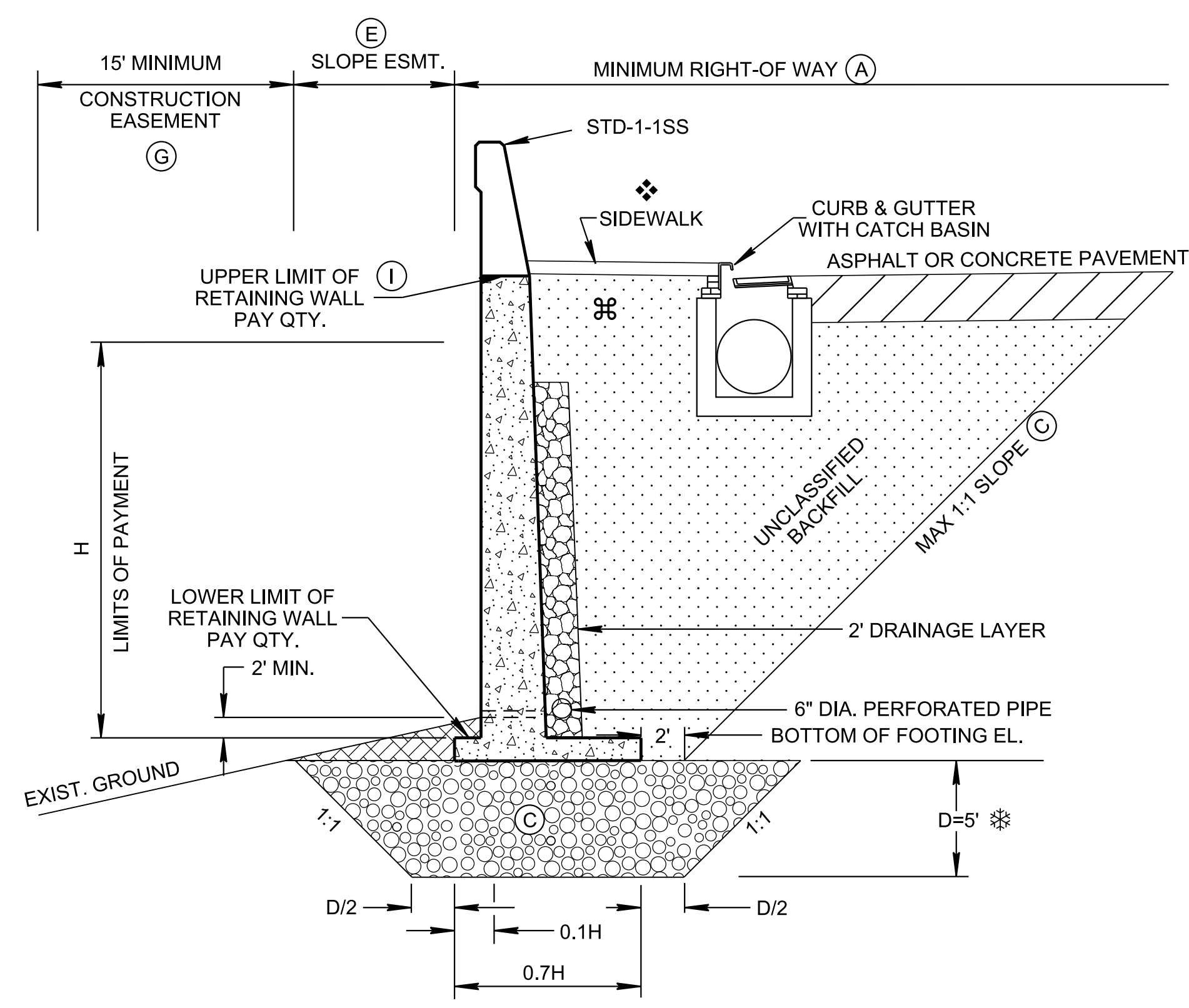


**CAST IN PLACE (CIP) WALL
TYPICAL SECTION IN CUT**



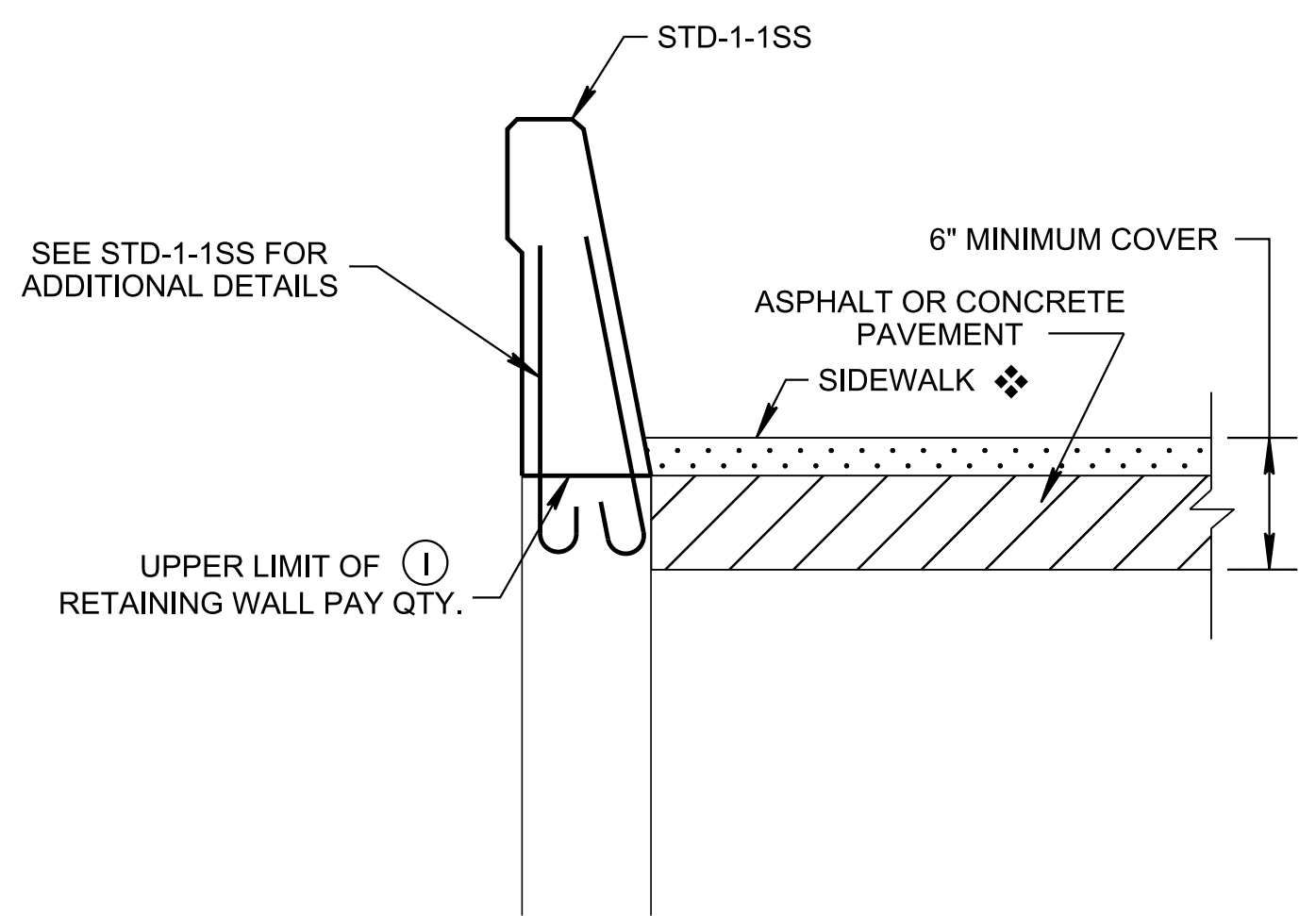
**CAST IN PLACE (CIP) WALL
TYPICAL SECTION IN FILL**

LEGEND

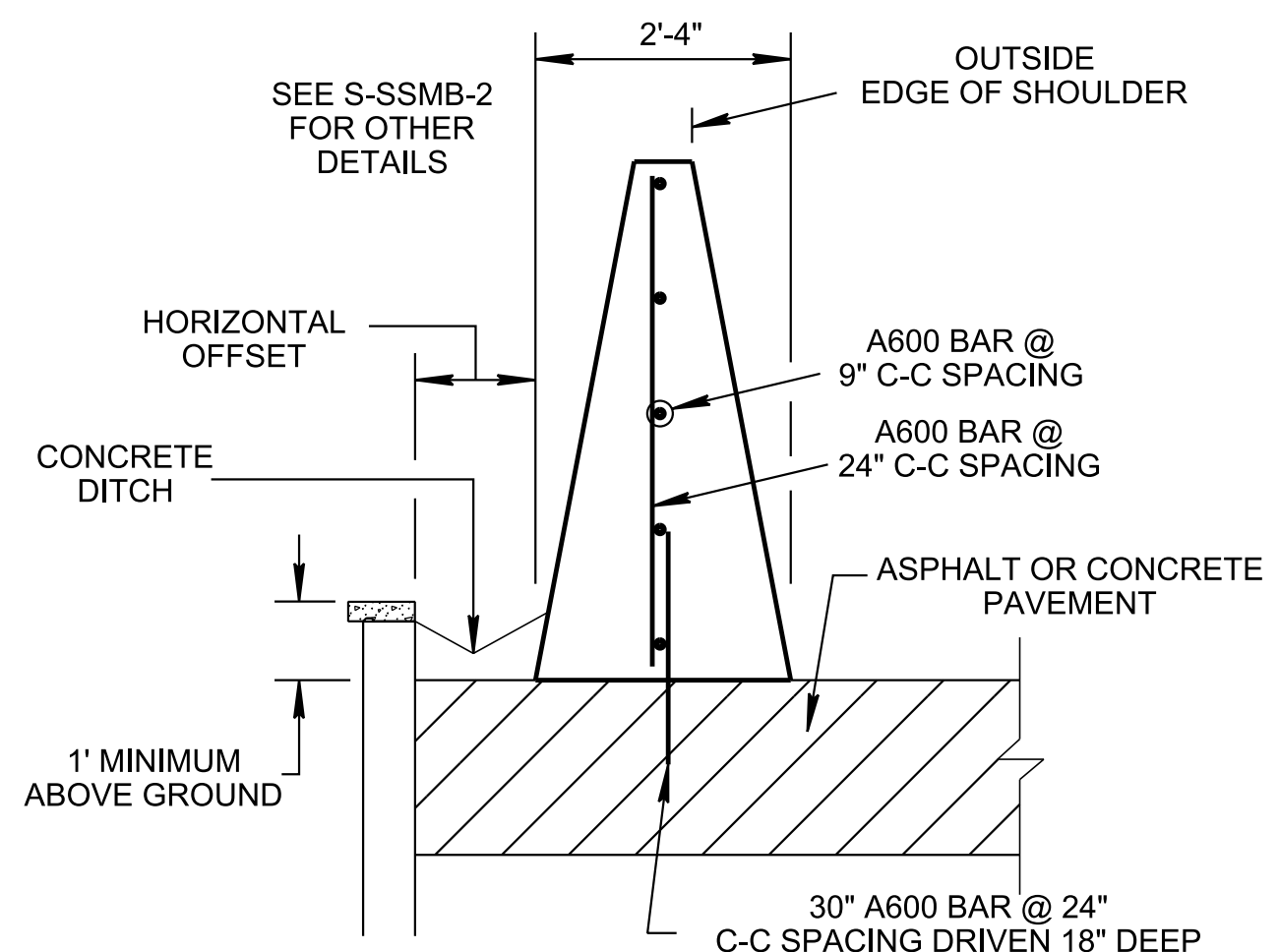
- ❖ WALL TYPE MAY ALSO BE USED WHEN ROAD SHOULDER, INSTEAD OF CURB, GUTTER, AND SIDEWALK.
- ⌘ WHEN LIGHT POLES ARE PROPOSED, WALL DESIGNER TO BE AWARE THAT THE FOUNDATION FOR THE POLES WILL LIKELY BE A MINIMUM OF 15' DEEP.
- ⊛ ONLY REQUIRED IN AREAS WITH UNSUITABLE FOUNDATION MATERIAL. 5' SHOWN ONLY FOR ESTIMATING SLOPE EASEMENT.

GENERAL NOTES

- THE PURPOSE OF THIS DRAWING IS TO BE A GUIDE AND TO ILLUSTRATE TO THE ROADWAY DESIGNER THE RIGHT-OF-WAY, SAFETY AND DRAINAGE REQUIREMENTS ASSOCIATED WITH RETAINING WALLS. THIS IS NOT A STRUCTURAL DESIGN DRAWING.**
- (A) THE ENTIRE WALL MUST BE BUILT WITHIN THE RIGHT-OF-WAY, INCLUDING FOUNDATION.
 - (B) A MINIMUM OF 15' CONSTRUCTION EASEMENT REQUIRED BEHIND SLOPE TIE IN.
 - (C) UNDERCUT DEPTH AND BACKFILL SLOPE TO BE DETERMINED BY THE GEOTECHNICAL ENGINEER.
 - (D) IF WALL IS WITHIN CLEAR ZONE OF ROADWAY, PLACE CONCRETE BARRIER WALL.
 - (E) BACKFILL AREA TO BE PURCHASED AS SLOPE EASEMENT UNTIL TIED IN WITH EXISTING GROUND LINE.
 - (F) BEGINNING AND END OF WALLS SHOULD BE PLACED OUTSIDE THE CLEAR ZONE. IF THIS OPTION IS NOT FEASIBLE, USE A TANGENTIAL GUARDRAIL END TERMINAL ATTACHED TO THE WALL ITSELF.
 - (G) AREA OUTSIDE OF WALL TO BE GRADED TO DRAIN AWAY FROM WALL. ALL GRADING TO BE INCLUDED IN CONSTRUCTION EASEMENT. WHERE AVAILABLE PURCHASING AS PERMANENT RIGHT-OF-WAY SHOULD BE CONSIDERED.
 - (H) LIMITS OF PAYMENT ARE FROM TOP OF WALL TO 2' BELOW FINISHED GRADE. ALL COSTS ASSOCIATED WITH THE FOOTING /FOUNDATION TO BE INCLUDED IN PRICE BID FOR THE WALL.
 - (I) ALTERNATIVELY, THE BARRIER WALL MAY BE DESIGNED AS PART OF THE CAST-IN-PLACE WALL. COST OF CONCRETE BARRIER INCORPORATED INTO THE RETAINING WALL SHALL BE INCLUDED IN THE COST OF THE RETAINING WALL. THE BARRIER SHALL BE DESIGNED AS TL-4 RAIL AS DEFINED BY SECTION 13 OF THE AASHTO BRIDGE DESIGN SPECIFICATIONS.



**CONCRETE BARRIER ATTACHMENT DETAIL
TO BE PROVIDED BY WALL DESIGNER**



**ALTERNATE ATTACHMENT DETAIL FOR
51" SINGLE SLOPE CONCRETE BARRIER**

4/14/2020 11:10:13 AM P:\StandDraw\DESIGN STANDARDS\Standards Drawings\Standard Roadway Drawings - CURRENT\In Progress\10-104.00 Roadway, Pavement Appurtenances and Fence IP\104.04 Walls IP\WCIP1-202005