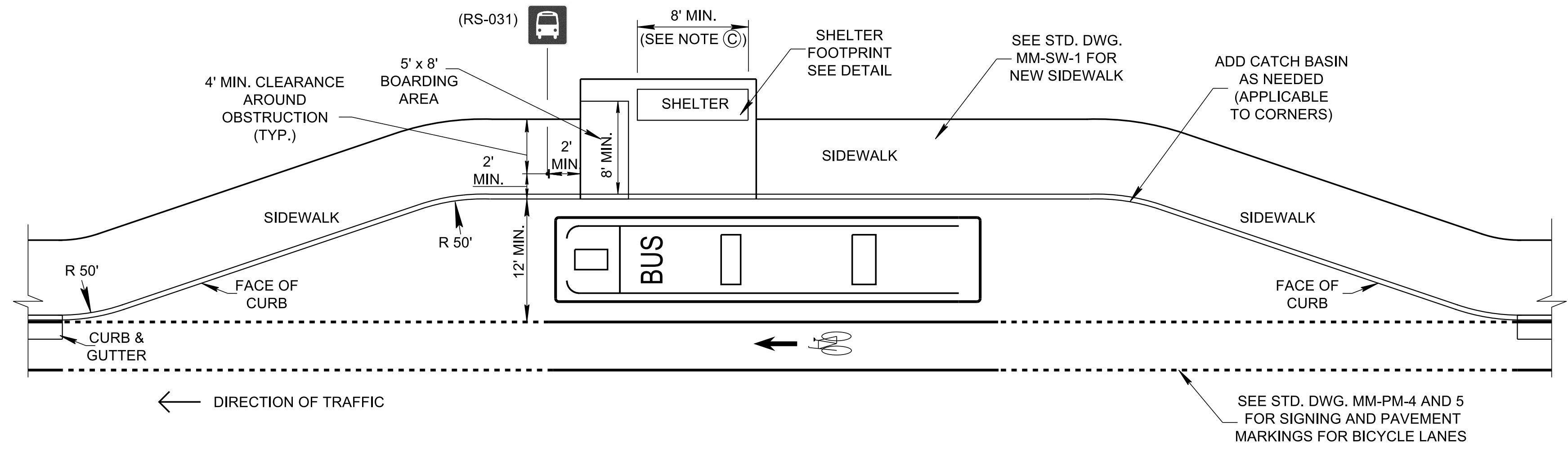
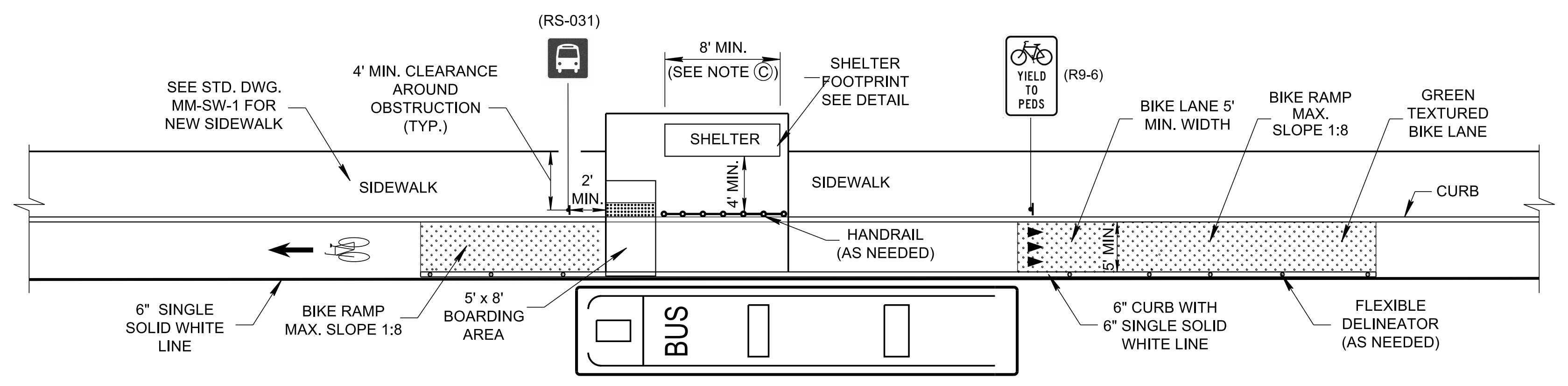


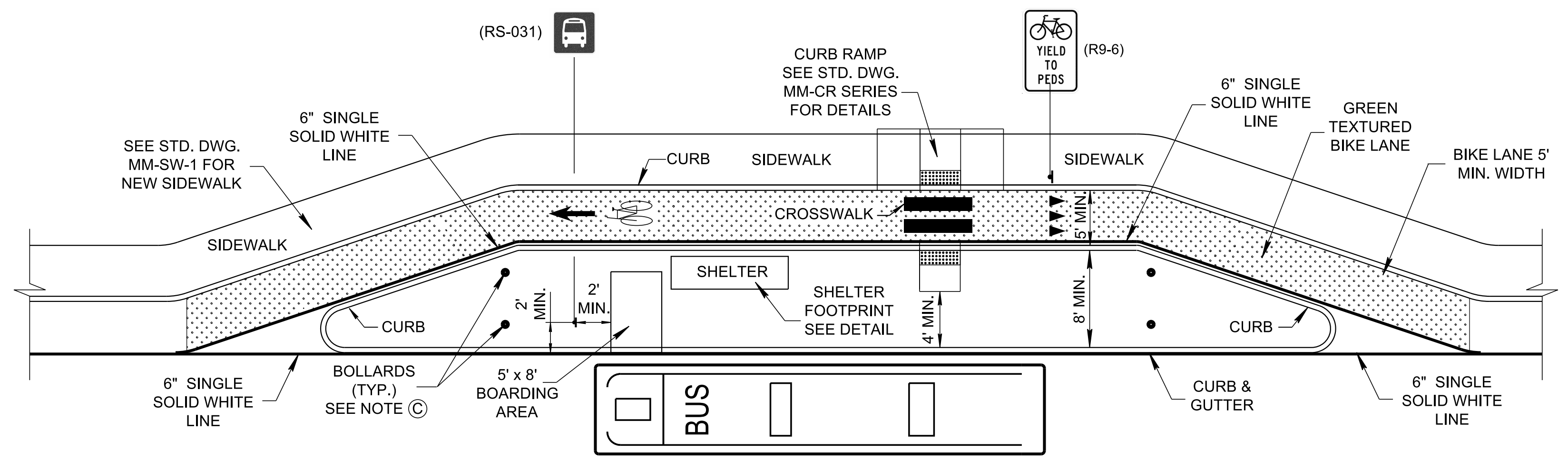
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**DETAIL A
FOR BUS PULL-OUT**

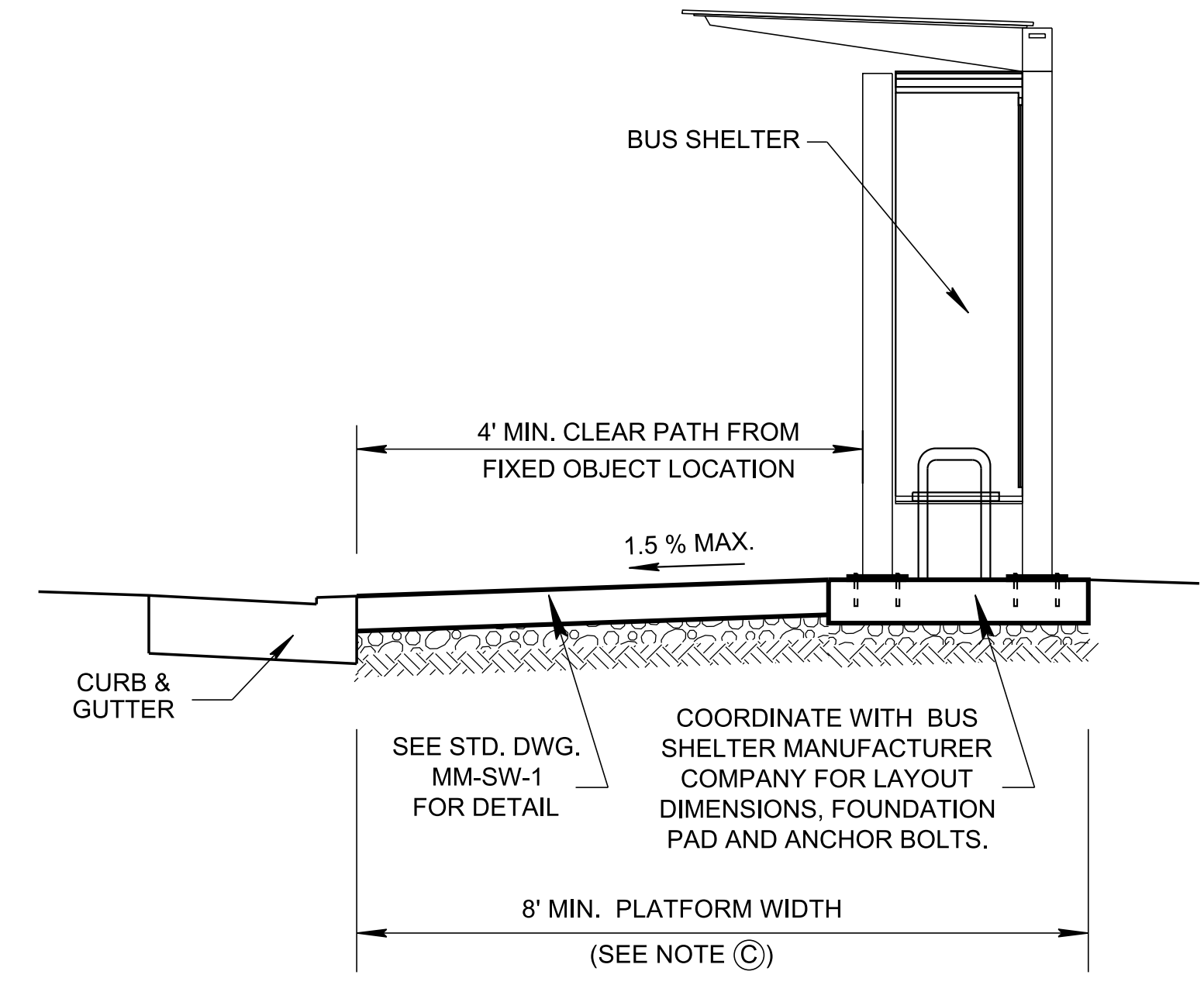


**DETAIL B
FOR BUS STOP WITH SHARED BIKE LANE**



**DETAIL C
FOR BUS BOARDING ISLAND**

LEGEND	
	GROUND MOUNT SIGN
	DETECTABLE WARNING SURFACE
	GREEN TEXTURED BIKE LANE
	DIRECTION OF TRAFFIC



BUS SHELTER DETAIL

GENERAL NOTES

- (A) THIS STANDARD DRAWING APPLIES TO THE CONSTRUCTION OR RECONSTRUCTION OF BUS STOP AND MODIFICATION OF SIDEWALKS, CURBS OR RAMPS ASSOCIATED WITH IT. TDOT ROADWAY DESIGN GUIDELINES AND ADA STANDARDS SHOULD BE REVIEWED FOR COMPLIANCE BEFORE BUS BOARDING AREA AND BUS SHELTER ARE INSTALLED OR MODIFIED.
- (B) EXISTING SITE CONDITIONS AND PEDESTRIAN INFRASTRUCTURE, PUBLIC RIGHT-OF-WAY AVAILABILITY, ACCESSIBILITY AND SAFETY ISSUES, AND OTHER CONCERNS MUST BE REVIEWED AND ADDRESSED BEFORE CONSTRUCTION.
- (C) COORDINATE WITH THE CITY PUBLIC BUS AUTHORITY FOR BUS STOP LOCATION, PLATFORM SIZE, SHELTER, BENCH, BUS STOP SIGNS AND BOLLARDS.
- (D) A MINIMUM CLEARANCE OF 48 INCHES (4 FEET) SHALL BE MAINTAINED FOR A CLEAR PATH AROUND BUS STOP ELEMENTS. MAXIMUM PLATFORM CROSS SLOPE IS 1.5%. PLATFORM SHALL BE 4" THICK CONCRETE UNLESS NOTED OTHERWISE. THE CONCRETE SHALL BE CLASS "A" AT 3000 PSI. ADD LONGITUDINAL AND TRANSVERSE JOINT MARKINGS AS NEEDED AND MATCH TO THE EXISTING SIDEWALK JOINTS. SEE STANDARD DRAWINGS MM-SW- SERIES FOR MORE INFORMATION.
- (E) WHEN THE EXISTING SIDEWALK IS LESS THAN REQUIRED PLATFORM WIDTH, IT SHOULD BE WIDENED ACCORDINGLY TO MEET CURRENT ADA STANDARDS. WHEN PLATFORM IS PLACED ADJACENT TO EXISTING SIDEWALK, THE CONTRACTOR SHALL CORRECT ALL ABRUPT CHANGES AND SLOPES TO PROVIDE A SMOOTH TRANSITION TO EXISTING SIDEWALK.
- (F) REFER TO STANDARD DRAWINGS MM-CR SERIES FOR CURB RAMPS, THE DETECTABLE WARNING SURFACE, MM-PM SERIES FOR PAVEMENT MARKINGS, AND CROSSWALK MARKING DETAILS AND MM-BPR-1 FOR BIKE AND PEDESTRIAN SAFETY RAIL.
- (G) DETECTABLE WARNING STRIPS SHOULD EXTEND PERPENDICULAR TO THE SIDEWALK AT BOTH ENDS OF THE BIKE RAMP (FROM THE TOP OF THE BIKE RAMP INTO THE SIDEWALK) TO DEMARCATE THE SHARED SPACE FOR THE VISUALLY IMPAIRED AND WARN AGAINST TRAVELING DOWN THE RAMP.
- (H) THE BIKE LANE SHOULD BE COLORED ONLY ON THE RAMPS AND IN SECTIONS LEADING UP TO THE RAMP, INDICATING THAT THE LOADING AREA IS NOT RESERVED FOR BICYCLES.
- (I) BOLLARD SHALL BE PROVIDED AT EACH SIDE OF THE ISLAND STOP. ADDITIONAL WIDTH BEYOND MINIMUM PLATFORM REQUIRED TO ACCOMMODATE BOLLARD AND TAPER.
- (J) PAYMENT

303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	PER TON
701-01-01	CONCRETE SIDEWALK (4")	PER S.F.
701-01-02	CONCRETE SIDEWALK (6")	PER S.F.
701-02.03	CONCRETE CURB RAMP	PER S.F.
702-01	CONCRETE CURB	PER C.Y.
713-15.40	SIGN INSTALLATION (DESCRIPTION)	PER LS
716-02.03	PLASTIC PAVEMENT MARKING (CROSS-WALK)	PER L.F.
716-04.12	PLASTIC PAVEMENT MARKING (YIELD LINE)	PER S.F.
716-04.21	GREEN TEXTURED BIKE LANE	PER S.Y.
716-80.01	PROTECTIVE BOLLARDS	PER EACH

STATE OF TENNESSEE
STANDARD DRAWING
DEPARTMENT OF TRANSPORTATION

TYPICAL
BUS STOP
LOCATION