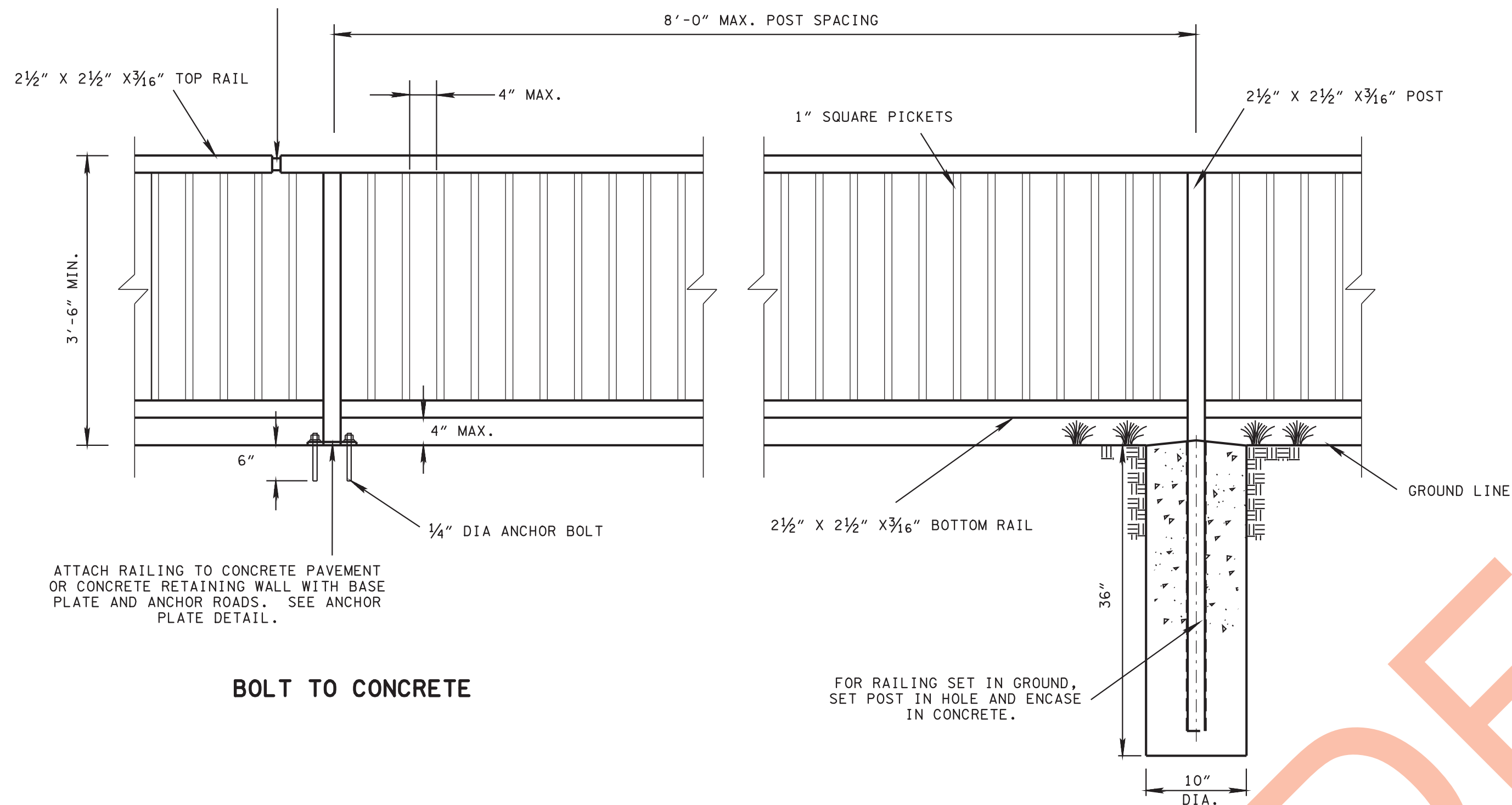


PROVIDE EXPANSION JOINT AT 48'-0" MAX. SPACING (TOP AND BOTTOM RAIL. SEE EXPANSION SLEEVE DETAIL.)

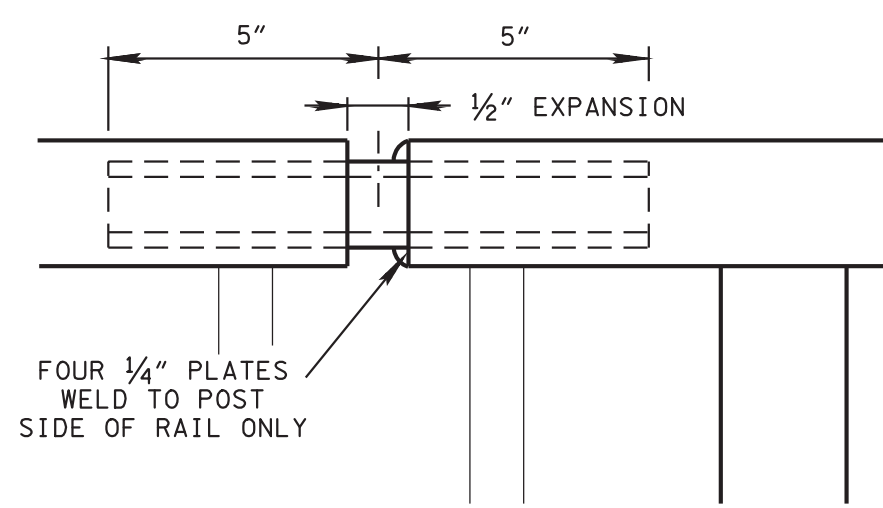


ATTACH RAILING TO CONCRETE PAVEMENT OR CONCRETE RETAINING WALL WITH BASE PLATE AND ANCHOR BOLTS. SEE ANCHOR PLATE DETAIL.

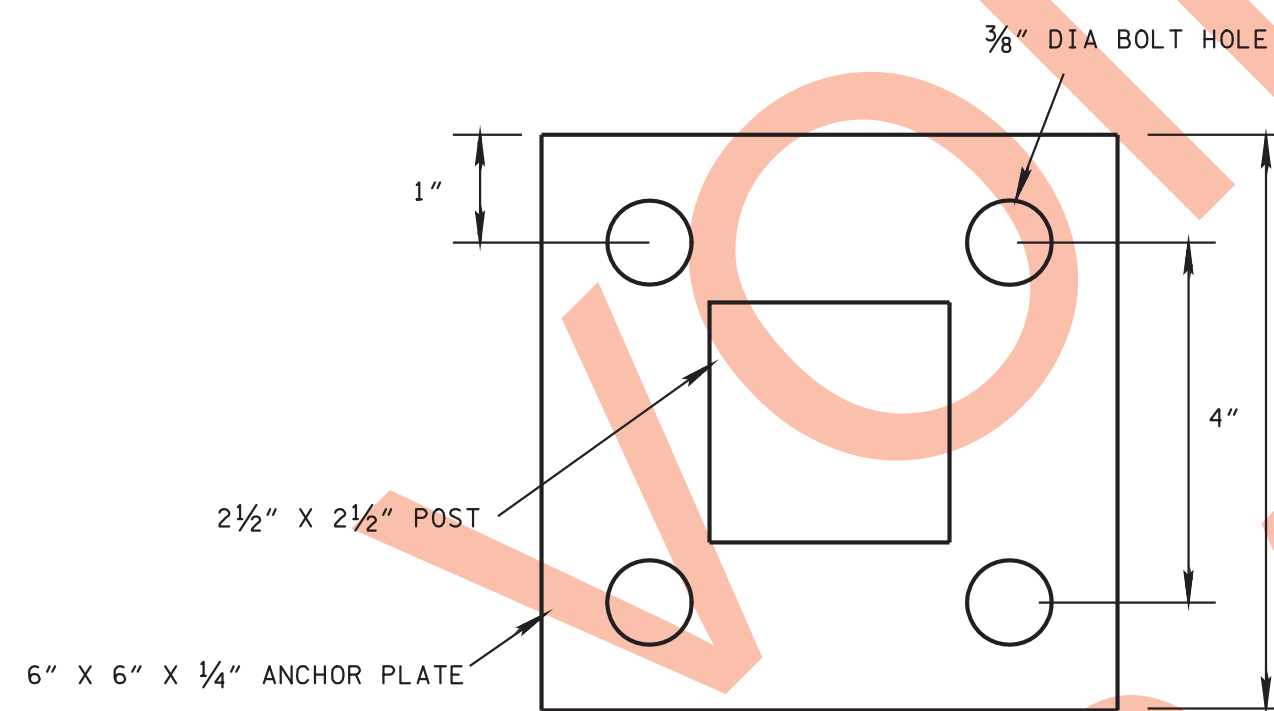
**BOLT TO CONCRETE**

FOR RAILING SET IN GROUND, SET POST IN HOLE AND ENCASE IN CONCRETE.

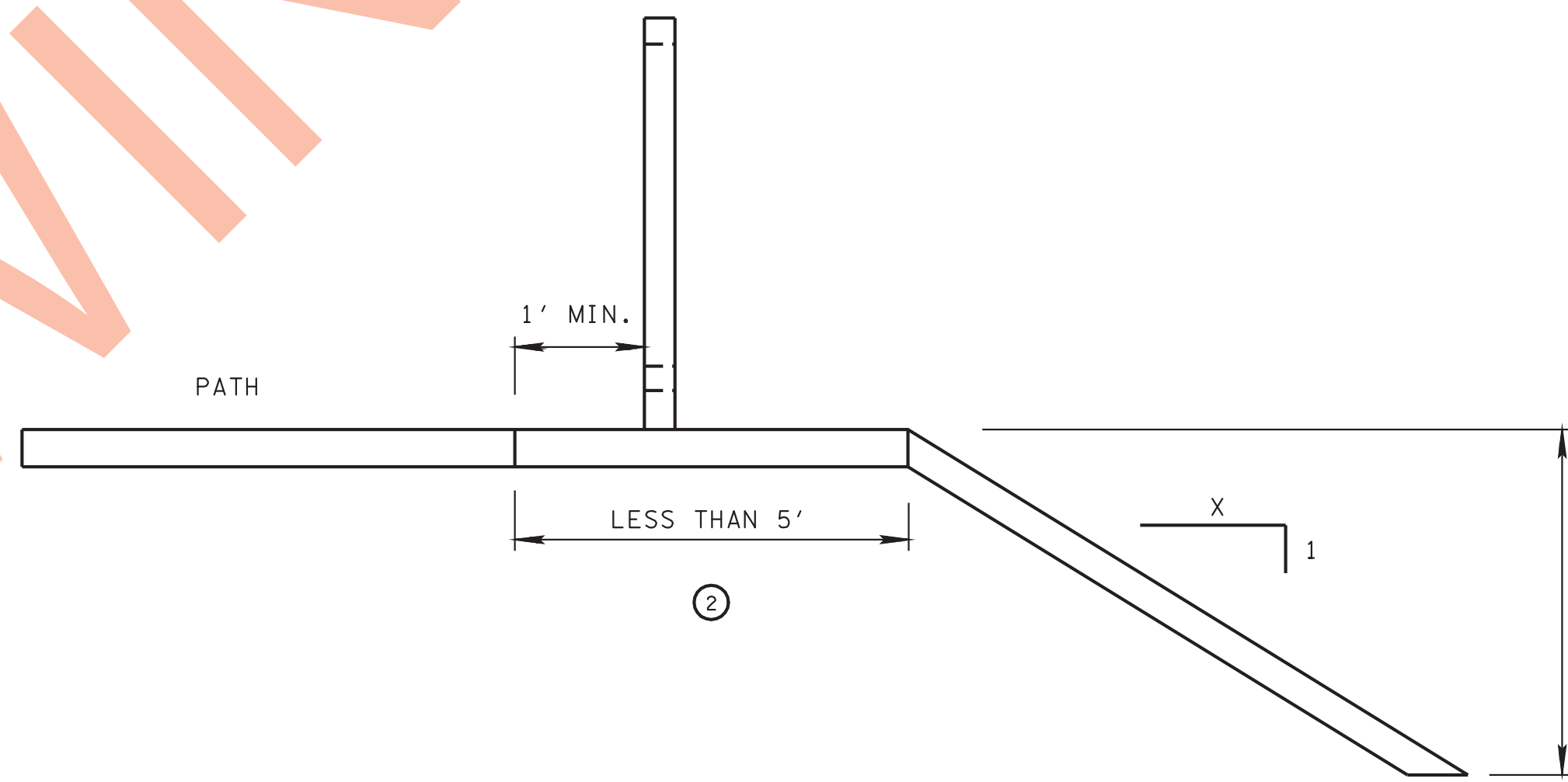
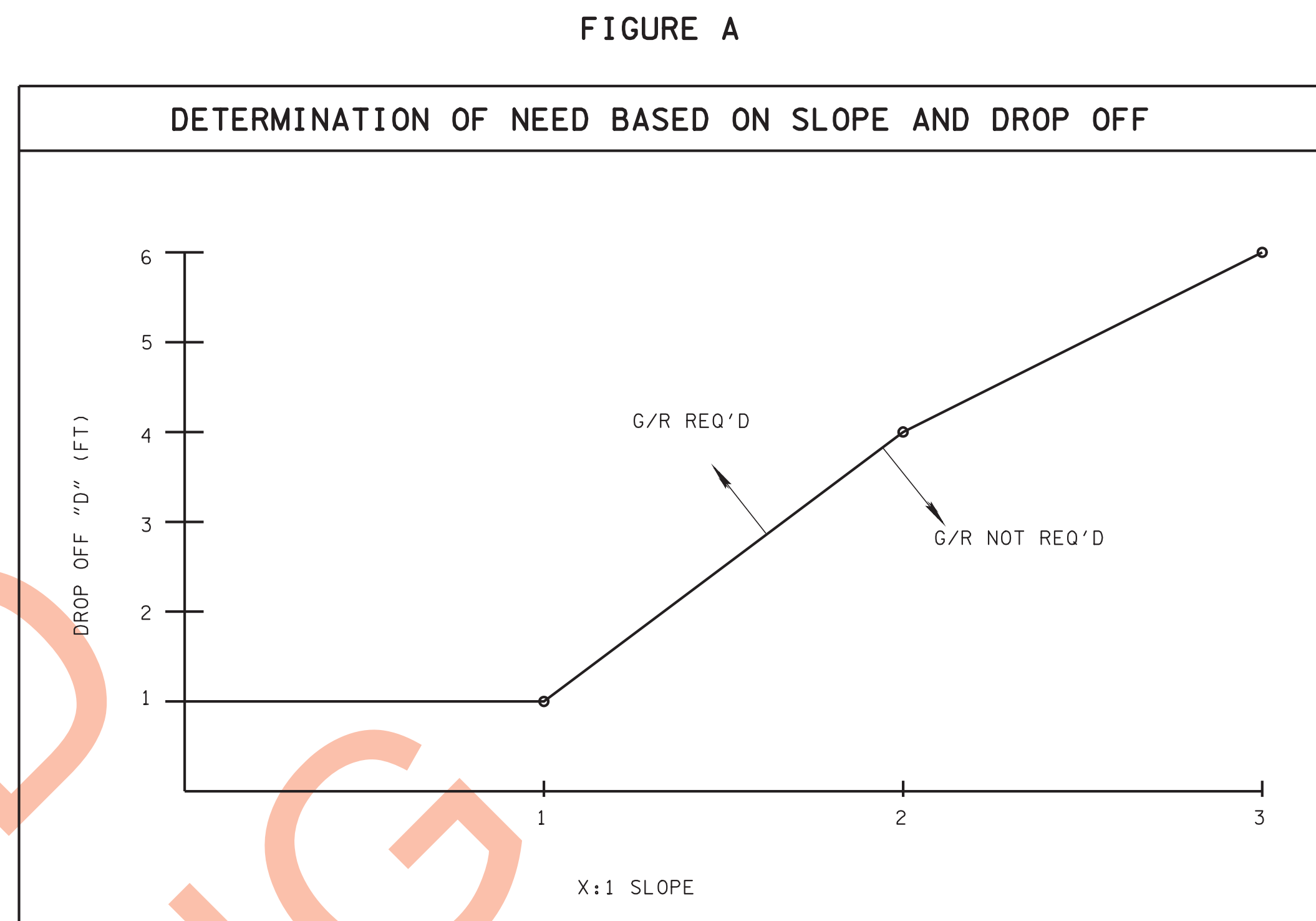
**POST SET IN GROUND**



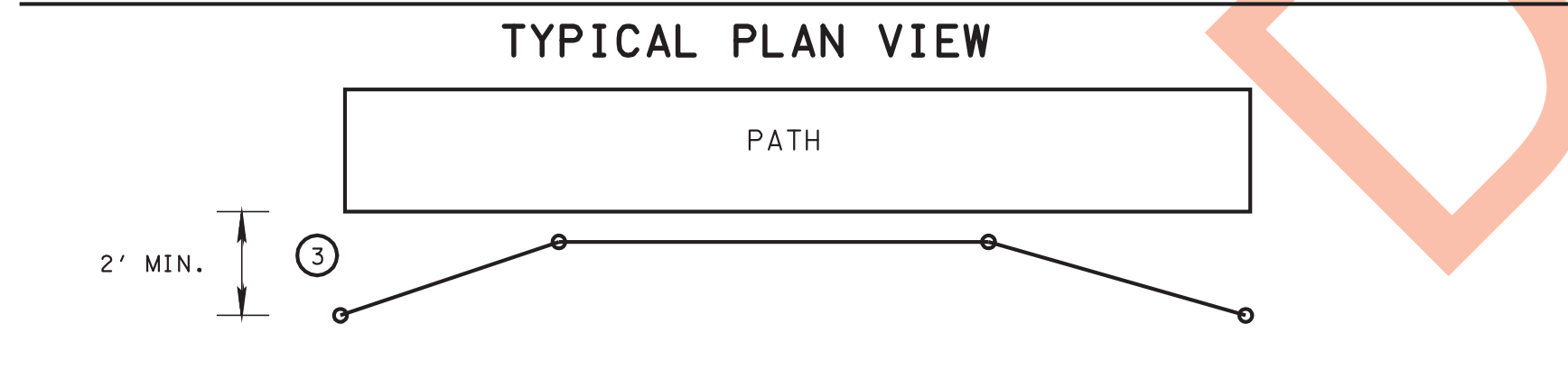
**EXPANSION SLEEVE DETAIL ⑤**



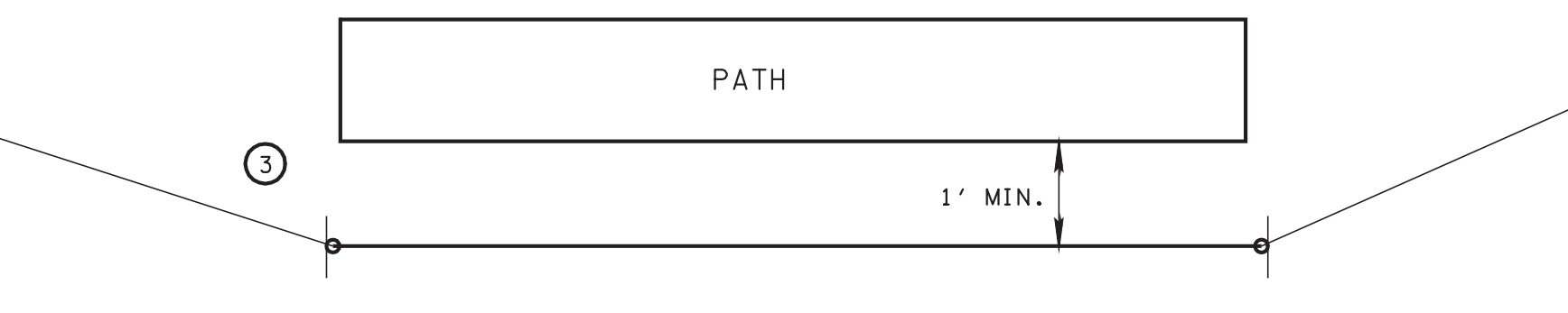
**ANCHOR PLATE DETAIL**



OM3-R



**TYPICAL PLAN VIEW**



**ALTERNATE PLAN VIEW**



OM3-L

**GENERAL NOTES**

- ① SAFETY RAIL SHALL BEGIN 25' BEFORE AND EXTEND 25' BEYOND AREA OF NEED.
- ② IF THE SHOULDER WITH A MAXIMUM CROSS SLOPE OF 6:1, IS 5' OR WIDER SAFETY RAIL IS NOT REQUIRED, BUT MAY BE INSTALLED BASED ON ENGINEERING JUDGEMENT.
- ③ SAFETY RAIL ENDS SHALL BE FLARED TO BEYOND 2' OF THE EDGE OF THE PATH OR MARKED WITH OBJECT MARKERS.
- ④ STEEL SHALL CONFORM TO ASTM A36 WELD ALL COMPONENTS 1/4" FILLET WELDS. GRIND WELDS AND CONNECTIONS AS REQUIRED TO PROVIDE A SMOOTH SURFACE, FREE OF BURRS.  
FIELD PAINT SAFETY RAIL AFTER INSTALLATION AS SPECIFIED IN THE CONTRACT DOCUMENTS.
- ⑤ DETAIL SHOWN IS FOR TOP RAIL. EXPANSION JOINT FOR BOTTOM RAIL IS SIMILAR.
- ⑥ SYSTEM REPLACEMENTS MAY BE ALLOWED PROVIDING THAT THE HEIGHT AND SPACING LIMITATIONS SHOWN ON THIS DRAWING ARE MET.
- ⑦ SAFETY RAIL (INCLUDING FOOTINGS OR ANCHOR PLATE AND BOLTS) TO BE PAID FOR UNDER ITEM NO. 604-01.04 PER LINER FOOT.

MINOR REVISION -- FHWA APPROVAL NOT REQUIRED.

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

**BIKE/PEDESTRIAN  
SAFETY RAIL**