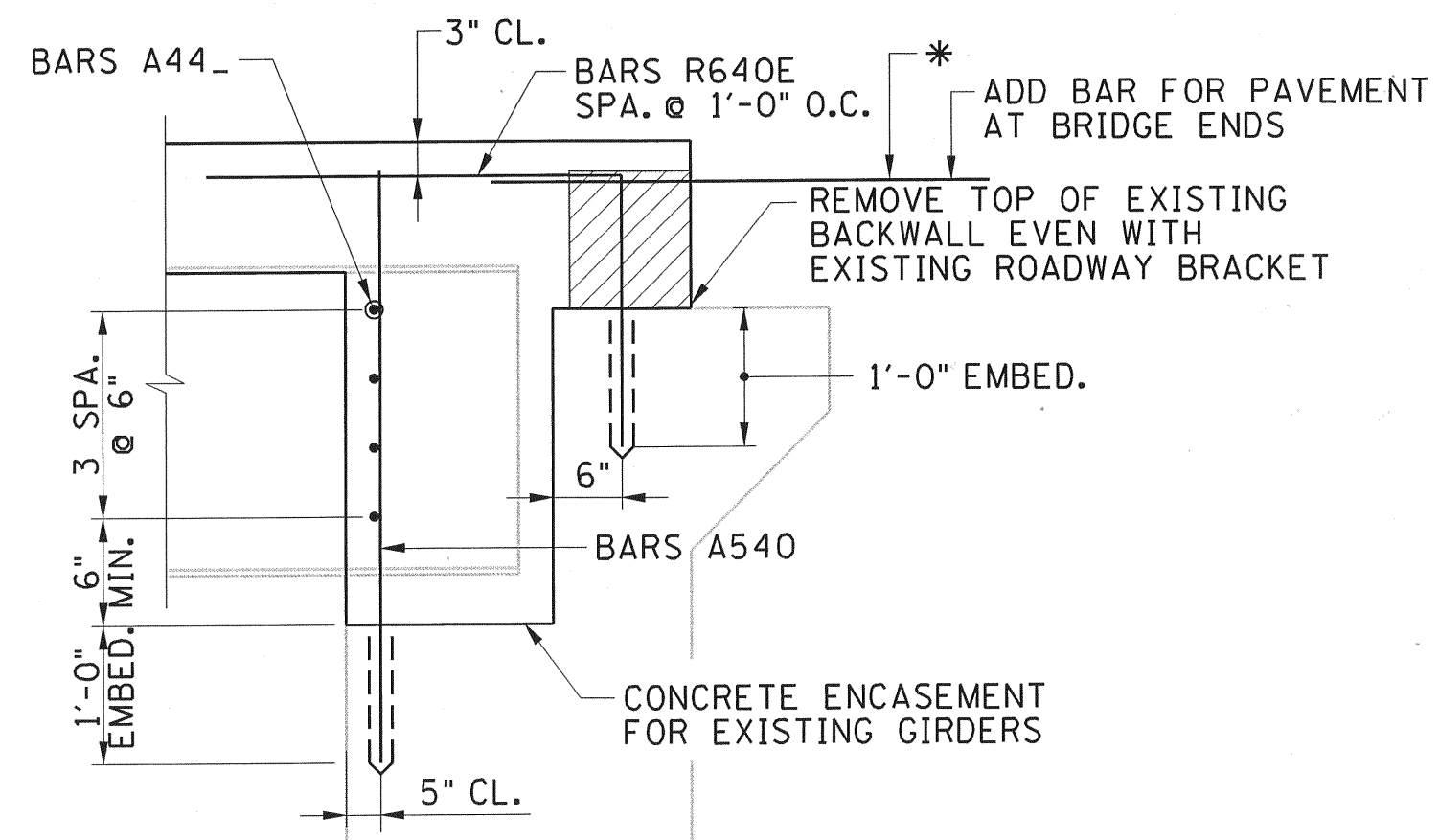


STATE OF TENNESSEE
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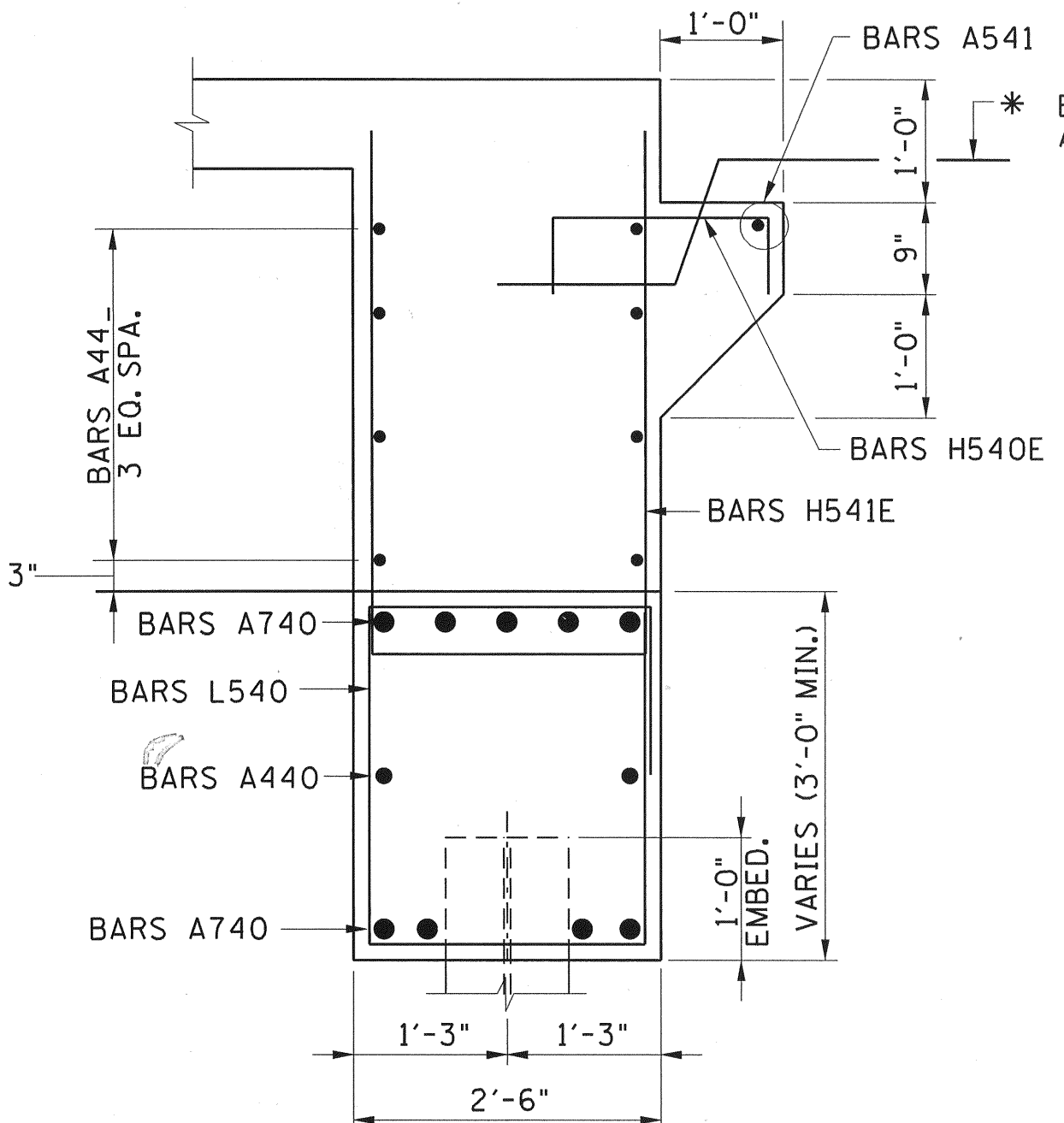
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GRESHAM SMITH AND PARTNERS

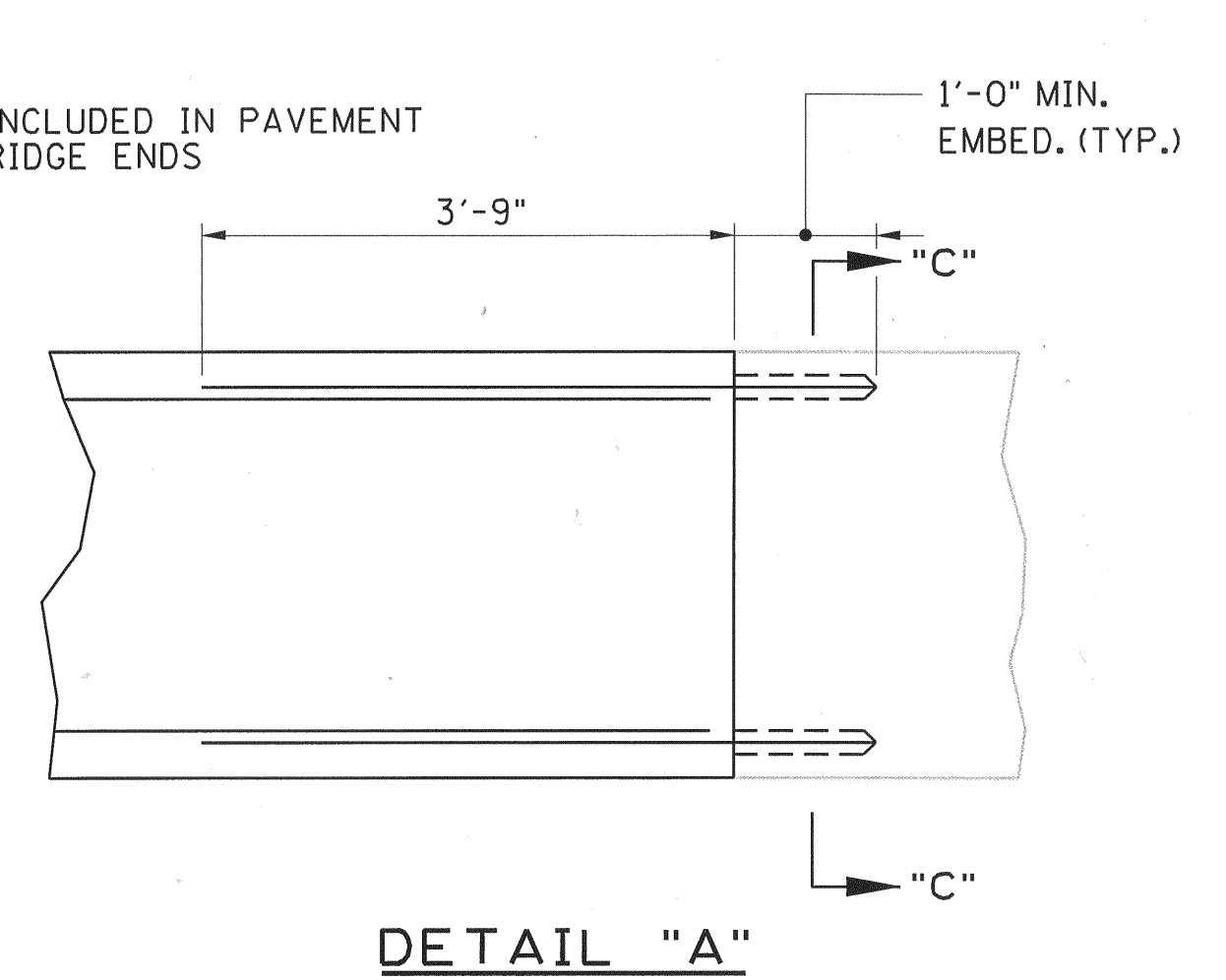
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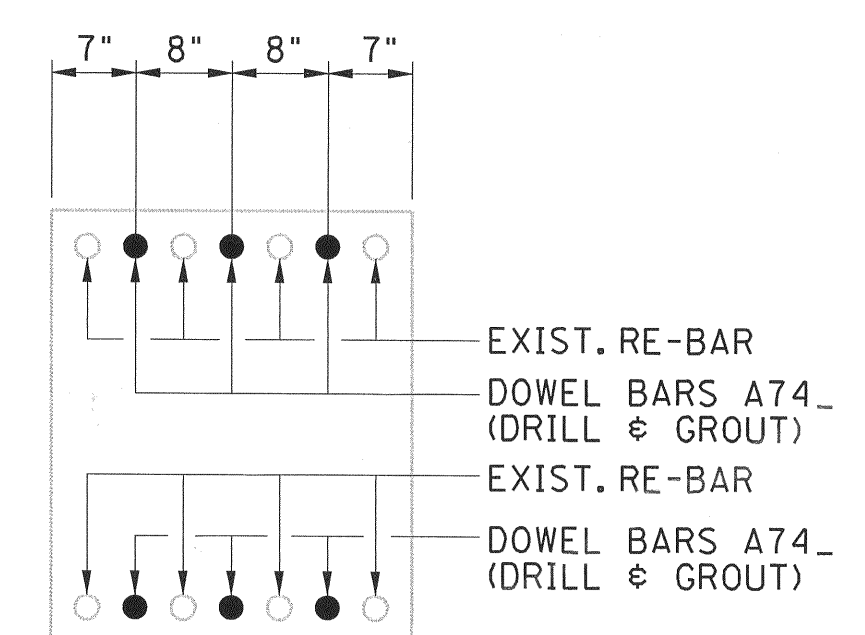
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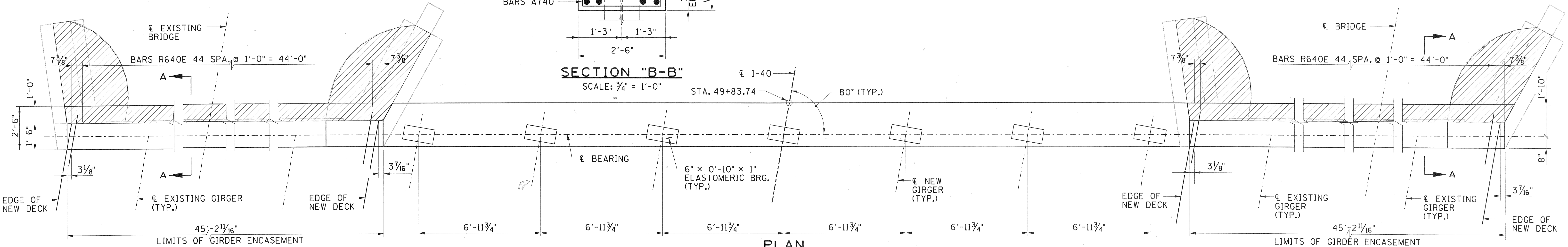
SECTION "B-B"
SCALE: 3/4" = 1'-0"



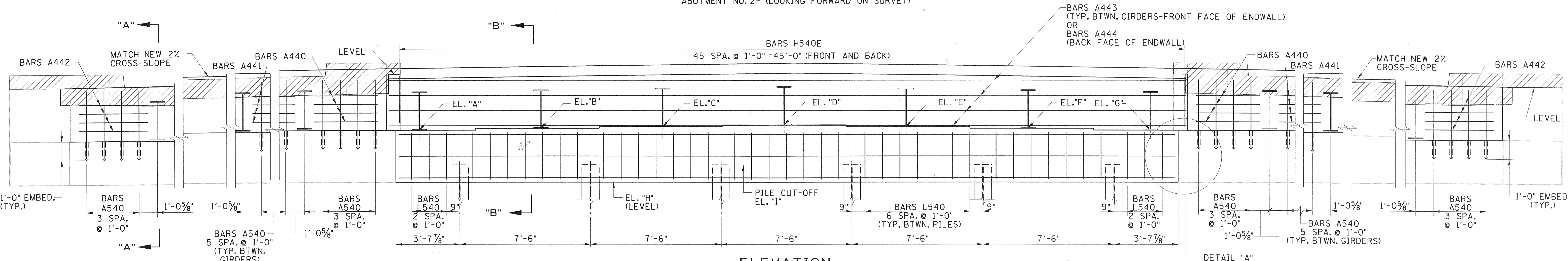
DETAIL "A"



SECTION "C"



PLAN
SCALE: 3/8" = 1'-0"
ABUTMENT NO. 2- (LOOKING FORWARD ON SURVEY)



ELEVATION
SCALE: 3/8" = 1'-0"

NOTE: SEE STD DWG SBR-2-125 FOR DETAILS OF BRIDGE RAIL TRANSITION AND ENDPOST.

NOTE: TOP OF BACKWALL TO CONFORM TO EXISTING CROSS-SLOPE AND MATCH NEW FINISHED GRADE OF SLAB FOR ENTIRE LENGTH. TOP OF WINWALLS SHALL BE REMOVED TO AN ELEVATION MATCHING FINISHED GRADE AT THE ABUTMENT BACKWALL.

NOTE: COST OF DRILL/GROUT TO BE INCLUDED IN ITEMS BID ON.

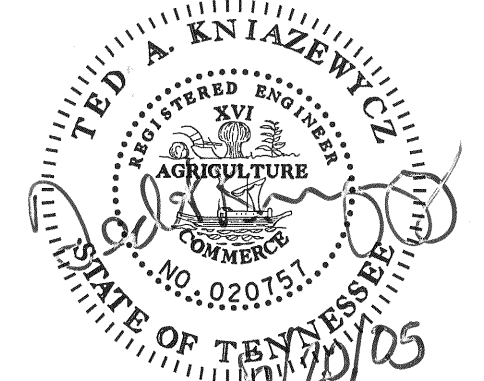
███ DENOTES: AREA TO BE REMOVED.

TABLE OF ELEVATIONS

ELEV. "A"	ELEV. "B"	ELEV. "C"	ELEV. "D"	ELEV. "E"	ELEV. "F"	ELEV. "G"	ELEV. "H"	ELEV. "I"
574.44	574.58	574.72	574.87	574.74	574.60	574.47	571.47	572.47

ESTIMATED QUANTITIES

CONCRETE (BRIDGES) C.Y.	EPOXY COATED REINFORCING STEEL (BRIDGES) LB.	STEEL BAR REINFORCEMENT (BRIDGES) LB.
40	1135	1819

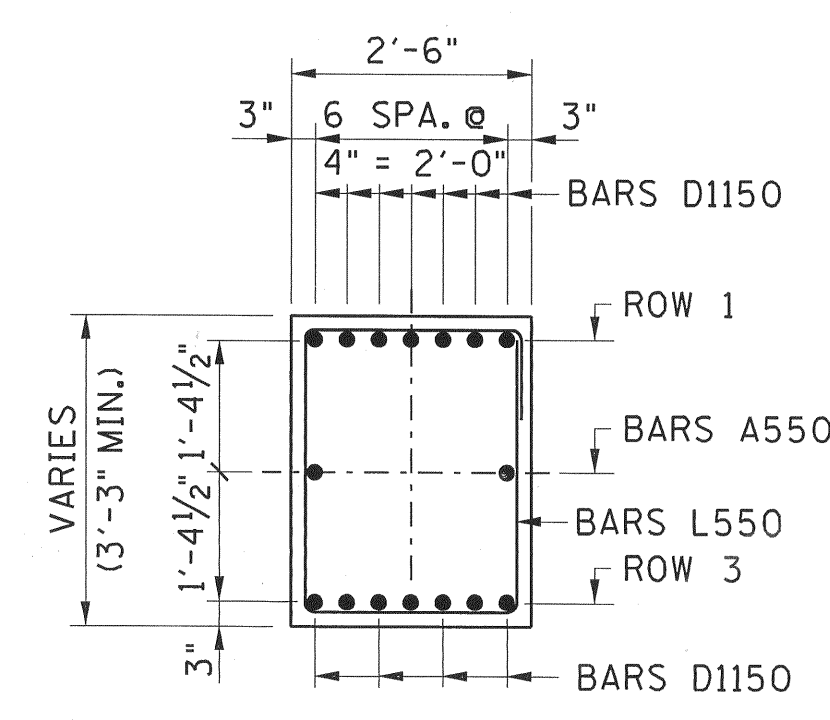
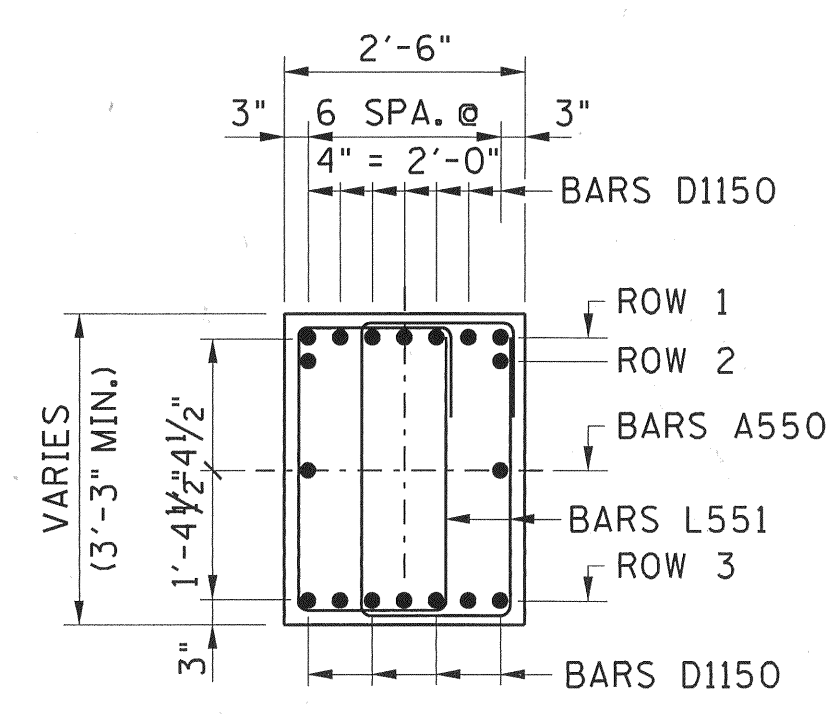
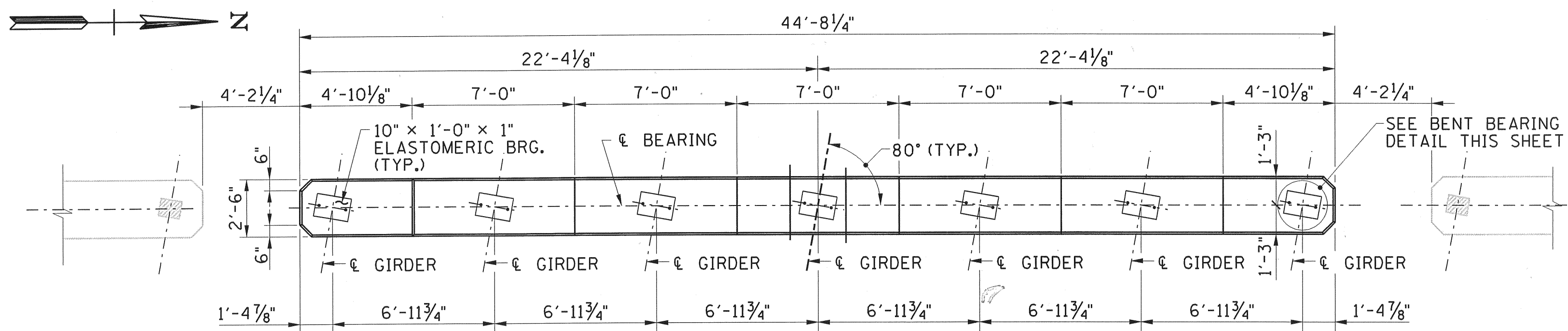


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

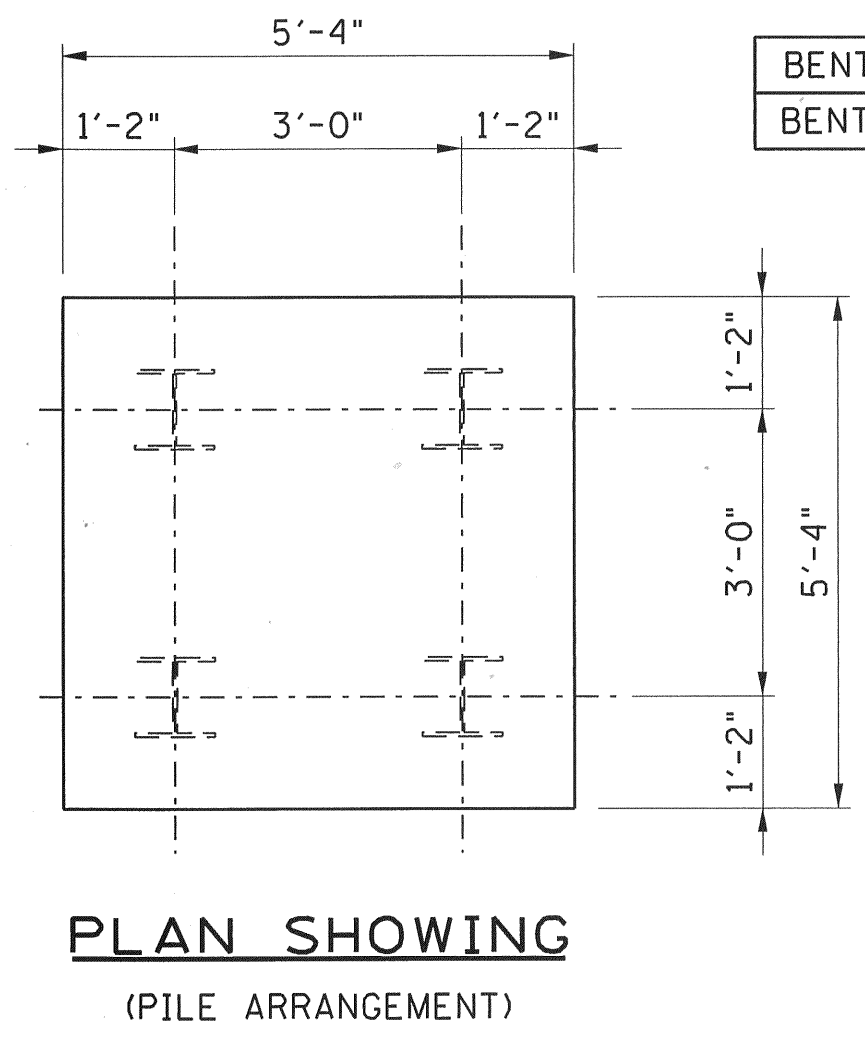
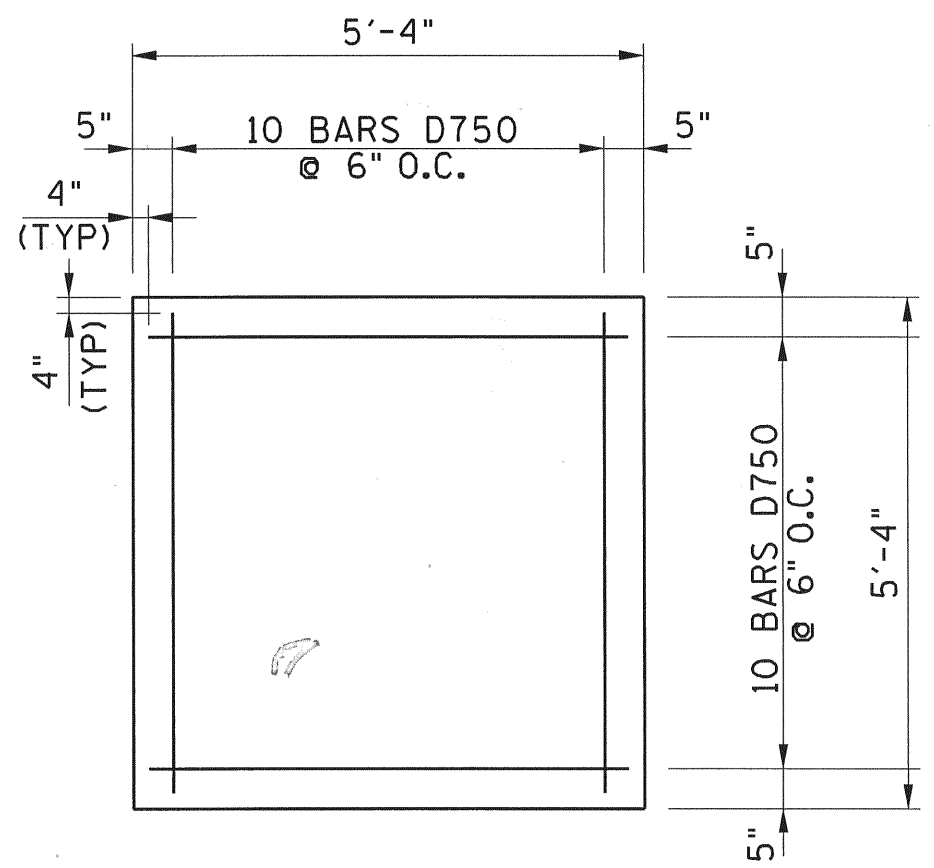
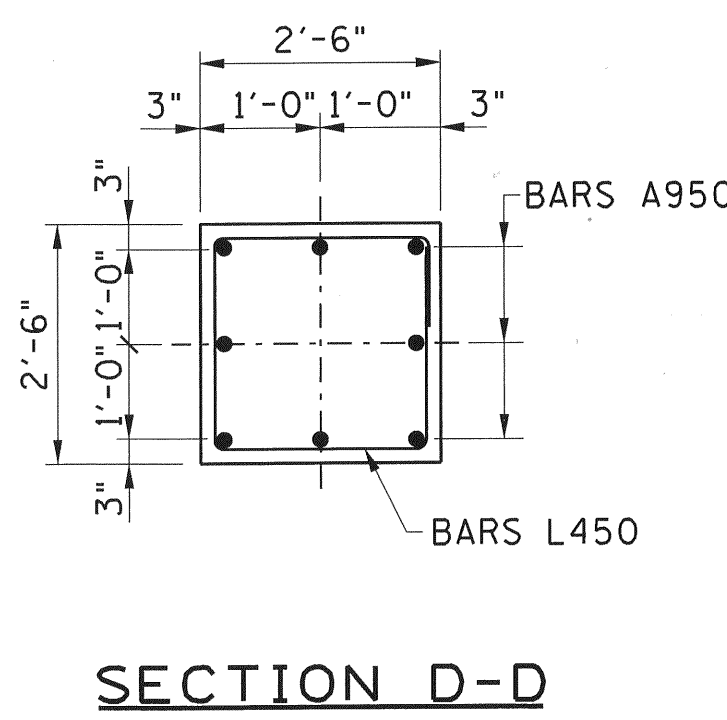
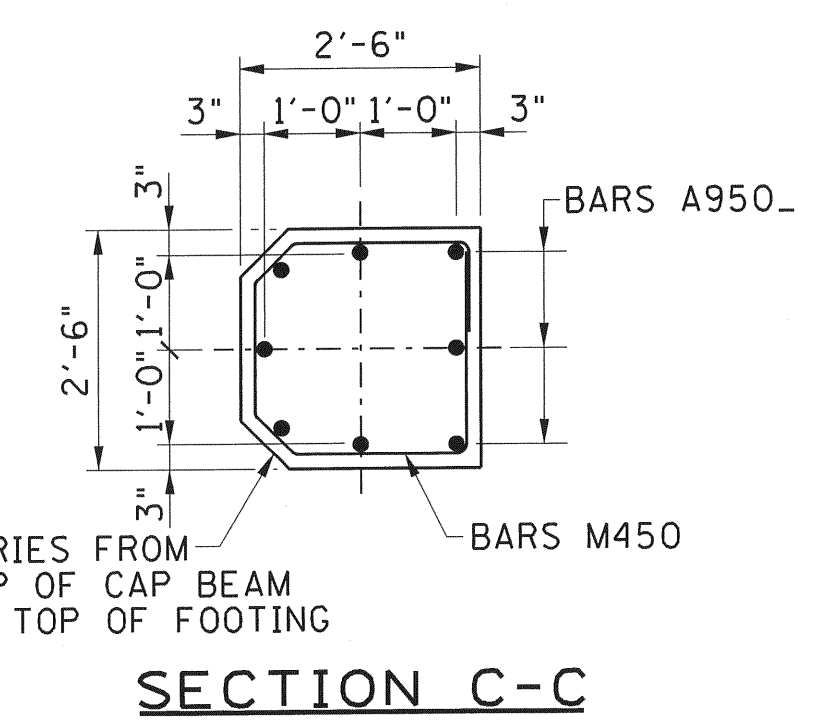
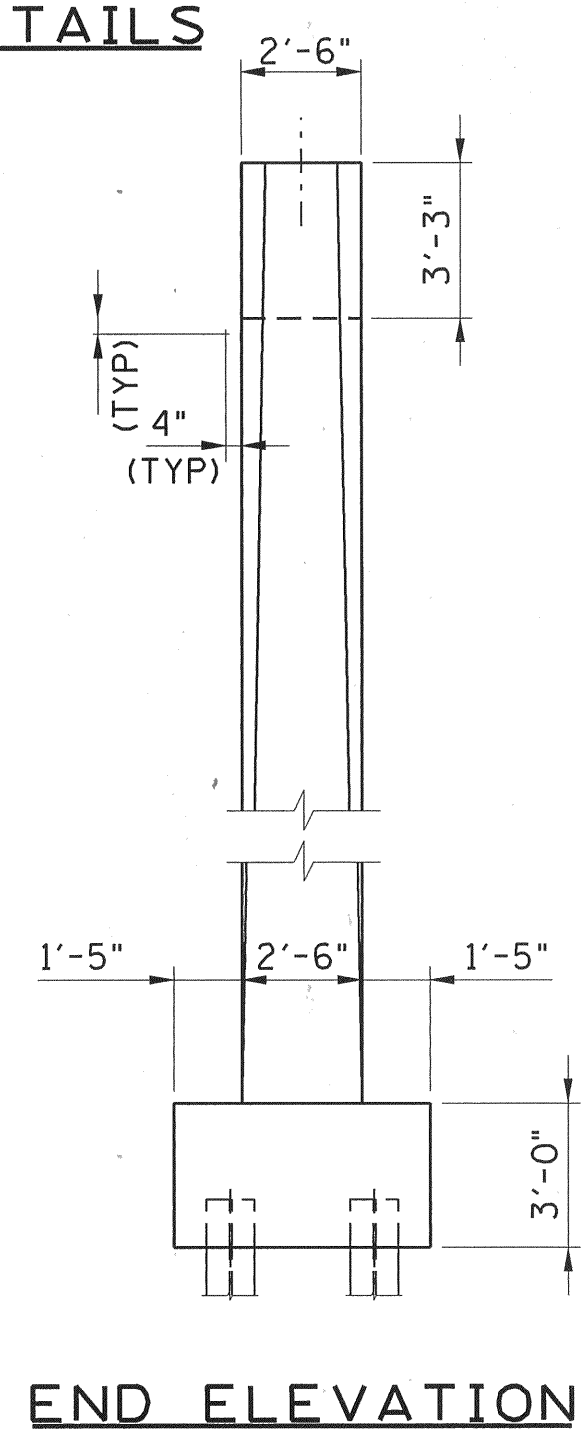
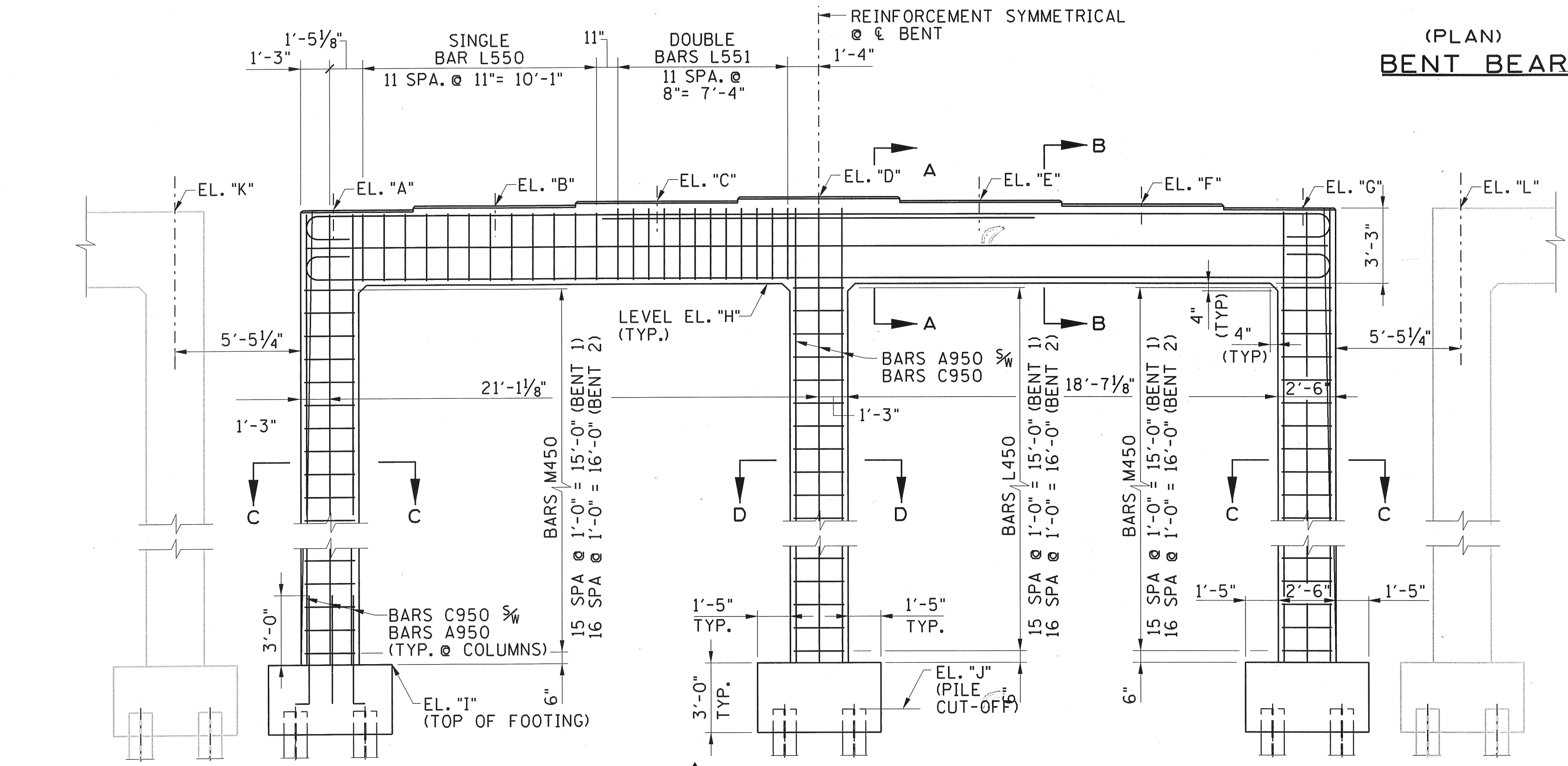
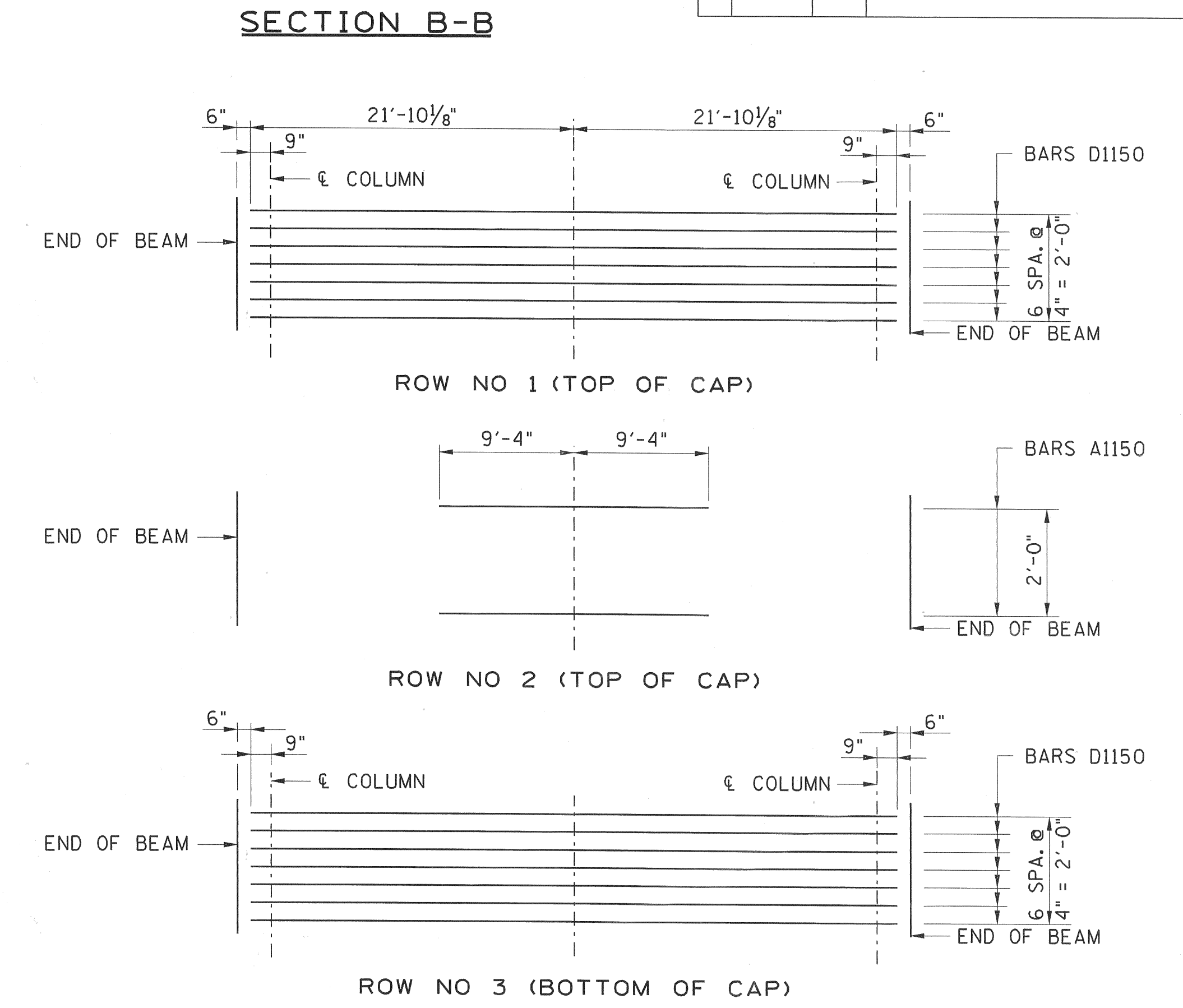
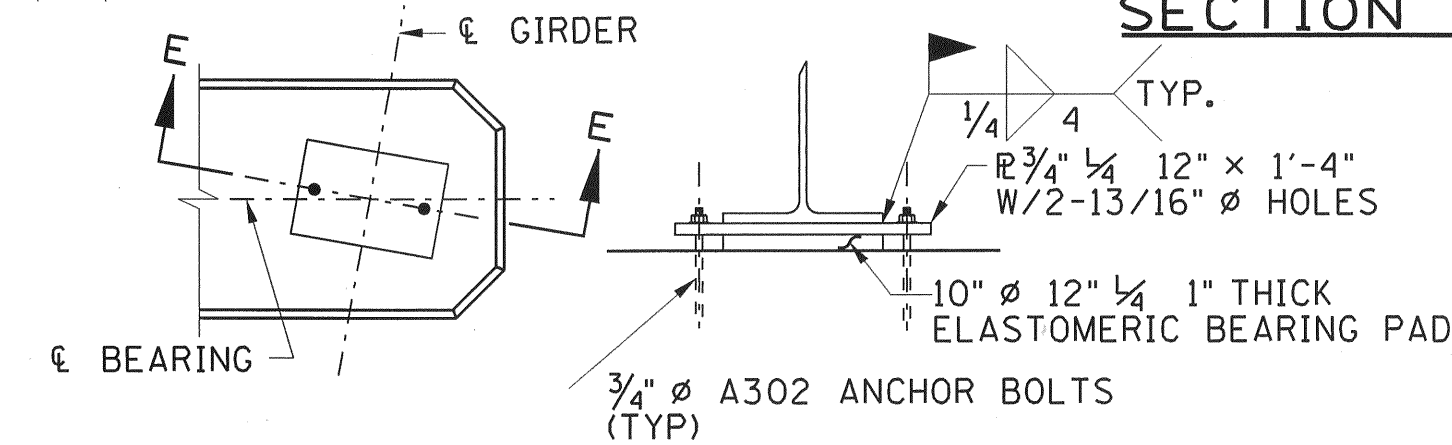
ABUTMENT NO. 2
I-40 OVER S.R. 53
STA. 48+65.68
BRIDGE NO. 80-I40-8.25 (LL & RL)
SMITH COUNTY
2004

DESIGNED BY: GSP-KTC
DRAWN BY: KTC
SUPERVISED BY: TAK
CHECKED BY: TAK





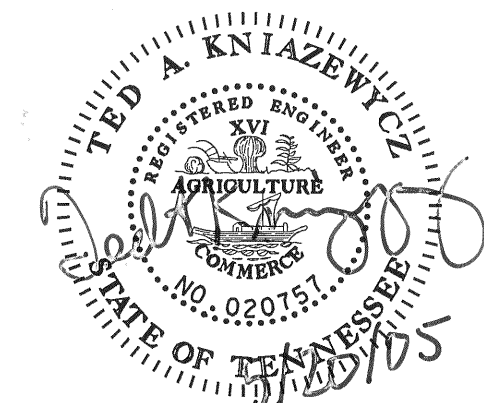
PROJECT NO.	YEAR	SHEET NO.	
80001-4178-04	2004		
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	5/20/05	JAC	UPDATED ELEVATIONS



	ELEV. "A"	ELEV. "B"	ELEV. "C"	ELEV. "D"	ELEV. "E"	ELEV. "F"	ELEV. "G"	ELEV. "H"	ELEV. "I"	ELEV. "J"	ELEV. "K"	ELEV. "L"
BENT NO. 1	574.02	574.15	574.28	574.41	574.27	574.13	573.98	570.77	551.57	549.57	573.46	573.41
BENT NO. 2	574.25	574.39	574.52	574.65	574.51	574.36	574.22	571.00	551.53	549.53	573.67	573.62

ESTIMATED QUANTITIES

CONCRETE (BRIDGES) C.Y.		STEEL BAR REINFORCEMENT (BRIDGES) LB.	
BENT 1	BENT 2	BENT 1	BENT 2
35	35	6055	6096



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

I-40 OVER S.R. 53
STA. 48+65.68
BRIDGE NO. 80-140-8.25 (LL & RL)
SMITH COUNTY
2004

DESIGNED BY GSP-KTC DATE 05/04
DRAWN BY DATE 05/04
SUPERVISED BY TAK DATE 05/04
CHECKED BY TAK DATE 05/04



PLAN SHOWING
(FOOTING REINFORCEMENT)

PLAN SHOWING
(PILE ARRANGEMENT)

